



AGENDA
CITY OF CEDAR FALLS, IOWA
PLANNING AND ZONING COMMISSION MEETING
WEDNESDAY, NOVEMBER 11, 2020
5:30 PM AT CITY HALL VIA VIDEO CONFERENCE

To protect against the spread of the COVID-19, the meeting will be held via Videoconference. The public may access/participate in the meeting in the following ways:

- a) By dialing the phone number +1 312 626 6799 or +1 929 205 6099 or +1 301 715 8592 or +1 346 248 7799 or +1 669 900 6833 or +1 253 215 8782 and when prompted, enter the meeting ID (access code) 886 2008 9534.
- b) iPhone one-tap: +13126266799,,88620089534# or +19292056099,,88620089534#
- c) Join via smartphone or computer using this link: <https://us02web.zoom.us/j/88620089534>.
- d) View the live stream on Channel 15 YouTube using this link: <https://www.youtube.com/channel/UCCzeig5nIS-dIEYisqah1uQ> (view only).
- e) Watch on Cedar Falls Cable Channel 15 (view only).

To request to speak when allowed on the agenda, participants must click "Raise Hand" if connected by smartphone or computer, or press *9 if connected by telephone. All participants will be muted by the presiding officer when not actually speaking.

Call to Order and Roll Call

Approval of Minutes

1. Planning and Zoning Commission Minutes of October 28, 2020.

Public Comments

Old Business

2. **Site Plan Review – Hwy-1 Highway Commercial (SP20-010)**
Location: 416 Brandilynn Blvd.
Applicant: Brightwork Real Estate
Previous discussion: October 28, 2020
Recommendation: Approval
P&Z Action: Review and make a recommendation to City Council

Adjourn to Work Session

Reminders:

- * November 24 (**Note: Tuesday meeting**) and December 9 - Planning & Zoning Commission Meetings
- * November 16 and December 7- City Council Meetings

**Cedar Falls Planning and Zoning Commission
Regular Meeting
October 28, 2020
Via Videoconference
Cedar Falls, Iowa**

MINUTES

The Cedar Falls Planning and Zoning Commission met in regular session on October 28, 2020 at 5:30 p.m. via videoconference due to precautions necessary to prevent the spread of the COVID-19 virus. The following Commission members were present: Hartley, Holst, Larson, Leeper, Lynch, Prideaux, Saul and Schrad. Adkins was absent. Karen Howard, Community Services Manager, Thomas Weintraut, Planner III, and Chris Sevy, Planner I, were also present.

- 1.) Chair Holst noted the Minutes from the October 14, 2020 regular meeting are presented. Mr. Schrad made a motion to approve the Minutes as presented. Ms. Lynch seconded the motion. The motion was approved unanimously with 8 ayes (Hartley, Holst, Larson, Leeper, Lynch, Prideaux, Saul and Schrad), and 0 nays.
- 2.) The first item of business was a rezoning request from R-4 to C-3 for 2128 College Street. Chair Holst introduced the item and Mr. Weintraut provided background information. He explained that the petitioner is requesting to rezone the 0.4 acre parcel from R-4, Multiple Residence and C-3, Commercial to C-3, Commercial. Mr. Weintraut explained that the Comprehensive Plan encourages mixed-use development along College Street in this location to fill in the gap between the "Upper" and "Lower" Hill with a more cohesive, walkable retail area. He noted that the site has access to public services and the street network, but noted that the multiple driveway curb cuts were inconsistent with the code requirements, so if redevelopment occurs may need to modified. Staff recommends approval of the request.

Ms. Prideaux made a motion to approve the item. Mr. Larson seconded the motion. The motion was approved unanimously with 8 ayes (Hartley, Holst, Larson, Leeper, Lynch, Prideaux, Saul and Schrad), and 0 nays.

- 3.) The next item for consideration by the Commission was a site plan review for 416 Brandilynn Boulevard, in the HWY-1, Highway Commercial District. Chair Holst introduced the item and Mr. Sevy provided background information. He explained that the applicant is proposing a new Tidal Wave Car Wash where a car wash is currently located. He discussed the setbacks, design elements and signage and noted that generally the plan meets the code standards. At this time the item is for discussion only for input from the Commission. Mr. Sevy explained that staff just recently received an updated set of plans and that there are a few technical issues with regard to the setbacks and signage that need to be resolved so recommend continuing to the next meeting.

John Lapointe, Brightwork Real Estate, expressed his appreciation of staff's assistance and was available for any questions. The Commission had no concerns with the item. Mr. Larson asked if a recommendation could be made at this time. Ms. Howard stated that staff recommended waiting until the next meeting to give staff time to review the updated set of plans to ensure that all final technical issues had been resolved. The Commission agreed to continue the discussion at the next meeting.

- 4.) The Commission then considered subdivision code text amendments for final plat phasing. Chair Holst introduced the item and Ms. Howard provided information. She explained that the subdivision code is intended to establish minimum standards for the design, development and

improvement of subdivisions so that adequate provisions are made for public facilities and services to serve existing and future needs. It should allow growth to occur in an orderly manner, consistent with the Comprehensive Plan and to promote public health, safety and general welfare of the citizens. She explained the purpose of the preliminary plat, noting that it shows the layout of streets, blocks and lots, as well as indicating how public improvements will be extended to serve the new lots created. It identifies sensitive environmental features and how those will be protected or their loss mitigated, and shows how the new development will fit into the larger community of neighborhoods. Preliminary plats also ensure that the design will protect the interests of current and future property owners. The Code requires the owners to include the entirety of their contiguous holdings on the plat to ensure the goals are met.

Ms. Howard explained that the final plat is the next step, noting that it is the obligation of the owner/developer to design and construct all the public improvements necessary to support the proposed development. The cost of the improvements is incorporated into the price of the lots made available for sale with the final plat approval. For larger subdivisions, the land is often final platted in phases based on the pace of development, as the construction of the improvements is an upfront cost that is re-captured with the sale of the lots.

She noted that there have been complaints from residents and concerns expressed by City Council concerning final plat phasing, as there are no specific rules regarding the phasing and it has been left largely up to the developer to determine the order. In some cases critical street connections have remained unfinished and this has short term and long term costs to the community. Ms. Howard discussed the results of poorly planned subdivision phasing. She provided the example of the Ironwood connection to Greenhill and the phasing of the Prairie Winds subdivision and the added congestion it created along Erik Road because the phasing wasn't well-planned. She showed the phasing plat that was provided as an example of the issues that can create if the phasing isn't completed so the critical connections are completed earlier in the development of the subdivision. She noted that these are just a couple of examples. She also showed street patterns within the city and how the lack of street connectivity can create hidden costs to the community in the form of increased commute times, increased traffic congestion, pedestrian safety issues, increased driver frustration and speeding, increased emergency response times, inefficient routing of utilities and services.

Ms. Howard discussed the solutions, which start with establishing a standard for final plat phasing to ensure that critical infrastructure connections occur prior to less critical areas of the subdivision. The developer would be given flexibility to propose phasing that meets the pace of market demand, but the City will have the discretion to determine if the final plat phase can function as an independent development. This will ensure that no essential infrastructure improvements are being circumvented or delayed.

The next steps would be consideration of street connectivity standards that would provide multiple street stubs to subdivision boundaries to allow continuation of the street pattern on adjacent properties. Limiting block lengths, enforcing existing restrictions on cul-de-sacs and establishing standards for intersection spacing along major roadways would also be considered. Consideration of requirements for temporary construction access to reduce construction traffic on existing streets would also be made. Code amendments to establish these standards will be brought forward for discussion at a future Commission meeting.

Ms. Howard discussed staff research of other cities and how they phase their final plats. A number of the cities require phasing plans as part of the submittal process, and four of the communities address critical infrastructure through separate development agreements. However, she noted that the development agreement process can be lengthy and that it may not be necessary for every subdivision.

She noted that at the last meeting, the Commission requested language to formalize the practice of requiring a phasing plan at the time of preliminary plat. Staff agrees that codification of the practice would provide clarity to the review process, so have drafted an amendment for consideration.

Ms. Howard stated that staff is bringing the proposed amendments to address issues currently being experienced. The proposed language is drafted to ensure that each subdivision will be reviewed on its own merits and all unique circumstances can be considered. It will serve as a quality control reminder for all parties to review the plat with an eye toward ensuring that critical connections are prioritized in the phasing process. Providing clear direction in the code helps to ensure that rules are fairly and consistently applied over time. Requiring one development phase prior to a less critical phase will not necessarily mean additional cost to the developer and provided an example. The provision will ensure that a subdivision is not platted in a manner that leaves out one small segment of critical infrastructure that will then be unlikely to be completed unless by the City at taxpayer expense. Ms. Howard provided the proposed amendment to the Code. Staff recommends that the Commission discuss and make a recommendation to City Council.

Mr. Holst asked for clarification on what is being considered. Ms. Howard explained that at this time two new paragraphs are proposed; one requiring a the phasing plan at the time of preliminary plat submittal; and another paragraph that codifies review of a proposed final phasing as stated in the staff report. She explained the second paragraph purpose and what would be considered when a final plat is proposed. Mr. Larson questioned verbiage in the second paragraph being discussed, and asked if it means that a phase would need to stand on its own. Ms. Howard confirmed that was the case and gave further information. Mr. Larson asked how this new segment would have helped with Prairie Winds and Prairie West had it been in place at that time. Ms. Howard noted the original phasing plan for Prairie Winds that showed phase V that included the Ashworth connection to Aldrich Elementary School. However, an alternative phasing was allowed that split that phase at the request of the developer. This phase V did not include the critical connection of Ashworth to Arbors Drive. If the amendment had been in place, the issue of the critical street connection may have been considered more carefully.

Mr. Holst noted that requiring a phasing plan should help but only if they want to vary from the plan. Howard noted that this would help in those situations that alternate needs are present at the final plat phase and allows more flexibility for developer to request alternate phasing to be considered at that point and P&Z and Council can consider. Mr. Larson asked how much fluctuation there can be from a preliminary plat to a final plat without having to resubmit and approve the preliminary plat. Ms. Howard stated that the preliminary plat has to be in substantial compliance with preliminary plat, including the street pattern and layout of the lots. Saul asked a question about how this would have applied in Prairie Winds. Howard explained how it might have helped to have that discussion at P&Z and Council. A phasing plan hasn't always been a requirement in the past, but this Code update would ensure that staff and Council have a chance to review all the aspects being considered.

Howard also noted that the City Council will be the ultimate decision maker in the process. Mr. Larson asked what measures are in place to enforce the timing of phase completion. Ms. Howard stated that each project will be considered on its own merits at that time. The developer is making decisions based on their specific project timelines, so it will help to have the phases laid out appropriately, but it will be on a case-by-case basis. She noted the more clear and objective standards we have in the code the more clear the process is for everyone. Mr. Holst noted the benefits of having the opportunity to consider the phasing to prevent connectivity problems. Mr. Larson expressed some additional concerns. Howard noted that there is nothing in this proposal to force a developer to build a road in an area that is not yet

platted or being considered for development. Ms. Saul asked about whether this would prevent development. Prideaux clarified her understanding. Mr. Larson asked other questions about street connectivity. Howard clarified that his concerns were related to general street connectivity and is different than the issue being considered with this paragraph. Mr. Larson agreed that including additional tools in the toolbox is a good thing. He then asked a question about the specific language in the second paragraph about a development standing on its own. Howard clarified what that clause means. She gave an example of making sure stormwater management is handled so that it doesn't affect other properties around them. She noted that the phase has to have the infrastructure necessary to support the homes in that phase and allows the neighborhood to function properly, including storm water management, sewer and water connections, and streets.

Holst requested a motion. Ms. Prideaux made a motion to approve the item. Ms. Lynch seconded the motion. Mr. Holst expressed the reasons for his support of these amendments. Ms. Saul stated that she feels this will help the overall process, but doesn't want to see it hinder projects. She stated that she thinks we have a pretty good understanding now so should be helpful in the process of review.

The motion was approved unanimously with 8 ayes (Hartley, Holst, Larson, Leeper, Lynch, Prideaux, Saul and Schrad), and 0 nays.

Mr. Larson asked about the timeline for the street connectivity standards. Ms. Howard stated they can't guarantee a specific timeline for those amendments. The next large code amendment project that will be considered is the proposed downtown code update to implement the vision plan. It is unlikely that the connectivity standards could be considered before then.

- 5.) The next item of business was a zoning code text amendment regarding adaptive re-use of institutional buildings. Chair Holst introduced the item and Mr. Sevy provided background information. He explained that a recent inquiry regarding an empty church and the options for reusing the building was brought to staff. Currently, the options include sitting vacant, selling to another church, converting the use to a public or parochial school or tear down the building and subdivide it into residential lots. However, many alternative uses which may be considered to be appropriate are currently prohibited by Code. Research was done to get information on how other communities handle the re-use of these buildings and it was decided that a conditional use permitting process would be the most appropriate process to consider these requests. There are critical issues to consider, such as compatibility with residential neighborhoods, nuisance issues such as noise and traffic, allowance of neighborhood input and additional conditions depending on the proposed use and characteristics of the site. Appropriate alternative uses to qualify for consideration include hospitality-oriented uses, conversion to a multi-family unit dwelling, community services, specialized educational facilities or professional offices. He noted that according to Iowa Code the Board of Adjustment is the appropriate body to hear these requests, which was confirmed by the City Attorney.

Mr. Sevy also discussed factors to consider in the neighborhood context. Each case should be considered on its own merits, because every location is different. Considerations must be given to hours of operation, noise and traffic, number of patrons, frequency of activity, required site updates for compatibility or compliance, historical or cultural value of the site, allowed signage and neighborhood concerns. He discussed the Code language in some detail and noted some examples from other cities with regard to conditional use standards and processes. Staff recommends approval of the item.

Ms. Saul made a motion to approve the item. Mr. Leeper seconded the motion. Mr. Holst

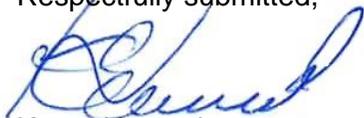
stated that he likes that the proposal brings the requests before the Board of Adjustment for approval and that he likes the way the process is structured and that each case is considered on its own merits and is not precedent setting. Ms. Saul noted that she likes the idea offering these options for these types of processes and ensuring that it fits into the neighborhood. Mr. Holst also stated that he likes that the neighborhood concerns are taken into account. Mr. Larson asked what the staff review process is like for this type of case. Mr. Sevy outlined that process and noted it would be similar to how other land use cases are handled. Howard described the Board of Adjustment meeting process and their particular purview.

The motion was approved unanimously with 8 ayes (Hartley, Holst, Larson, Leeper, Lynch, Prideaux, Saul and Schrad), and 0 nays.

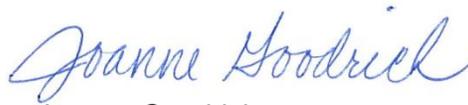
- 6.) Howard provided updates to the Commission. As there were no further comments, Ms. Lynch made a motion to adjourn. Mr. Hartley seconded the motion. The motion was approved unanimously with 8 ayes (Hartley, Holst, Larson, Leeper, Lynch, Prideaux, Saul and Schrad), and 0 nays.

The meeting adjourned at 7:03 p.m.

Respectfully submitted,



Karen Howard
Community Services Manager



Joanne Goodrich
Administrative Assistant

DEPARTMENT OF COMMUNITY DEVELOPMENT



City of Cedar Falls
220 Clay Street
Cedar Falls, Iowa 50613
Phone: 319-273-8600
Fax: 319-273-8610
www.cedarfalls.com

MEMORANDUM
Planning & Community Services Division

TO: Planning & Zoning Commission
FROM: Chris Sevy, Planner I
DATE: November 11, 2020
SUBJECT: Tidal Wave Site Plan: Case # SP20-010

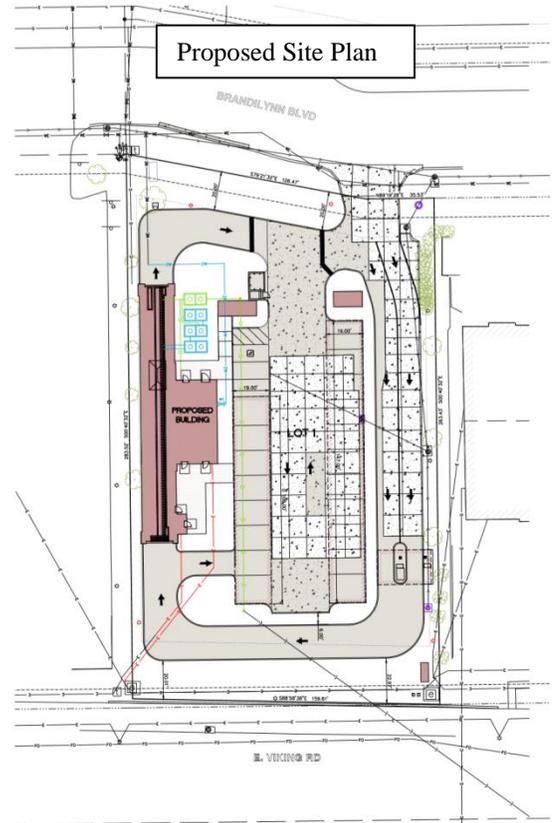
REQUEST: Site plan approval for construction of a new car wash.
PETITIONER: Brightwork Real Estate (developer); MMS Consultants (engineer)
LOCATION: 416 Brandilynn Boulevard

PROPOSAL

The applicant proposes to construct a one story, 3,538 square foot Tidal Wave, a car wash concept new to Cedar Falls, on 416 Brandilynn Boulevard. The proposal includes a canopied pay station, a drive through car wash facility, canopied stations for patrons to vacuum their cars, and a dumpster enclosure. The proposed layout of the site sits on about 1 acre and is oriented toward Brandilynn Boulevard. This project will include demolition of the existing car wash facility that currently sits on the lot.

BACKGROUND

This area was rezoned from (A-1) Agricultural District to (HWY-1) Highway Commercial District in 1998 which is when this area began to be developed. The GenCom Subdivision plat, of which this parcel is a part, was approved in 2003 and the existing car wash on the site was approved and built in 2004.



ANALYSIS

The HWY-1 District is intended to promote general service commercial uses that serve a broader market area (i.e. city-wide or regional customer base). The ordinance requires a detailed site plan review prior to approval in order to ensure that the development site satisfies a number of basic aesthetic standards. Attention to details such as parking, open green space, landscaping, signage, building design and other similar factors help to ensure orderly development in the entire area.

Following is a review of the zoning ordinance requirements for the proposed development:

- 1) Use: Service stations are listed as a permitted use in the HWY-1 District and a car wash fits into that category. **Use is allowed.**
- 2) Setbacks: There are platted building setbacks of 20 feet along Brandilynn Boulevard and 30 feet along Viking Road. The building is 62 feet from Brandilynn and 80 feet from Viking Road. At these distances the building more than meets these setbacks. The default side yard setback is 5 feet. The district also requires that a 20-foot landscaped setback be maintained along streets and accessways. Drives, aisles, and parking areas may not encroach into this landscaped setback. The applicant's sit plan conforms to these standards. **Building setbacks are satisfied. The parking lot setbacks are also satisfied.**

3) Parking: For an automatic carwash, five stacking spaces are required for each washing bay. There are at least six stacking spaces between the pay station and the bay entrance and seven more stacking spaces behind the pay station in each lane (there are two lanes approaching the pay station). Stacking space requirements are enumerated in the code for vacuuming units. However, this rule is intended for a drive-thru format that is similar to that of a fuel pump at a filling station and is meant to ensure that waiting vehicles do not stack into the public right-of-way. The applicant proposes a concept/format that provides 19 vacuum stations that are in the form of large parking stalls, which are adequate in number to prevent stacking. Therefore, it is not practical to require stacking spaces behind these stalls. Based on the projected number of employees, the applicant figures that two parking spaces are required (one space for every two employees). Based on the site plan they are providing three parking spaces (independent of the vacuum stalls) including one ADA space. The parking meets the zoning standards with minimum stall dimensions of 9' x 19' (vacuum stalls are wider) and a 30-foot wide aisle (24 feet is the minimum requirement).

The amount and dimension of the parking stalls and drives are met.

4) Open Green Space: The HWY-1 District requires that open green space/landscape area be provided at the rate of 10% of the development site excluding the required 20-foot landscaped setback area. Below is a summary that details how this provision is met.

<u>New Development Site</u>	<u>43,190 SF</u>	
Required Open/Green Space	4,319 SF	10%
Provided Open/Green Space	8,752 SF	20.3%

Landscaping is shown throughout the site, both around the building as well as within the parking lot, along the streets and required setbacks.

The open green space exceeds the minimum requirement and is well distributed.

5) Landscaping: The HWY-1 District requires landscaping at the rate of 0.02 points per sq. ft. of total development site area which comes to 864 points required. Their plan achieves 870 points. An additional 241 points are required for street tree planting and their plan achieves 250.

Trees are required in the vehicular use area at the rate of one overstory tree per 15 parking spaces. This equates to a requirement of 2 overstory trees. 3 overstory trees are proposed within the parking area to meet this requirement.

In addition to parking lot trees, there are trees located along the street frontages, with shrubs and additional trees being located around each of the buildings as required. The required screening around the parking/vehicular use area is also fulfilled by their proposed plan. **Landscaping requirements are met.**

- 6) **Building Design:** The HWY-1 zone requires a design review of various elements. The intent of the zone is to promote and facilitate comprehensively planned commercial developments located adjacent to major transportation corridors and interchanges. It is further the purpose of the zone to encourage high standards of building architecture and site planning. The elements in the code are noted below with a review on how each is addressed. **Overall, staff finds that the proposed building design meets the zoning standards based on the following analysis.**



An existing Tidal Wave Car Wash that reflects the design of the proposed building. This photo was taken from their company website.

Proportion: The relationship between the width and height of the front elevations of adjacent buildings shall be considered in the construction or alteration of a building; the relationship of width to height of windows and doors of adjacent buildings shall be considered in the construction or alteration of a building.

Renderings of the proposed building have been submitted. The scale and proportion of the new building will be similar to the existing building and the surrounding buildings. It is 1-story with 14 foot wall heights and a 6/12 roof pitch. The maximum height of the proposed building is 28 feet (at the tip of the cupola) while the general roofline sits at 21 feet. The neighboring buildings to the east and west have similar dimensions though their maximum heights don't appear to be quite as high. Staff finds that the proposal is consistent with neighboring buildings and with the height allowances in the zoning district.

Roof shape, pitch, and direction: The similarity or compatibility of the shape, pitch, and direction of roofs in the immediate area shall be considered in the construction or alteration of a building.

The adjacent buildings utilize flat roofs with a parapet around the edge. The IHOP next door has some roof style features similar to the cupola on the proposed building. The proposed building and the vacuum canopies will not utilize a flat roof and instead have a 6/12 pitch (rise/run). This is similar to the existing building on site and does not appear to be incompatible with the surrounding buildings.

Pattern: Alternating solids and openings (wall to windows and doors) in the front facade and sides and rear of a building create a rhythm observable to viewers. This pattern of solids and openings shall be considered in the construction or alteration of a building.

The new building will feature stone veneer façades with varying surfaces of stucco, precast concrete, and masonry veneer. The sides of the building will have clear windows running the course of the drive-thru and providing natural light to patrons. Overall, the building materials and patterns meet or exceed the city's standards.

Materials and texture: Materials and texture. The similarity or compatibility of existing materials and textures on the exterior walls and roofs of buildings in the immediate area shall be considered in the construction or alteration of a building. A building or alteration shall be considered compatible if the materials and texture used are appropriate in the context of other buildings in the immediate area.

Glass, stone, stucco, architecturally finished concrete masonry, and metal roof are the exterior materials proposed for the new building. All of the materials besides the roof, doors, and trim will provide a masonry texture. These are similar material and texture choices to those of adjacent buildings.

TIDAL WAVE AUTO SPA - EXTERIOR FINISH SCHEDULE:

The schedule includes the following items:

- GLAZING - CLEAR**
(PROVIDE SPANDREL GLAZING IN GLAZED GABLE TRUSSES LOCATED AT ENCLOSED ATTICS)
- PREFINISHED METAL ROOF**
COLOR - REGAL BLUE
(PREFIN SOFFIT, FASCIA, FLASHING COLOR-MATCH ROOF COLOR)
- PRECAST WATERTABLE COURSE**
OLD CASTLE - WHITE
- STONE VENEER**
KENTUCKY LEDGESTONE
- STOREFRONT FRAME**
COLOR - BRONZE
- EXPOSED STRUCTURAL STEEL**
TRUSSES AND COLUMNS.
PAINT - BLACK
- HM DOORS/FRAMES**
PAINT SW 2735 ROCKWEED
- SPLIT FACE MASONRY**
OLD CASTLE -

DO NOT PAINT BOTTOM SIDE OF ANY EXPOSED METAL ROOF PANEL

Color: The similarity or compatibility of existing colors of exterior walls and roofs of buildings in the area shall be considered in the construction or alteration of a building.

With exception of the blue roof, the new building will primarily use lighter earth tones with minor accents such as frames and doors that are darker. These colors are typical of the Tidal Wave brand and are used on most of their locations.

Architectural features: Architectural features, including but not limited to, cornices, entablatures, doors, windows, shutters, and fanlights, prevailing in the immediate area, shall be considered in the construction or alteration of a building. It is not intended that the details of existing buildings be duplicated precisely, but those features should be regarded as suggestive of the extent, nature, and scale of details that would be appropriate on new buildings or alterations.

Architectural features of the new building include a cross gabled roof, a cupola, an abundant use of windows, and a well-textured façade using stone and masonry. The roof line only varies at the cupola. These style features and materials maintain identity while being generally consistent with neighboring buildings.



- 7) Trash Dumpster Site: The dumpster and enclosure is located near the northeast corner of the building. This enclosure will be made of concrete masonry and stucco that will match materials used on the principal structure. It will have two doors to operate towards the east where cars enter and exit the vacuum area. The enclosure will measure 8' by 15'. **Dumpster facility location and enclosure is acceptable.**

8) **Lighting:** The HWY-1 District regulations do not have specific lighting design guidelines. However, all new site plans require review of the lighting to consider potential nuisance issues and incompatibilities. The vehicular use area will utilize seven 20-foot high fully downcast fixtures produced by CREE Inc. (example fixture pictured to the right). The building will utilize wall-mounted and shielded downcast lights produced by Williams Outdoor. These models appear to properly light critical areas of the site without producing glare or spillover light directing light on to other properties. **Lighting is acceptable.**



1) **Signage:** Separate sign permits will be required for all signage prior to installation. However, the proposed signage plan for the site must be part of this review by the Planning and Zoning Commission and City Council. The signage plan has been included as an attachment.

Wall signs may only be on two wall surfaces and must be less than 20% of the wall area. For the new building, two wall signs are illustrated on the building above the exit and entrance of the automatic wash bay which face north and south, respectively. These signs have a sign area of 19 square feet on walls with an area of 217 square feet. These wall signs meet the quantity and size requirements.

Freestanding signs are not limited in number but are limited in height to 25 feet above the surface of the roadway and their aggregate sign area should not exceed 250 square feet. The following is a breakdown of freestanding signs with heights and sign areas:

<u>Sign Type</u>	<u>Sign Area (sq ft)</u>	<u>Height (feet)</u>
Monument	98.3	20
Menu Signs	55	9.17
Dumpster Door Panels	65	7.5
Total:	218.3	

There are also several directional signs. Their height must be less than 6 feet and have a sign area less than 6 square feet. The proposed directional signs meet these requirements.

Signage is acceptable.

2) **Storm Water Management:** Due to the regional detention basin installed during the Gencom Phase II development, including Walmart and the surrounding sites, the engineering division has determined that no additional detention is required.

However, water quality improvements are required with redevelopment. A water quality device will be installed on-site to collect water run-off prior to it entering the storm sewer network along Brandilynn Boulevard. **Criterion is met, subject to final engineering review prior to issuance of a building permit.**

TECHNICAL COMMENTS

Cedar Falls Utilities (CFU) has reviewed the site plan for Tidal Wave Auto Spa. Water, electric, gas, and communications are all available to the site in accordance with CFU Service Policies. The Utility Plan shows a new 2” water service connecting to an existing 8” water service into the property. The existing water service is connected to the 12” water main with a 4” valve and reduces to a 2” copper water service. Water services in Cedar Falls are owned, installed and maintained by the property owner. Gas services are owned, installed and maintained by CFU. The Utility Plan shows a new gas service connected to the existing gas service near the northwest corner of the new building. HVAC or plumbing contractor will make the gas piping connection to the CFU installed gas meter. The general contractor must contact CFU for coordination of utility service locations.

Any other minor remaining technical issues will be addressed prior to Council approval or at the time of building plan review.

A courtesy notice to surrounding property owners was mailed on October 21, 2020.

STAFF RECOMMENDATION

The Community Development Department has reviewed this site plan and recommends approval, subject to the following conditions:

1. Developer’s plan must conform to all city staff recommendations and technical requirements.

PLANNING & ZONING COMMISSION

Introduction and Discussion 10/28/2020	The next item for consideration by the Commission was a site plan review for 416 Brandilynn Boulevard, in the HWY-1, Highway Commercial District. Chair Holst introduced the item and Mr. Sevy provided background information. He explained that the applicant is proposing a new Tidal Wave Car Wash where a car wash is currently located. He discussed the setbacks, design elements and signage and all requirements have been met. At this time the item is for discussion only for input from the Commission. Mr. Sevy explained that staff just recently received the completed plans and that they are finishing the review process.
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John Lapointe, Brightwork Real Estate, was available for any questions. The Commission had no concerns with the item. Ms. Howard stated that staff will be reviewing the plans to ensure that all requirements are met.

Attachments: Complete Site Plan Set (Submitted 10/22)
Proposed Signage Plan
Proposed Building Elevations

Proposed Canopy Elevations

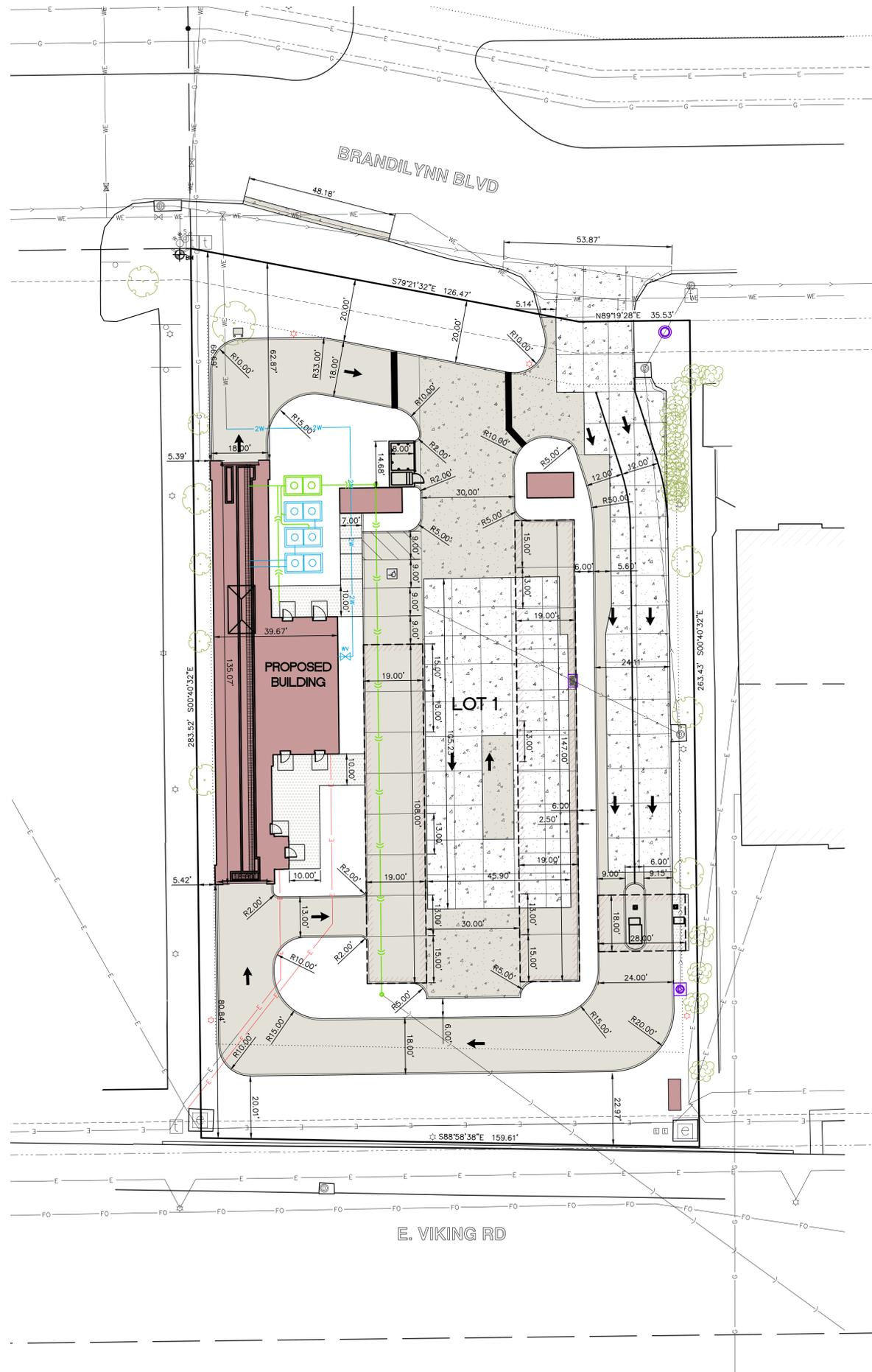
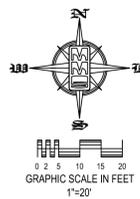
SITE PLAN GENCOM ADDITION, LOT 1 CEDAR FALLS, IOWA

PREPARED BY:
MMS CONSULTANTS INC.
1917 S. GILBERT STREET
IOWA CITY, IA 52240

OWNER:
VIKING ROAD WASH PARTNERS
416 BRANDILYNN BLVD
CEDAR FALLS, IA 50613

APPLICANT:
BRIGHTWORK REAL ESTATE
ATTN: JOHN LAPOINTE
3708 WEST SWANN AVENUE, SUITE 200
TAMPA, FL 33609

APPLICANT'S ATTORNEY:
GARDNER BREWER MARTINEZ-MONFORT LAW
ATTN: CHRIS BREWER
400 NORTH ASHLEY STREET, SUITE 1100
TAMPA, FL 33602



LEGAL DESCRIPTION
LOT 1, GENCOM ADDITION, CEDAR FALLS, BLACK HAWK COUNTY, IOWA EXCEPT THAT PART CONVEYED TO THE CITY OF CEDAR FALLS IN DOCUMENT #2018-10402. RECORDER'S OFFICE. SAID PARCEL CONTAINS 0.99 ACRES AND IS SUBJECT TO EASEMENTS AND RESTRICTIONS OF RECORD.

PROPOSAL
APPLICANT PLANS TO DEVELOP A CAR WASH FACILITY ON THE 0.99 ACRE SITE.

DEVELOPMENT SCHEDULE

PHASE	AREA OF WORK	LAND USE TYPE(S)	CONSTRUCTION PERIOD
LOT 1	RETAIL (CAR WASH)		FALL 2020 THRU SUMMER 2020

DEVELOPMENT CHARACTERISTICS
CURRENT ZONING IS HWY-1 (HIGHWAY COMMERCIAL)

SETBACK REQUIREMENTS

BUILDING SETBACKS:	REQUIRED
FRONT YARD	20 FEET
SIDE YARD	9 FEET
REAR YARD	30 FEET

LOT CHARACTERISTICS

LOT AREA	43,190 SF (100%)(0.99 AC)
BUILDING AREA - PROPOSED	3,538 SF (8.2%)
VACUUM CANOPIES/AMENITIES	5,629 SF
NOTE: VACUUM CANOPIES/AMENITIES CALCULATIONS INCLUDE OVERHEAD STRUCTURES THAT ARE LOCATED ABOVE EXISTING AND PROPOSED PAVING AREAS	
TOTAL BUILDING AREAS	9,167 SF
PAVING AREA - PROPOSED	16,145 SF (37.4%)
PAVING AREA - EXISTING (TO REMAIN)	8,895 SF (20.6%)
GREEN SPACE AREA	14,912 SF (33.8%)

PARKING REQUIREMENTS
RETAIL (CAR WASH) 1 SPACE / 2 EMPLOYEES = 2 SPACES
3 STANDARD/EMPLOYEE SPACES PROVIDED (1 ADA)
19 ADDITIONAL WORKING SPACES PROVIDED (VACUUM AREAS)
TOTAL SPACES PROVIDED: 22 SPACES (1 ADA)

GENERAL NOTES
- ALL GROUND MOUNTED MECHANICAL UNITS SHALL BE SCREENED WITH LANDSCAPING
- SEE SIGNAGE PACKAGE FOR FINAL SIGN LAYOUT.

LIGHTING
- FREE STANDING LIGHT POLES NOT TO EXCEED 25 FEET.
- PARKING LIGHTS TO BE NON-ADJUSTABLE DOWNLIGHTS.
- WALL MOUNTED LIGHTS NOT TO BE FORWARD THROWING.
- MAXIMUM WATTAGE = 250 WATTS.
- LIGHT SPREAD SHALL NOT IMPACT BEYOND PROPERTY LIMITS.
- SEE LIGHTING PACKAGE FOR FINAL LAYOUT.

THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL PER IDOT STANDARD ROAD PLAN TC-419 OR SUDAS 8030-110 AND CITY OF CEDAR FALLS REQUIREMENTS AT ALL TIMES DURING WORK WITHIN PUBLIC R.O.W.

THE CONTRACTOR SHALL COORDINATE WITH UTILITY PROVIDERS FOR ANY REQUIRED RELOCATION OF EXISTING UTILITIES.

STANDARD LEGEND AND NOTES

- PROPERTY &/or BOUNDARY LINES
- CONGRESSIONAL SECTION LINES
- RIGHT-OF-WAY LINES
- EXISTING RIGHT-OF-WAY LINES
- CENTER LINES
- EXISTING CENTER LINES
- LOT LINES, INTERNAL
- LOT LINES, PLATTED OR BY DEED
- PROPOSED EASEMENT LINES
- EXISTING EASEMENT LINES
- BENCHMARK
- RECORDED DIMENSIONS
- CURVE SEGMENT NUMBER

EXISTING (R)
22-1

- POWER POLE
- POWER POLE W/DROP
- POWER POLE W/TRANS
- POWER POLE W/LIGHT
- GUY POLE
- LIGHT POLE
- SANITARY MANHOLE
- FIRE HYDRANT
- WATER VALVE
- DRAINAGE MANHOLE
- CURB INLET
- FENCE LINE
- EXISTING SANITARY SEWER
- PROPOSED SANITARY SEWER
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- WATER LINES
- ELECTRICAL LINES
- TELEPHONE LINES
- GAS LINES
- FIBER OPTIC
- OVERHEAD ELECTRIC
- CONTOUR LINES (INTERVAL)
- PROPOSED GROUND
- EXISTING TREE LINE
- EXISTING DECIDUOUS TREE & SHRUB
- EXISTING EVERGREEN TREES & SHRUBS

THE ACTUAL SIZE AND LOCATION OF ALL PROPOSED FACILITIES SHALL BE VERIFIED WITH CONSTRUCTION DOCUMENTS, WHICH ARE TO BE PREPARED AND SUBMITTED SUBSEQUENT TO THE APPROVAL OF THIS DOCUMENT.

SHEET INDEX

- 1 SITE LAYOUT AND DIMENSION PLAN
- 2 SITE LAYOUT AND UTILITY PLAN
- 3 SITE DEMOLITION PLAN
- 4 EROSION CONTROL PLAN
- 5 GRADING PLAN
- 6 LANDSCAPE PLAN
- 7 GENERAL NOTES & DETAILS

I hereby certify that this engineering document was prepared by me or under my direct supervision and I am a duly licensed Professional Engineer under the laws of the State of Iowa.

KELLY J. BECKLER
14624
IOWA

KELLY J. BECKLER
P.E. Iowa Lic. No. 14624
My license expires on December 31, 20__

Project sheets covered by this seal:

GENCOM ADDITION, LOT 1
CEDAR FALLS, IOWA

PROJECT LOCATION

LOCATION MAP
NOT TO SCALE

CIVIL ENGINEERS
LAND PLANNERS
LAND SURVEYORS
LANDSCAPE ARCHITECTS
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SITE LAYOUT AND DIMENSION PLAN

GENCOM ADDITION, LOT 1
TIDAL WAVE
AUTO SPA
CEDAR FALLS
BLACK HAWK COUNTY
IOWA

MMS CONSULTANTS, INC.

Date: 08/14/20

Designed By: NPB Field Book No:

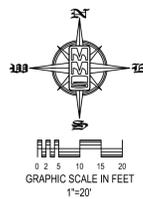
Drawn By: NPB Scale: 1"=20'

Checked By: KJB Sheet No: 1

Project No: IOWA CITY 10927-002

of 16

SITE PLAN GENCOM ADDITION, LOT 1 CEDAR FALLS, IOWA

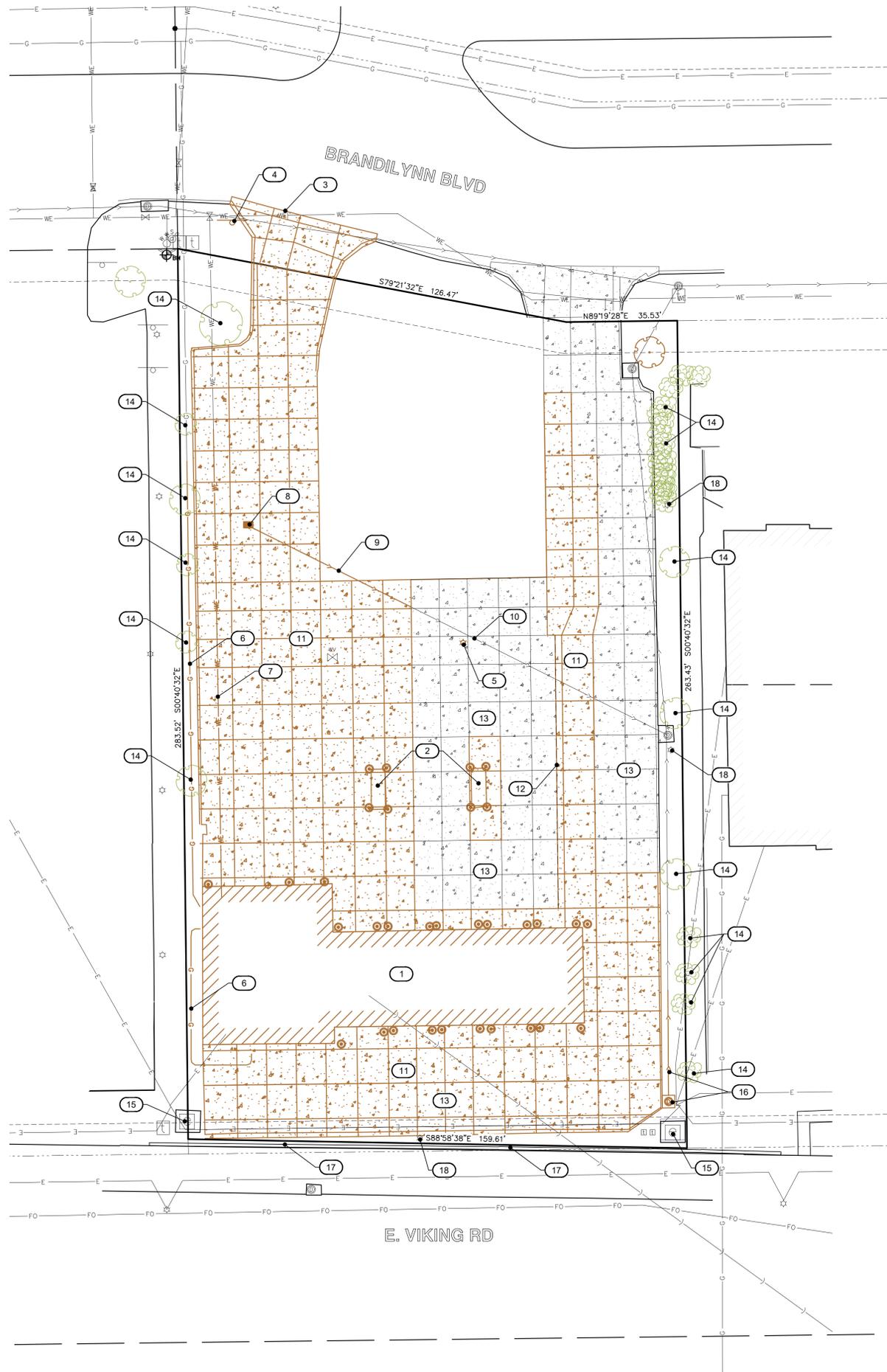


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- KEYNOTES:**
- 1 REMOVE EXISTING BUILDING AND RELATED MATERIALS
 - 2 REMOVE EXISTING FEATURE AND RELATED MATERIALS
 - 3 SAWCUT & REMOVE CURB DEPRESSION; INSTALL 6" VERTICAL CURB SECTION.
 - 4 REMOVE EXISTING SIGN
 - 5 REMOVE EXISTING LIGHT AND POLE
 - 6 REMOVE EXISTING GAS SERVICE LINE (COORDINATE WITH UTILITY PROVIDER)
 - 7 REMOVE EXISTING WATER SERVICE LINE
 - 8 REMOVE EXISTING STORM INTAKE
 - 9 REMOVE 58 LF EXISTING STORM SEWER
 - 10 ABANDON IN PLACE 52 LF EXISTING STORM SEWER
 - 11 REMOVE EXISTING CONCRETE PAVEMENT TO NEAREST JOINT
 - 12 SAWCUT AND REMOVE EXISTING CONCRETE PAVEMENT
 - 13 PROTECT EXISTING CONCRETE PAVEMENT
 - 14 PROTECT EXISTING LANDSCAPING (TYP.)
 - 15 PROTECT EXISTING TRANSFORMER AND UNDERGROUND ELECTRIC
 - 16 REMOVE EXISTING STORM INTAKE AND 34 LF EXISTING STORM SEWER
 - 17 PROTECT EXISTING SIDEWALK AND RETAINING WALL ADJACENT TO E. VIKING RD.
 - 18 PROTECT EXISTING LIGHT AND POLE.
 - 19 REMOVE EXISTING LANDSCAPING

- PAVING REMOVAL LEGEND**
- REMOVE EXISTING CONCRETE PAVEMENT (20,100 SF)
 - EXISTING CONCRETE PAVEMENT TO REMAIN (8,895 SF)
 - REMOVE EXISTING BUILDING

- DEMOLITION NOTES:**
- 1) COORDINATE ALL DEMOLITION ACTIVITIES WITH OWNER BEFORE COMMENCING ANY DEMOLITION WORK.
 - 2) THE CONTRACTOR SHALL CONTACT ALL UTILITIES AND ARRANGE FOR THEIR LOCATION OF FACILITIES BEFORE BEGINNING DEMOLITION. THE CONTRACTOR SHALL COORDINATE WITH ALL PUBLIC UTILITIES FOR DISCONNECTION AND REMOVAL OF THEIR FACILITIES SERVING STRUCTURE BEING DEMOLISHED.
 - 3) CLEARING AND REMOVAL OF TREES, BUSHES AND BRUSH SHALL INCLUDE REMOVAL FROM THE SITE OF ALL TRUNKS, BRANCHES, STUMPS AND ROOTS. THE CONTRACTOR SHALL REMOVE ALL STUMPS AND ROOTS.
 - 4) REMOVAL OF BUILDINGS INCLUDES REMOVAL AND DISPOSAL OF ALL BUILDING MATERIALS, FOUNDATIONS, SIDEWALKS AND BELOW SLAB PIPING AND CONDUITS FOR THE BUILDINGS INDICATED ON THIS SHEET.
 - 5) ALL EXCAVATIONS FOR REMOVAL OF FOUNDATIONS, UTILITIES AND TREES SHALL BE BACKFILLED WITH SUITABLE ON SITE SOIL AND COMPACTED TO 95% STANDARD PROCTOR DENSITY.
 - 6) WHERE WORK CAUSES REMOVAL OF PAVEMENT WITHIN EXISTING RIGHTS-OF-WAY, PAVEMENT SHALL BE REPLACED AS SOON AS PRACTICAL.
 - 7) REMOVAL OF FENCES INCLUDES REMOVAL AND DISPOSAL OF ALL FENCE MATERIALS FROM THE PROJECT SITE.
 - 8) ADJACENT STREETS ARE TO BE MAINTAINED CLEAN AS PER CITY OF CEDAR FALLS REQUIREMENTS.
 - 9) WHEN PEDESTRIAN TRAFFIC FLOW IS INTERRUPTED, CONTRACTOR SHALL INSTALL PROPER SIGNAGE ("SIDEWALK CLOSED AHEAD USE OTHER SIDE").
 - 10) THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL CONFORMING TO THE CURRENT EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" WHEN WORK EXTENDS INTO THE PUBLIC STREET.

STANDARD LEGEND AND NOTES

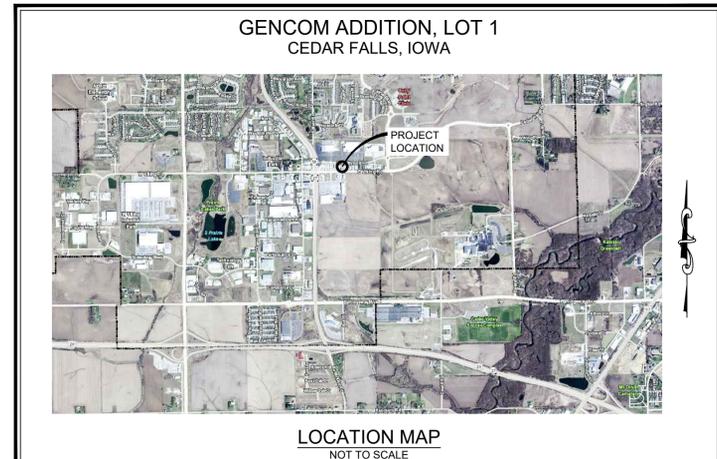
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- POWER POLE W/LIGHT
- GUY POLE
- LIGHT POLE
- FIBER OPTIC BOX
- GAS METER
- CABLE TELEVISION BOX
- TELEPHONE MANHOLE
- SANITARY MANHOLE
- SANITARY CLEANOUT
- FIRE HYDRANT
- WATER VALVE
- WATER MONITOR WELL
- DRAINAGE MANHOLE
- CURB INLET
- BOLLARD
- 1.5' SQUARE BOLLARD WITH ELECTRICAL RECEPTICAL
- FENCE LINE
- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- WATER LINES
- ELECTRICAL LINES
- OVERHEAD ELECTRICAL LINES
- FIBER OPTIC LINES
- TELEPHONE LINES
- GAS LINES
- CONTOUR LINES (1' INTERVAL)
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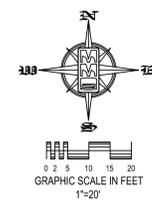
SITE DEMOLITION PLAN

GENCOM ADDITION, LOT 1
TIDAL WAVE
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CEDAR FALLS
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Project No: IOWA CITY 10927-002 of 18

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SITE PLAN GENCOM ADDITION, LOT 1 CEDAR FALLS, IOWA



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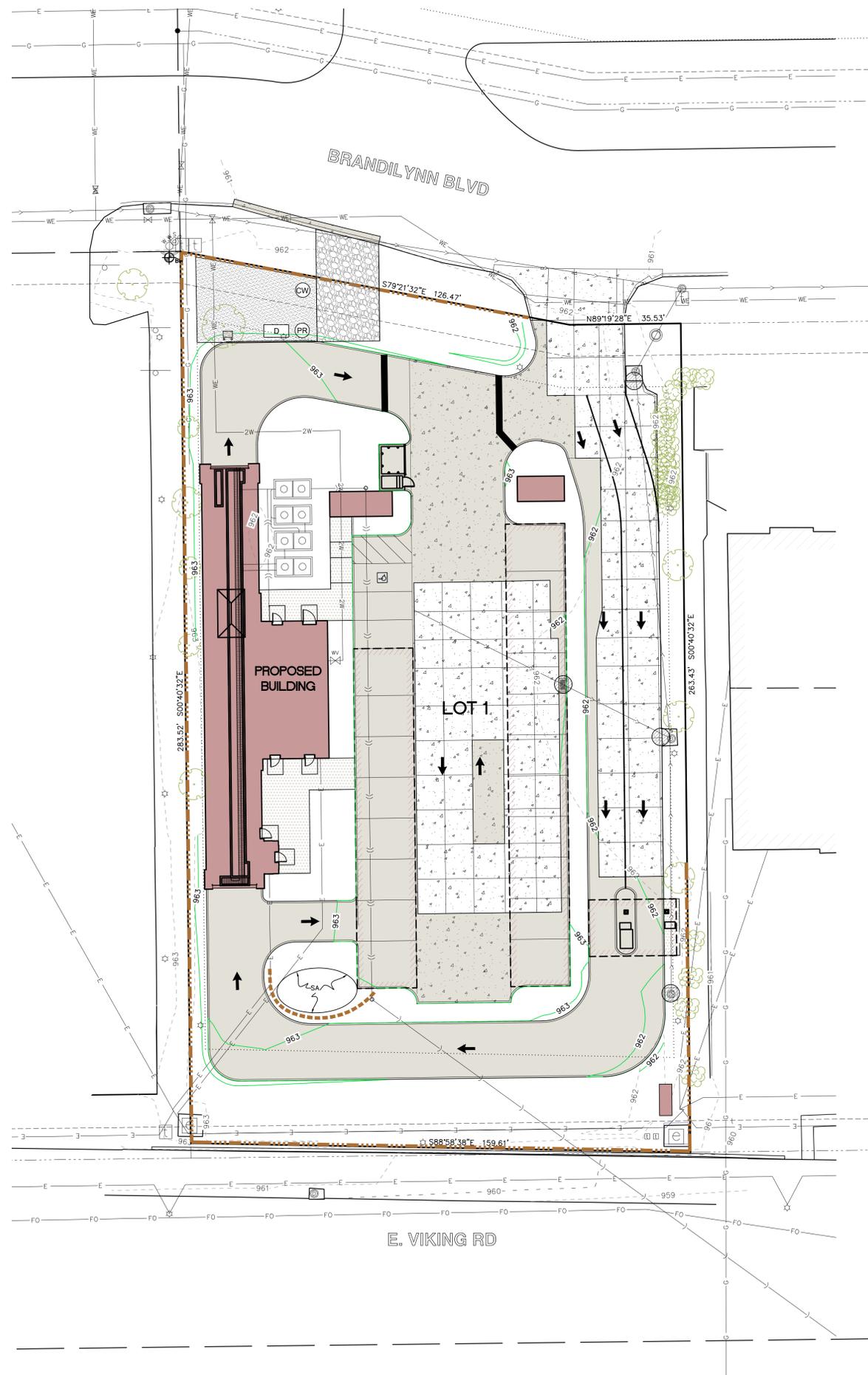
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SILT FENCE DETAIL

N.T.S.

INSTALLATION

- POSTS SHALL BE 1.33 POUNDS PER LINEAL FOOT STEEL WITH A MINIMUM LENGTH OF 5 FEET. STEEL POSTS SHALL HAVE PROJECTIONS FOR FASTENING WIRE TO THEM.
- SILT FENCE FABRIC SHALL CONFORM TO I.D.O.T. STANDARD SPECIFICATION SECTION 4196.01.A. SILT FENCING SHALL BE A MINIMUM OF 24" AND A MAXIMUM OF 36" HIGH WHEN COMPLETE.
- THE FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL CUT TO THE LENGTH OF THE FENCE TO AVOID THE USE OF JOINTS. WHEN JOINTS ARE NECESSARY, THE FILTER CLOTH SHALL BE SPLICED TOGETHER ONLY AT A SUPPORT POST, WITH A MINIMUM 6" OVERLAP, AND SECURELY SEALED.
- POSTS SHALL BE SPACED A MAXIMUM OF 8 FEET APART AND DRIVEN SECURELY INTO THE GROUND ALONG THE FENCE ALIGNMENT. POSTS SHALL BE DRIVEN INTO THE GROUND A MINIMUM OF 28".
- A TRENCH SHALL BE EXCAVATED APPROXIMATELY 4" WIDE BY 12" DEEP ALONG THE UPSLOPE SIDE OF THE POSTS.
- FILTER FABRIC SHALL BE STAPLED OR WIRED TO THE POSTS SUCH THAT THE FABRIC EXTENDS INTO THE TRENCH AS SHOWN ABOVE. THE FABRIC SHALL BE FASTENED A MINIMUM OF THREE PLACES ON EACH POST. THE TRENCH SHALL BE BACK FILLED WITH EXCAVATED MATERIAL AND THOROUGHLY COMPACTED.

MAINTENANCE

- SILT FENCES SHALL BE INSPECTED WEEKLY AND AFTER EACH RAINFALL EVENT OF 0.5 INCHES OR MORE. DURING PERIODS OF PROLONGED RAIN INSPECTIONS SHALL BE AT LEAST DAILY. ANY REPAIRS NEEDED TO MAINTAIN THE SILT FENCE'S EFFECTIVENESS SHALL BE MADE IMMEDIATELY.
- SHOULD THE FABRIC ON A SILT FENCE DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO STABILIZING THE UPSLOPE AREAS THE FABRIC SHALL BE REPLACED PROMPTLY.
- SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN THE DEPOSITS REACH APPROXIMATELY ONE-HALF THE HEIGHT OF THE FENCE. SILTS REMOVED SHALL BE PLACED IN A PROTECTED PLACE THAT WILL PREVENT THEIR ESCAPE FROM THE CONSTRUCTION SITE.
- ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE IS NO LONGER NEEDED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND SEED.
- SILT FENCE SHALL REMAIN IN PLACE UNTIL IT IS NO LONGER NEEDED AS DIRECTED BY THE POLLUTION PREVENTION PLAN. GENERALLY SILT FENCES SHALL REMAIN UNTIL THE UPSLOPE AREAS ARE STABILIZED WITH AN ESTABLISHED GRASS COVER AS A MINIMUM.

EROSION CONTROL LEGEND

<ul style="list-style-type: none"> FINAL FILTER SOCK SILT FENCE TEMPORARY ROCK CONSTRUCTION ENTRANCE/EXIT TEMPORARY PARKING AND STORAGE CONCRETE TRUCK/EQUIPMENT WASHOUT PORTABLE RESTROOM DOCUMENT LOCATION (PERMITS, SWPPP, INSPECTION FORMS, ETC.) FILTER SOCK INLET PROTECTION FILTER SOCK BEHIND CURB AT CURB RAMP 	<ul style="list-style-type: none"> PERIMETER SILT FENCE TEMPORARY SOIL STOCKPILE AREA DIRECTION OF OVERLAND FLOW DUMPSTER FOR CONSTRUCTION WASTE RIP RAP OUTLET PROTECTION
---	---

THE ABOVE LISTED ITEMS ARE SHOWN IN THEIR RECOMMENDED LOCATIONS. IF A CONTROL MEASURE IS ADDED OR MOVED TO A MORE SUITABLE LOCATION, INDICATE THE REVISION ON THIS SHEET. THE BLANKS LEFT FOR OTHER MEASURES SHOULD BE USED IF AN ITEM NOT SHOWN ABOVE IS IMPLEMENTED ON SITE. ADDITIONAL PRACTICES FOR EROSION PREVENTION AND SEDIMENT CONTROL CAN BE FOUND IN APPENDIX D OF THE SWPPP.

STANDARD LEGEND AND NOTES

<ul style="list-style-type: none"> PROPERTY &/OR BOUNDARY LINES CONGRESSIONAL SECTION LINES RIGHT-OF-WAY LINES EXISTING RIGHT-OF-WAY LINES CENTER LINES EXISTING CENTER LINES LOT LINES, INTERNAL LOT LINES, PLATTED OR BY DEED PROPOSED EASEMENT LINES EXISTING EASEMENT LINES BENCHMARK RECORDED DIMENSIONS CURVE SEGMENT NUMBER 	<ul style="list-style-type: none"> POWER POLE POWER POLE W/DROP POWER POLE W/TRANS POWER POLE W/LIGHT GUY POLE LIGHT POLE SANITARY MANHOLE FIRE HYDRANT WATER VALVE DRAINAGE MANHOLE CURB INLET FENCE LINE EXISTING SANITARY SEWER PROPOSED SANITARY SEWER EXISTING STORM SEWER PROPOSED STORM SEWER WATER LINES ELECTRICAL LINES TELEPHONE LINES GAS LINES FIBER OPTIC OVERHEAD ELECTRIC CONTOUR LINES (INTERVAL) PROPOSED GROUND EXISTING TREE LINE EXISTING DECIDUOUS TREE & SHRUB EXISTING EVERGREEN TREES & SHRUBS
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GRADING AND EROSION CONTROL NOTES

TOTAL SITE AREA: 0.99 - ACRES
TOTAL AREA TO BE DISTURBED: 0.75 ACRES

EROSION CONTROL MEASURES SHOWN SHALL BE USED DURING FILL ACTIVITIES. EROSION CONTROL MEASURES SHALL BE REEVALUATED AND MODIFIED, IF NECESSARY, AT THE TIME OF SITE DEVELOPMENT.

ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES THAT COULD BE USED ON SITE, IF NEEDED, CAN BE FOUND IN APPENDIX D OF THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) ENGINEER PREPARED FOR THE SITE. IF ADDITIONAL MEASURES ARE USED, INDICATE THE TYPE AND LOCATION OF SAID MEASURE ON THIS PLAN.

CONTRACTOR SHALL INSTALL A ROCK ENTRANCE AND PERFORM REGULAR CLEANING OF VEHICLES THAT LEAVE THE SITE.

FOLLOWING INSTALLATION OF PERIMETER SILT FENCE AND TEMPORARY CONSTRUCTION ENTRANCE THE CONTRACTOR SHALL CONTACT THE CITY INSPECTOR TO SCHEDULE A SITE INSPECTION PRIOR TO ANY SOIL DISTURBING ACTIVITIES.

THE CONTRACTOR SHALL FOLLOW THE NPDES PERMIT, SWPPP, AND THE CITY CSR REGULATIONS WHENEVER APPLICABLE.

THE EROSION CONTROL CONTRACTOR SHALL INSTALL FILTER SOCKS OR OTHER APPROVED FORM OF INLET PROTECTION AT EACH STREET INTAKE ADJACENT TO THE SITE.

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GENCOM ADDITION, LOT 1 CEDAR FALLS, IOWA

LOCATION MAP
NOT TO SCALE

EROSION CONTROL PLAN

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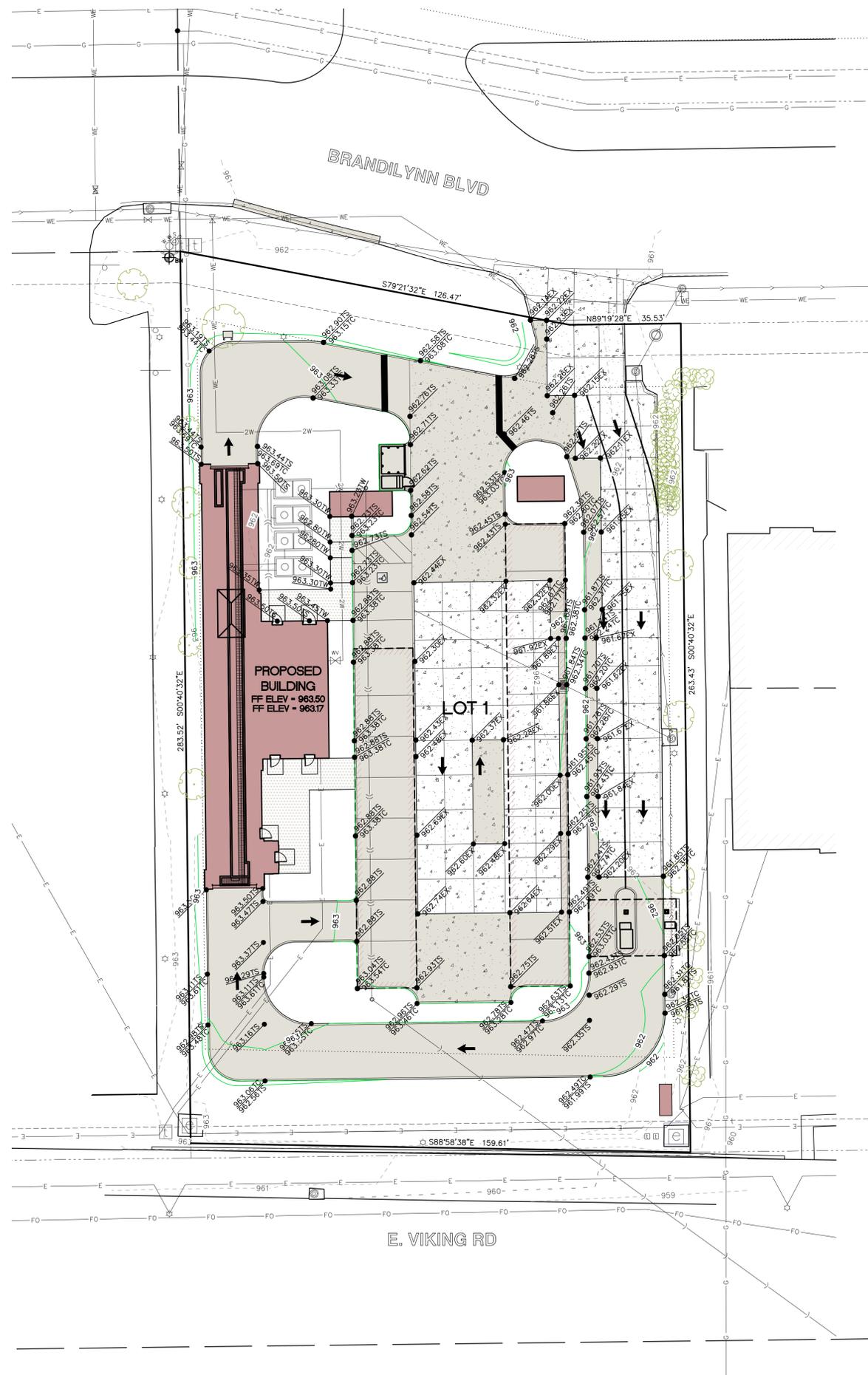
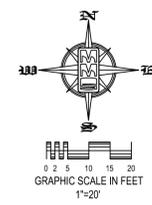
SITE PLAN GENCOM ADDITION, LOT 1 CEDAR FALLS, IOWA

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GRADING LEGEND

- 000.00EX - EXISTING SURFACE
- 000.00TS - TOP SLAB
- 000.00TW - TOP WALK
- 000.00TC - TOP CURB
- 000.00TR - TOP RIM
- 000.00TOP - TOP OF WALL
- 000.00BOT - BOTTOM OF WALL
- 000.00FG - FINISHED GRADE

GRADING NOTES

- MAXIMUM SLOPE ON CUTS AND FILLS SHALL BE 3:5. HORIZONTAL TO 1: VERTICAL.
- NO EXCAVATION SHALL BE ALLOWED WITHIN 2' OF PROPERTY LINES.
- WHERE HEIGHT OF FILL IS GREATER THAN 30' AN INTERMEDIATE TERRACE OF AT LEAST 6' WIDE SHALL BE ESTABLISHED AT MID HEIGHT. SEE TYPICAL FILL SECTION.
- COMPACTION TO BE 90% MODIFIED PROCTOR WHERE > 6:1 SLOPE.
- ALL TREES OUTSIDE THE LIMITS OF GRADING OPERATIONS SHALL BE SAVED, UNLESS OTHERWISE INDICATED TO BE REMOVED. TREES NEAR THE EDGES OF GRADING LIMITS AND IN THE STORM WATER DETENTION BASIN AREAS SHALL BE SAVED IF POSSIBLE, WITHIN THE REQUIREMENTS OF THE SPECIFICATIONS.
- PRIOR TO ANY GRADING A CONSTRUCTION SAFETY FENCE SHALL BE INSTALLED AROUND TRUNKS OF TREES TO BE PROTECTED.
- STABILIZATION SEEDING SHALL BE COMPLETED AS SOON AS POSSIBLE, BUT NOT MORE THAN 14 DAYS, UPON COMPLETION OF GRADING IN ANY AREA OF GRADING OPERATIONS. DISTURBED AREAS SHALL BE KEPT AS SMALL AS POSSIBLE TO PREVENT LARGE SCALE EROSION PROBLEMS. IF THE GRADING CONTRACTOR STOPS GRADING OPERATIONS FOR MORE THAN 14 DAYS, THEN STABILIZATION SEEDING SHALL BE DONE ON ALL DISTURBED AREAS.
- SILT FENCE LOCATIONS AND LENGTHS, AS INDICATED, ARE APPROXIMATE ONLY. FINAL LOCATIONS AND LENGTHS WILL BE DETERMINED, AS NEEDED, UPON COMPLETION OF GRADING OPERATIONS IN AN AREA.
- ALL STREET SUBGRADES SHALL BE CONSTRUCTED AND COMPACTED IN ACCORDANCE WITH CITY OF CEDAR FALLS DESIGN AND CONSTRUCTION STANDARDS AND PROCEDURES.

NOTES:

- ALL ELEVATIONS ARE TOP OF SLAB UNLESS NOTED OTHERWISE.
- TEMPORARY STABILIZATION IS REQUIRED ON DISTURBED AREAS AFTER THE 14TH DAY OF INACTIVITY.
- FINAL STABILIZATION SHALL BE IMPLEMENTED WITHIN 14 DAYS OF FINAL GRADING COMPLETION.

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- - - - - EXISTING EASEMENT LINES
- - - - - BENCHMARK
- - - - - RECORDED DIMENSIONS
- - - - - CURVE SEGMENT NUMBER

EXISTING (EX) / PROPOSED (PROP)

- POWER POLE
- POWER POLE W/DROP
- POWER POLE W/TRANS
- POWER POLE W/LIGHT
- GUY POLE
- LIGHT POLE
- SANITARY MANHOLE
- FIRE HYDRANT
- WATER VALVE
- DRAINAGE MANHOLE
- CURB INLET
- FENCE LINE
- EXISTING SANITARY SEWER
- PROPOSED SANITARY SEWER
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- WATER LINES
- ELECTRICAL LINES
- TELEPHONE LINES
- GAS LINES
- FIBER OPTIC
- OVERHEAD ELECTRIC
- CONTOUR LINES (INTERVAL)
- PROPOSED GROUND
- EXISTING TREE LINE
- EXISTING TREE LINE
- EXISTING DECIDUOUS TREE & SHRUB
- EXISTING EVERGREEN TREES & SHRUBS

THE ACTUAL SIZE AND LOCATION OF ALL PROPOSED FACILITIES SHALL BE VERIFIED WITH CONSTRUCTION DOCUMENTS, WHICH ARE TO BE PREPARED AND SUBMITTED SUBSEQUENT TO THE APPROVAL OF THIS DOCUMENT.

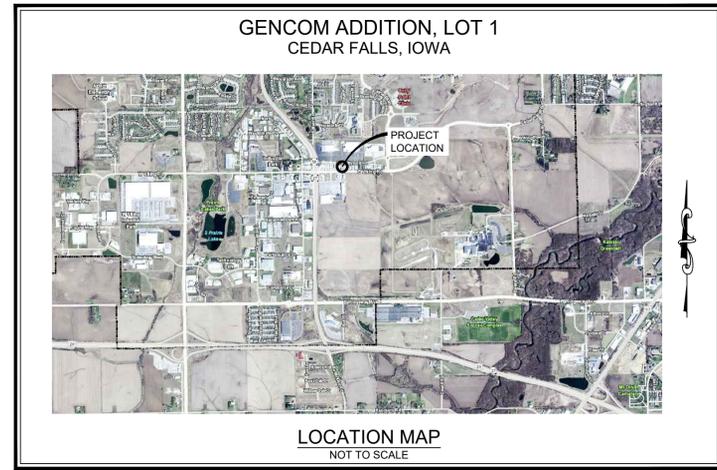
SHEET INDEX

- SITE LAYOUT AND DIMENSION PLAN
- SITE LAYOUT AND UTILITY PLAN
- SITE DEMOLITION PLAN
- EROSION CONTROL PLAN
- GRADING PLAN**
- LANDSCAPE PLAN
- GENERAL NOTES & DETAILS

GRADING PLAN

THE CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL PER IDOT STANDARD ROAD PLAN TC-419 OR SUDAS 8030-110 AND CITY OF CEDAR FALLS REQUIREMENTS AT ALL TIMES DURING WORK WITHIN PUBLIC R.O.W.

THE CONTRACTOR SHALL COORDINATE WITH UTILITY PROVIDERS FOR ANY REQUIRED RELOCATION OF EXISTING UTILITIES.



**GENCOM ADDITION, LOT 1
TIDAL WAVE
AUTO SPA
CEDAR FALLS
BLACK HAWK COUNTY
IOWA**

MMS CONSULTANTS, INC.

Date: 08/14/20

Designed By: NPB Field Book No:

Drawn By: NPB Scale: 1"=20'

Checked By: KJB Sheet No: 5

Project No: IOWA CITY 10927-002 of 20



CIVIL ENGINEERS
LAND PLANNERS
LAND SURVEYORS
LANDSCAPE ARCHITECTS
ENVIRONMENTAL SPECIALISTS

1917 S. GILBERT ST.
IOWA CITY, IOWA 52240
(319) 351-8282
www.mmsconsultants.net

Date	Revision
08/21/20	PER CITY COMMENTS - NPB
10/16/20	PER CITY COMMENTS - NPB
10/20/20	PER CITY COMMENTS - NPB
10/27/20	PER CITY COMMENTS - NPB

SANITARY SEWER AND WATERMAIN NOTES

- SANITARY SEWER & WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE IOWA STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS) MANUAL, 2017 EDITION.
- SANITARY SEWERS SHALL BE PVC TRUSS PIPE (SUDAS 4010.2.01.E), AS NOTED ON THE PLANS. SANITARY SEWER SERVICE LINES SHALL BE PVC SDR-23.5 WITH GASKETED JOINTS.
- WATER MAINS SHALL BE PVC DR-18 PIPE.
- CONTRACTOR TO PROVIDE FERNOCO "BAND-SEAL" COUPLINGS FOR DISSIMILAR PIPE CONNECTIONS.
- GRANULAR TRENCH BACKFILL SHALL BE CRUSHED STONE CONFORMING TO I.D.O.T. STANDARD SPECIFICATION 4120.04 WITH 1" MAXIMUM AGGREGATE SIZE. COMPACT TO 90% MODIFIED PROCTOR DENSITY.
- ALL SANITARY SEWERS SHOWN ON THE PROFILE VIEW AND ALL SANITARY SEWERS AND WATER MAINS UNDER PAVING OR WITHIN 5 FEET OF PAVING SHALL BE BACKFILLED WITH EITHER OF THE FOLLOWING COMPACTED TO 90% MODIFIED PROCTOR DENSITY:
 - SUITABLE EXCAVATED MATERIAL, IF EXCAVATED MATERIAL IS NOT SUITABLE, THEN
 - CRUSHED STONE AS SPECIFIED FOR GRANULAR TRENCH BACKFILL.
- ALL SANITARY SEWER SERVICE LINES CROSSING STREET RIGHT-OF-WAY SHALL BE BACKFILLED IN ACCORDANCE WITH THE PRECEDING NOTE.
- CONTRACTOR SHALL PROVIDE SUDAS CLASS F-3 BEDDING FOR ALL PVC TRUSS SANITARY SEWERS UNLESS OTHERWISE NOTED.

ALL SANITARY SEWER SERVICE LINES SHALL BE EXTENDED :

 - TO THE UTILITY EASEMENT LINE FOR THOSE LOCATIONS WHERE THE LOTS BEING SERVED ARE ON THE OPPOSITE SIDE OF THE STREET FROM THE SEWER MAIN.
 - TO THE UTILITY EASEMENT LINE FOR THOSE LOCATIONS WHERE THE LOTS BEING SERVED ARE ADJACENT TO THE SEWER MAIN.
- THE END OF ALL SANITARY SEWER SERVICES MUST BE MARKED WITH A WOOD 2 x 4 PAINTED GREEN.
- ALL SANITARY SEWER MANHOLES IN PAVING AREAS OR AREAS SUBJECT TO WATER INUNDATION SHALL BE PROVIDED WITH CHECK EXTERIOR CHIMNEY SEAL OR APPROVED EQUIV. ALL SANITARY MANHOLES IN PAVING SHALL HAVE 3-PIECE FLOATING CASTING

AIR TESTING

- ALL SANITARY SEWER AND SERVICE LINES SHALL BE AIR TESTED TO PASS THE FOLLOWING TEST:

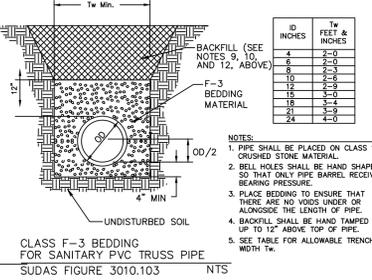
NOTE: CITY REPRESENTATIVE MUST BE PRESENT DURING TESTING.

 - PERFORM FROM MANHOLE-TO-MANHOLE AFTER BACKFILL.
 - PLACE PNEUMATIC PUGS: (1) SEALING LENGTH: EQUAL TO OR GREATER THAN PIPE DIAMETER, (2) CAPABLE OF RESISTING INTERNAL TEST PRESSURE WITHOUT EXTERNAL BRACING OR BLOCKING.
 - INTRODUCE LOW-PRESSURE AIR INTO SEALED LINE AND ACHIEVE INTERNAL AIR PRESSURE OF 5 psi & MAINTAIN FOR A MINIMUM OF 5 MINUTES.
 - LIMIT INTERNAL PRESSURE IN SEALED LINE BELOW 8 PSIG.
 - ALLOW 2 MINUTES MINIMUM FOR AIR PRESSURE TO STABILIZE. DISCONNECT LOW-PRESSURE AIR HOSE FROM CONTROL PANEL.
 - MINIMUM TIME FOR PRESSURE TO DROP FROM 3.5 TO 2.5 PSIG GREATER THAN MAXIMUM PRESSURE EXERTED BY GROUNDWATER ABOVE PIPE INVERT.

PIPE DIAMETER IN INCHES	TIME IN MINUTES
4	2.0
6	3.0
8	4.0
10	5.0
12	5.5
15	7.5
- IN AREAS WHERE GROUND WATER IS KNOWN TO EXIST, THE HEIGHT OF WATER ABOVE THE TOP OF THE PIPE BEING TESTED, IN FEET, SHALL BE DETERMINED AND THAT HEIGHT DIVIDED BY 2.3 TO ESTABLISH THE PRESSURE THAT WILL BE ADDED TO ALTERNATIVELY, THE ENGINEER MAY ALLOW THE CONTRACTOR TO MEASURE INFILTRATION INTO THE SEWER BY USING A V-NOTCH WEIR OR OTHER SUITABLE DEVICE.
- LOCATE, REPAIR AND RETEST LEAKS.
- AIR TESTING SHALL BE CONSIDERED INCIDENTAL TO SANITARY SEWER CONSTRUCTION.

ALL PVC TRUSS SEWERS SHALL HAVE A DEFLECTION TEST PERFORMED AS FOLLOWS:

 - DEFLECTION TEST SHALL BE CONDUCTED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS.
 - DEFLECTION TEST TO BE CONDUCTED USING A RIGID BALL OR MANDELUM WITH A DIAMETER EQUAL TO 95% OF THE INSIDE DIAMETER OF THE PIPE. NO MECHANICAL PULLING DEVICES ALLOWED.
 - NO PIPE SHALL EXCEED A DEFLECTION OF 5%.

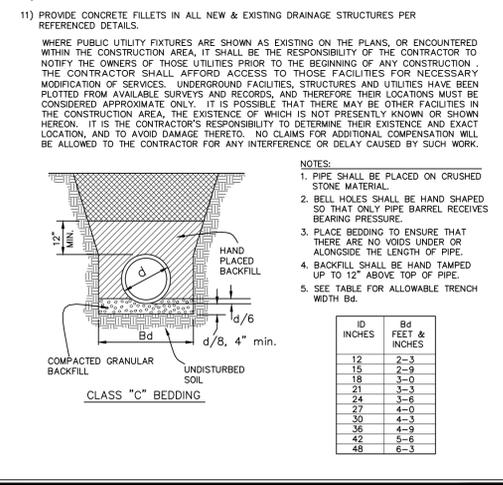


- THE FOLLOWING MINIMUM CLEARANCES MUST BE MAINTAINED :
- WATER MAIN SHALL BE LOCATED 10 FEET HORIZONTALLY DISTANT FROM ALL SANITARY SEWER AND STORM SEWER.
- WATER MAIN SHALL NOT PASS THROUGH OR CONTACT A SEWER OR A SEWER MANHOLE. A MINIMUM HORIZONTAL SEPARATION OF 3 FEET SHALL BE MAINTAINED.
- VERTICAL SEPARATION OF WATER MAINS CROSSING OVER ANY SANITARY SEWER SHOULD BE A MINIMUM OF 18-INCHES, MEASURED OUTSIDE TO OUTSIDE FROM THE CLOSEST EDGE OF EACH PIPE. IF PHYSICAL CONDITIONS PROHIBIT THIS SEPARATION, THE WATER MAIN SHALL NOT BE PLACED CLOSER THAN 6-INCHES ABOVE A SEWER OR 18-INCHES BELOW A SEWER. THE SEPARATION DISTANCE SHALL BE THE MAXIMUM FEASIBLE IN ALL CASES.
- WHERE THE WATER MAIN CROSSES SEWER, ONE FULL LENGTH OF WATER PIPE SHALL BE LOCATED SO BOTH JOINTS ARE AS FAR AS POSSIBLE FROM THE SEWER. THE WATER AND SEWER PIPES MUST BE ADEQUATELY SUPPORTED AND HAVE WATER TIGHT JOINTS. A LOW PERMEABILITY SOIL SHALL BE USED FOR BACKFILL MATERIAL WITHIN 10-FEET OF THE POINT OF CROSSING.
- NOMINAL DEPTH OF WATER MAIN = 5.5 FEET TO TOP OF PIPE.
- EXISTING OR PROPOSED VALVE BOXES THAT FALL WITHIN PAVEMENT MUST HAVE A SUP-TYPE VALVE BOX.
- THE ENTIRE WATERMAIN SYSTEM, INCLUDING SERVICES TAPS IF APPLICABLE, SHALL BE PRESSURE TESTED PER AWWA C600. THE TEST SHALL BE PERFORMED AT A MINIMUM OF 150 PSI FOR 2 HOURS WITH A MAXIMUM LOSS OF 5 PSI.
- WATER MAINS SHALL BE DISINFECTED IN ACCORDANCE WITH SPECIFICATIONS.
- FIRE HYDRANTS SHALL BE WUELLER SUPER CENTURIUM 250 OR APPROVED CITY EQUAL THAT OPEN RIGHT.
- BED WATER MAIN ON NATIVE MATERIAL, DIG IN BELLS, AND BACKFILL WITH SUITABLE MATERIAL.
- ALL CONCRETE SANITARY MANHOLES SHALL HAVE CONSHIELD ANTI-MICROBIAL ADDITIVE INCORPORATED IN THE CONCRETE MIX.

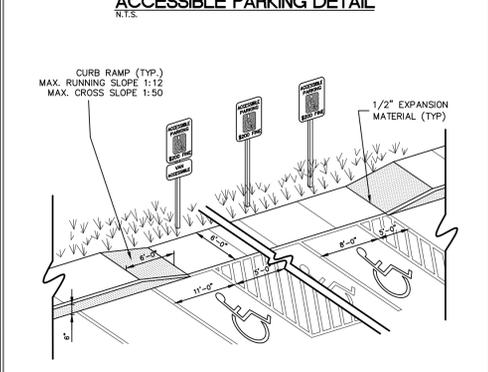
WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS, OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THOSE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THAT THERE MAY BE OTHER FACILITIES IN THE CONSTRUCTION AREA, THE EXISTENCE OF WHICH IS NOT PRESENTLY KNOWN OR SHOWN HEREOF. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATION, AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.

STORM SEWER NOTES

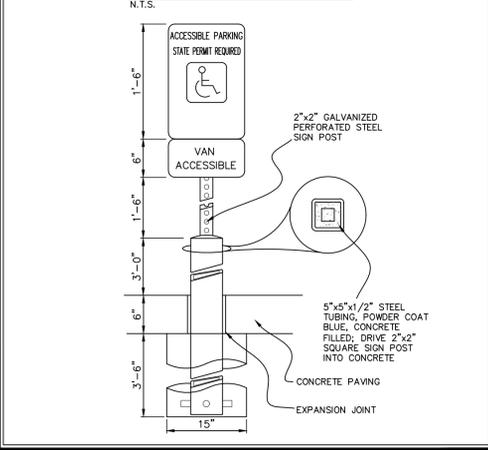
- STORM SEWER CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STANDARD DETAILED SPECIFICATIONS AND DETAILED SPECIFICATION REQUIREMENTS PREPARED FOR THIS PROJECT. CITY OF CEDAR FALLS DESIGN AND CONSTRUCTION STANDARDS AND PROCEDURES SHALL PREVAIL.
- ALL STORM SEWERS SHALL BE CLASS 3 RCP UNLESS NOTED OTHERWISE IN THE PLANS.
- AT PLACES WHERE A FLARED END SECTION IS REQUIRED, PIPE LENGTH INCLUDES THE FLARED END. THE LAST TWO JOINTS ARE TO BE TIED WHERE FLARED END SECTIONS ARE REQUIRED.
- ALL RCP STORM SEWERS SHALL BE PROVIDED WITH CLASS "C" BEDDING, UNLESS NOTED OTHERWISE. PVC SEWERS SHALL BE PROVIDED WITH CRUSHED STONE ENCASEMENT.
- STORM SEWERS SHOWN ON THE PROFILE VIEW SHALL BE BACKFILLED WITH EITHER OF THE FOLLOWING COMPACTED TO 90% MODIFIED PROCTOR DENSITY:
 - SUITABLE EXCAVATED MATERIAL, IF EXCAVATED MATERIAL IS NOT SUITABLE, THEN
 - CRUSHED STONE AS SPECIFIED FOR GRANULAR TRENCH BACKFILL SHALL BE USED.
- GRANULAR TRENCH BACKFILL SHALL BE CRUSHED STONE CONFORMING TO I.D.O.T. STANDARD SPECIFICATION 4120.04 WITH 1" MAXIMUM AGGREGATE SIZE. COMPACT TO 90% MODIFIED PROCTOR DENSITY.
- ALL STORM SEWERS SHALL HAVE CONFINED "O" RING GASKETS. STORM SEWERS 36" AND SMALLER SHALL HAVE BELL AND SPIGOT JOINTS. STORM SEWERS LARGER THAN 36" MAY HAVE TONGUE AND GROOVE JOINTS. NO MASTIC JOINTS ALLOWED.
- ALL PIPE SHALL BE CERTIFIED.
- ALL STORM INTAKES SHALL BE A MINIMUM OF 48 INCHES FROM TOP OF CURB/RIM TO SUBGRADE. IF INVERT ELEVATIONS ARE INSUFFICIENT TO PROVIDE THIS REQUIRED DEPTH, THE CONTRACTOR TO PROVIDE DEEPER STRUCTURE AND POUR CONCRETE FILLET IN INTAKE TO MAKE INTAKE PIPES DRAIN AT INVERT ELEVATIONS LISTED.
- LIFT HOLES IN STORM SEWER WILL NOT BE ALLOWED.
- PROVIDE CONCRETE FILLETS IN ALL NEW & EXISTING DRAINAGE STRUCTURES PER REFERENCE DETAILS.



ACCESSIBLE PARKING DETAIL



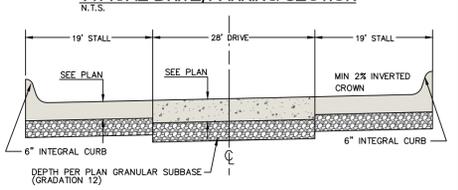
ACCESSIBLE SIGN DETAIL



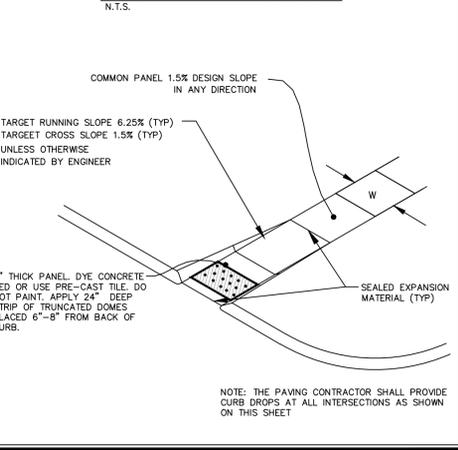
PAVING CONSTRUCTION NOTES

- PAVEMENT CONSTRUCTION SHALL BE IN ACCORDANCE WITH I.D.O.T. SPECIFICATION SECTION 2301.
- I.D.O.T. CLASS C-3 CONCRETE SHALL BE USED, UNLESS NOTED OTHERWISE.
- PAVEMENT JOINTS SHALL CONFORM TO I.D.O.T. STANDARD ROAD PLAN PV-101.
- SUBGRADE UNDER PROPOSED PAVEMENT SHALL BE COMPACTED TO 95% STANDARD PROCTOR DENSITY, TO A DEPTH OF SIX (6) INCHES, UNLESS NOTED OTHERWISE.
- ALL SAWED PAVEMENT JOINTS SHALL BE SEALED.

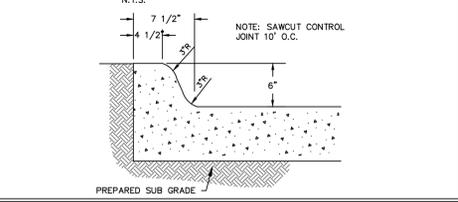
TYPICAL DRIVE/PARKING SECTION



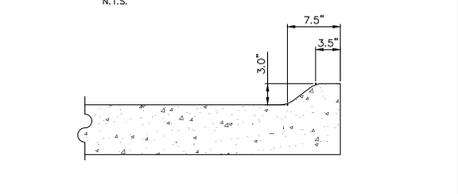
CURB DROP FOR SIDEWALK



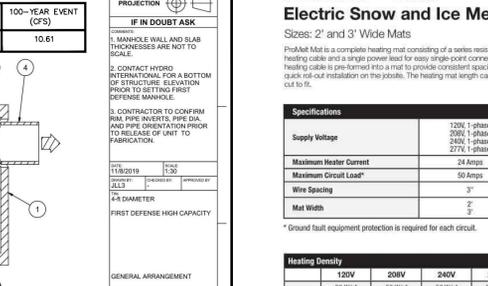
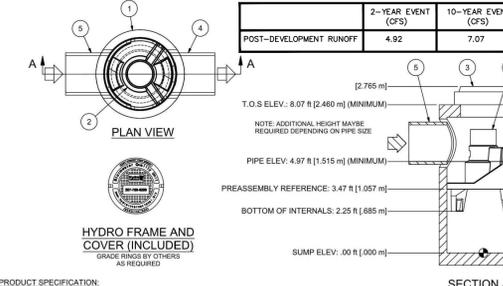
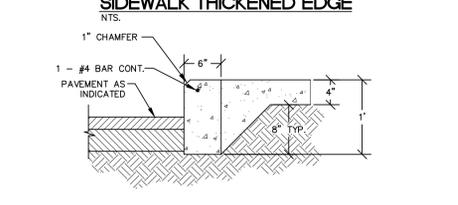
6" STANDARD CURB DETAIL



3" ROLL CURB DETAIL



SIDEWALK THICKENED EDGE



ProMelt™ Mat Electric Snow and Ice Melt Mat

ProMelt Mat is a complete heating mat consisting of a series resistance heating cable and a single point lead for easy single-point connection. The heating cable is pre-formed into a mat to provide consistent spacing and quick on-site installation on the job site. The heating mat length cannot be cut to fit.

Supply Voltage	120V 1-phase	208V 1-phase	240V 1-phase	277V 1-phase
Maximum Heater Current	24 Amps	12 Amps	10 Amps	9 Amps
Maximum Circuit Load*	50 Amps	25 Amps	20 Amps	18 Amps
Wire Spacing	3"	3"	3"	3"
Mat Width	2'	2'	2'	2'

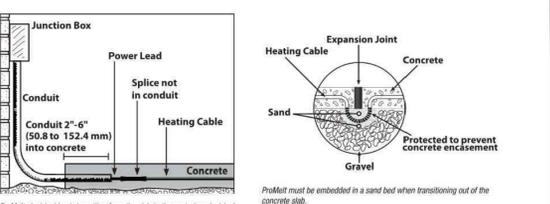
Application: ProMelt Mats are used to melt ice or snow from an exterior surface and are designed for outdoor use only, embedded in concrete, asphalt, or sand.



ProMelt Mats are available in 2' and 3' wide with various lengths with voltage options of 120, 208, 240, and 277 volts.

Installation Parameters: Determine a time to install the mat when equipment, heavy tools, and site traffic will be minimal. Apply the surfacing courses over the mat the same day the mat is installed.

WARNING: In order to avoid property damage, injury and/or death please refer to the complete installation manual and warnings provided with the product.



Model No.	Length	Amps	Ohms
SM027700204	5.0	3.7	42-13
SM027700204	10.0	4.3	20-25
SM027700204	15.0	5.6	18-17
SM027700204	20.0	7.7	7-10
SM027700204	25.0	11.8	6-8
SM027700204	30.0	19.0	4-6



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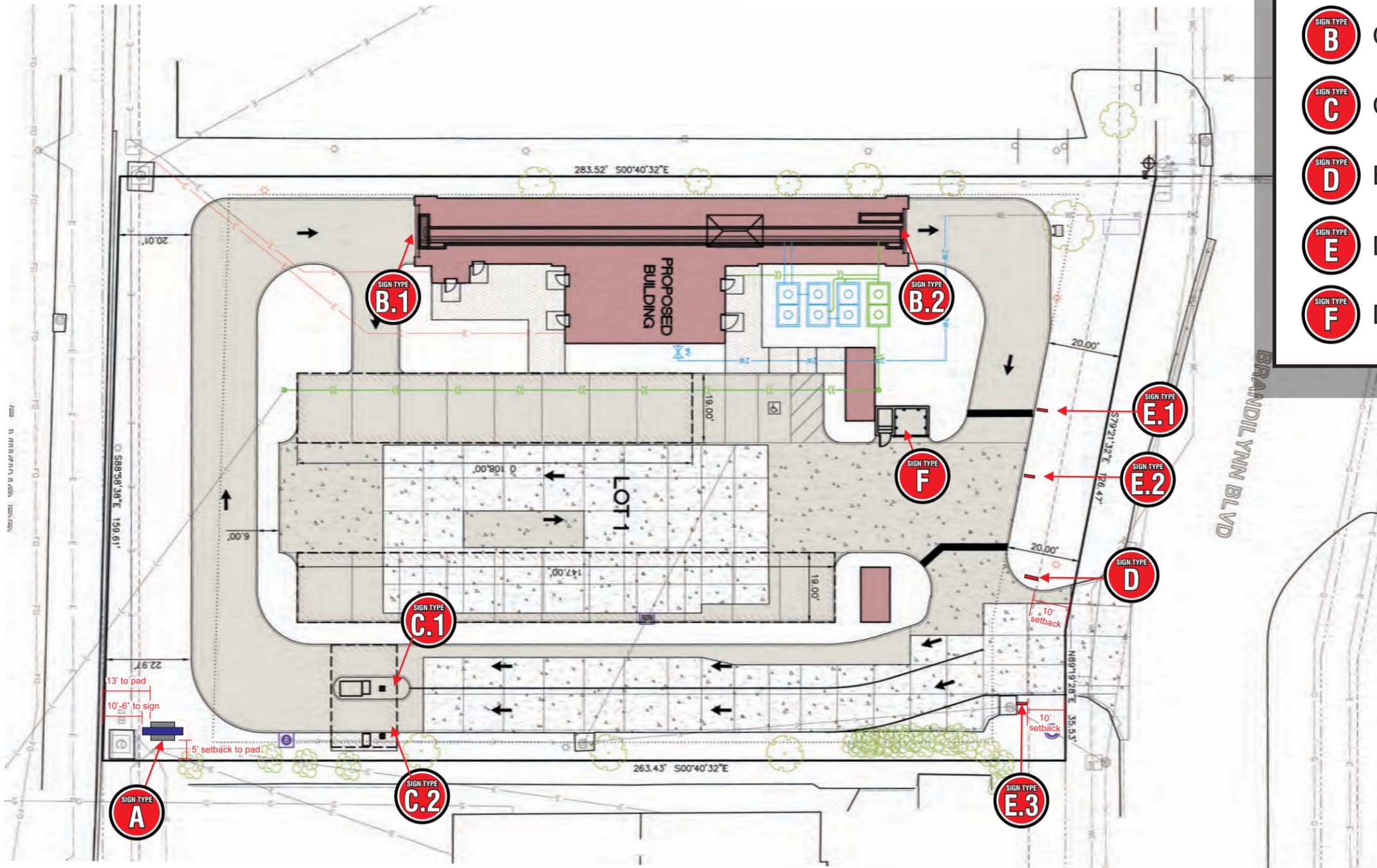
Date	Revision
09/21/20	PER CITY COMMENTS - NPB
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10/27/20	PER CITY COMMENTS - NPB

GENERAL NOTES & DETAILS

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AUTO SPA
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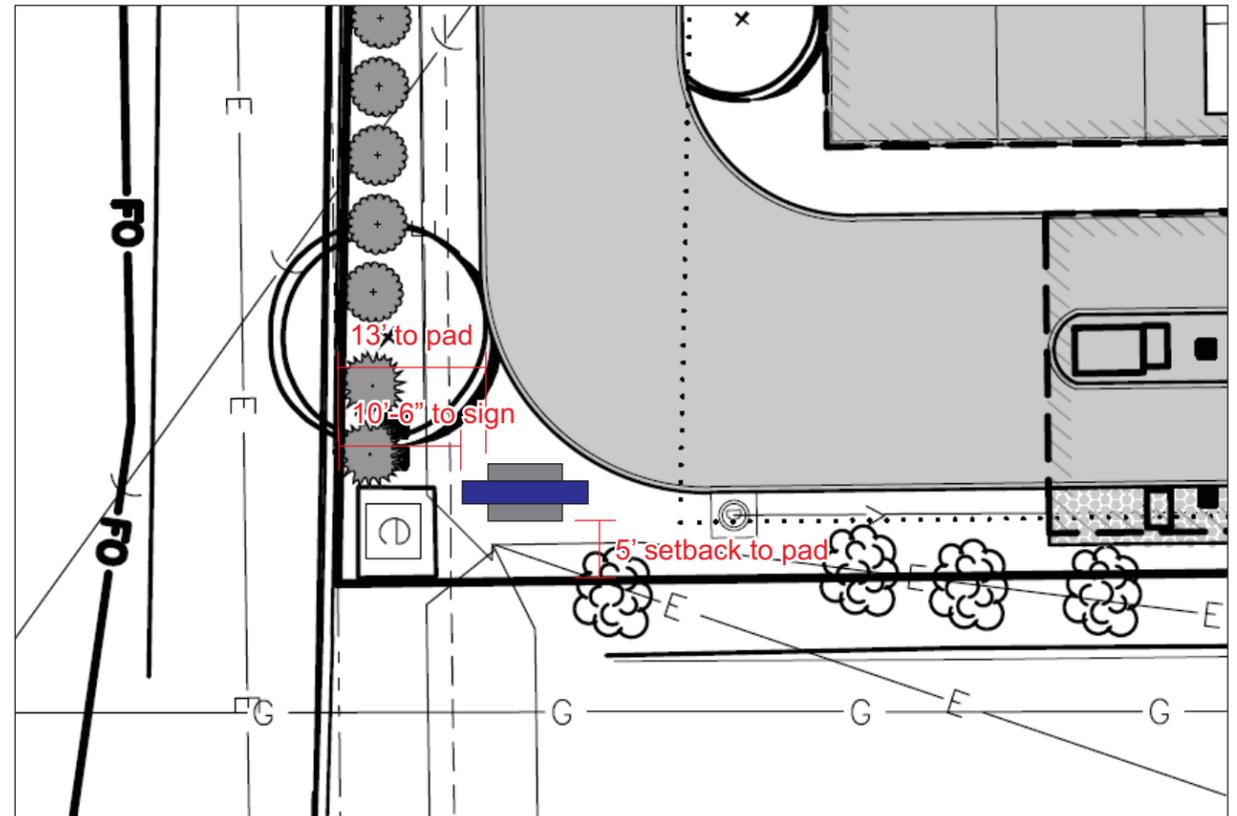
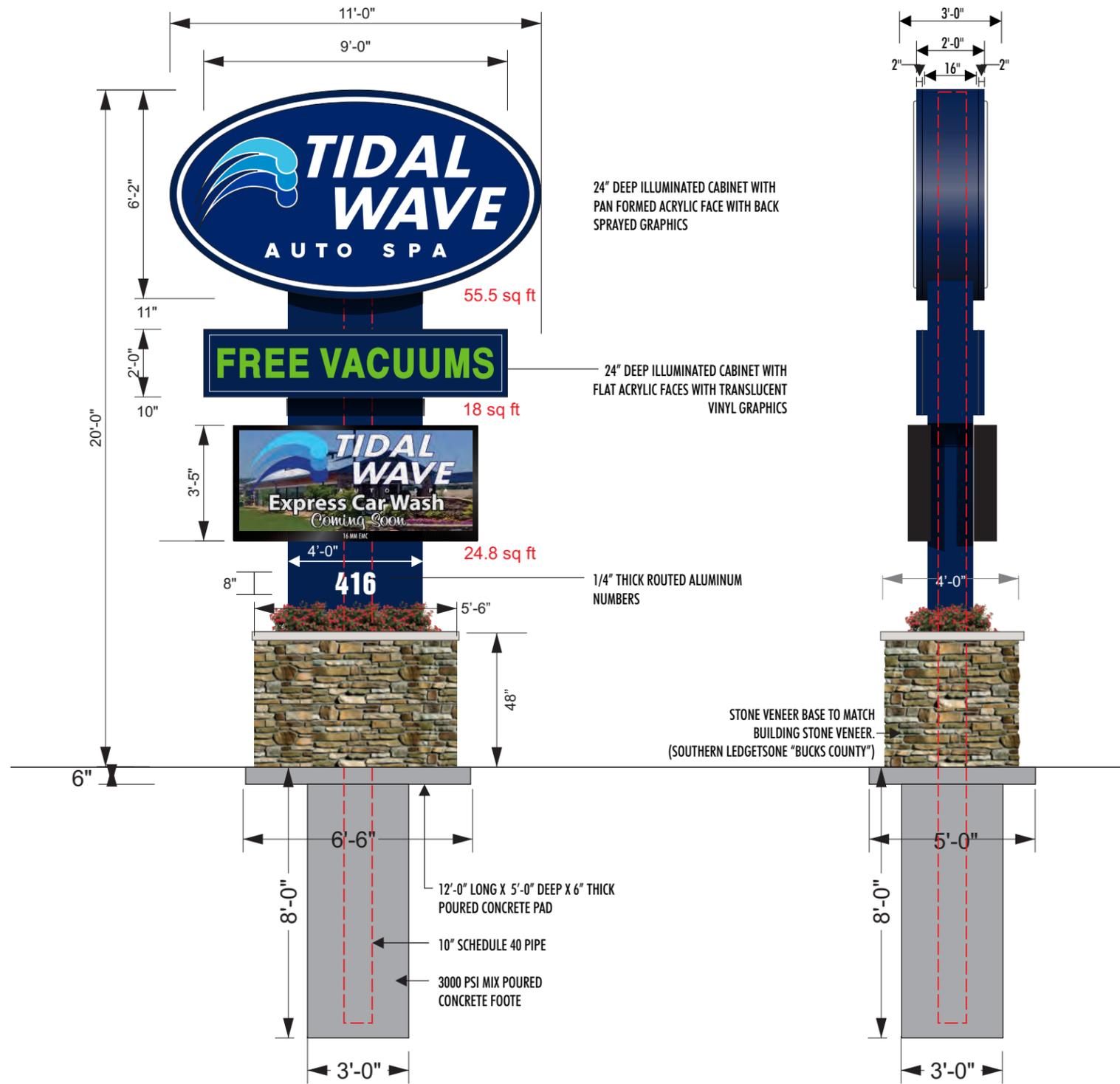
MMS CONSULTANTS, INC.
Date: 08/14/20
Designed by: NPB Field Book No:
Drawn by: NPB Scale: NTS
Checked by: KJB Sheet No:
Project No: 10927-002
IOWA CITY
of 22

- SHEET INDEX
- SITE LAYOUT AND DIMENSION PLAN
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 - SITE DEMOLITION PLAN
 - EROSION CONTROL PLAN
 - 5 GRADING PLAN
 - LANDSCAPE PLAN
 - GENERAL NOTES & DETAILS



- SIGN TYPE A** MONUMENT SIGN
- SIGN TYPE B** CHANNEL LETTERS
- SIGN TYPE C** CAR WASH MENUS
- SIGN TYPE D** ENTRANCE SIGN
- SIGN TYPE E** DIRECTIONAL SIGNS
- SIGN TYPE F** DUMPSTER DOOR PANELS

SIGN TYPE A: MONUMENT SIGN
DOUBLE FACED
QTY: 1 - 98.3 SQ. FT.



SCALE: 1/16" = 1'-0"

CLAYTON Signs
 Since 1965

5198 NORTH LAKE DRIVE
 LAKE CITY, GA 30260
 404-361-3800
 FAX 404-361-7038
 WWW.CLAYTONSIGNS.COM

DATE - 10.5.20
 SIGN TYPE
 EXTERIOR SIGN PACKAGE

CLIENT
 TIDAL WAVE
 416 Brandilynn Blvd
 Cedar Falls, IA 50613

REVISIONS:
 REVISION # - DATE

DESIGNER Ryan Mc
 ACCOUNT REP. NAME
 Todd Willis

SCALE: AS INDICATED
 DRAWING FILE NAME
 O:\RM\TIDAL WAVE\ CEDAR FALLS, IA\
 TIDAL WAVE CEDAR FALLS IA SIGN PACKAGE -
 10.5.20 .CDR

DRAWING STATUS: **NF**
 NOT FINAL

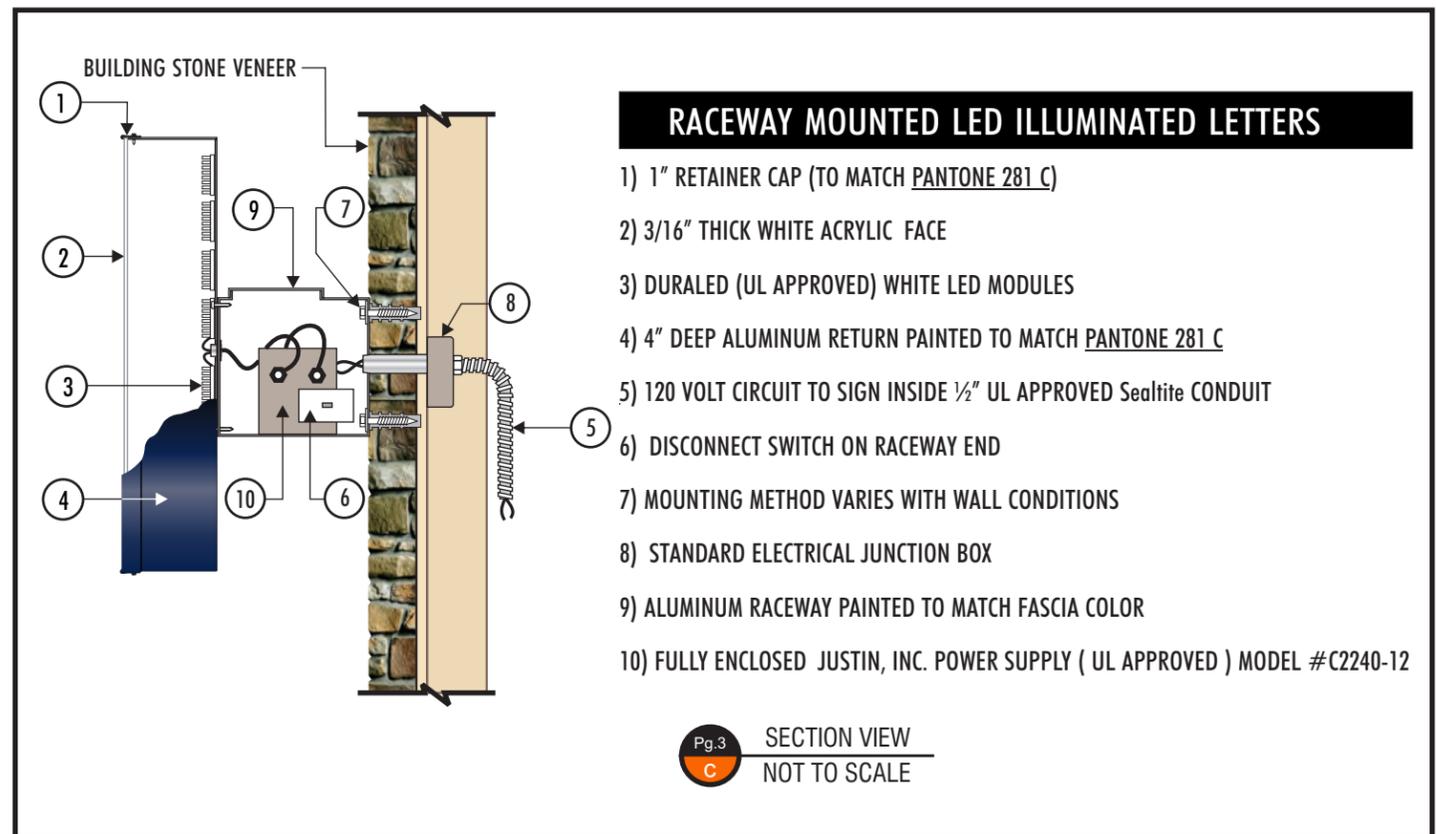
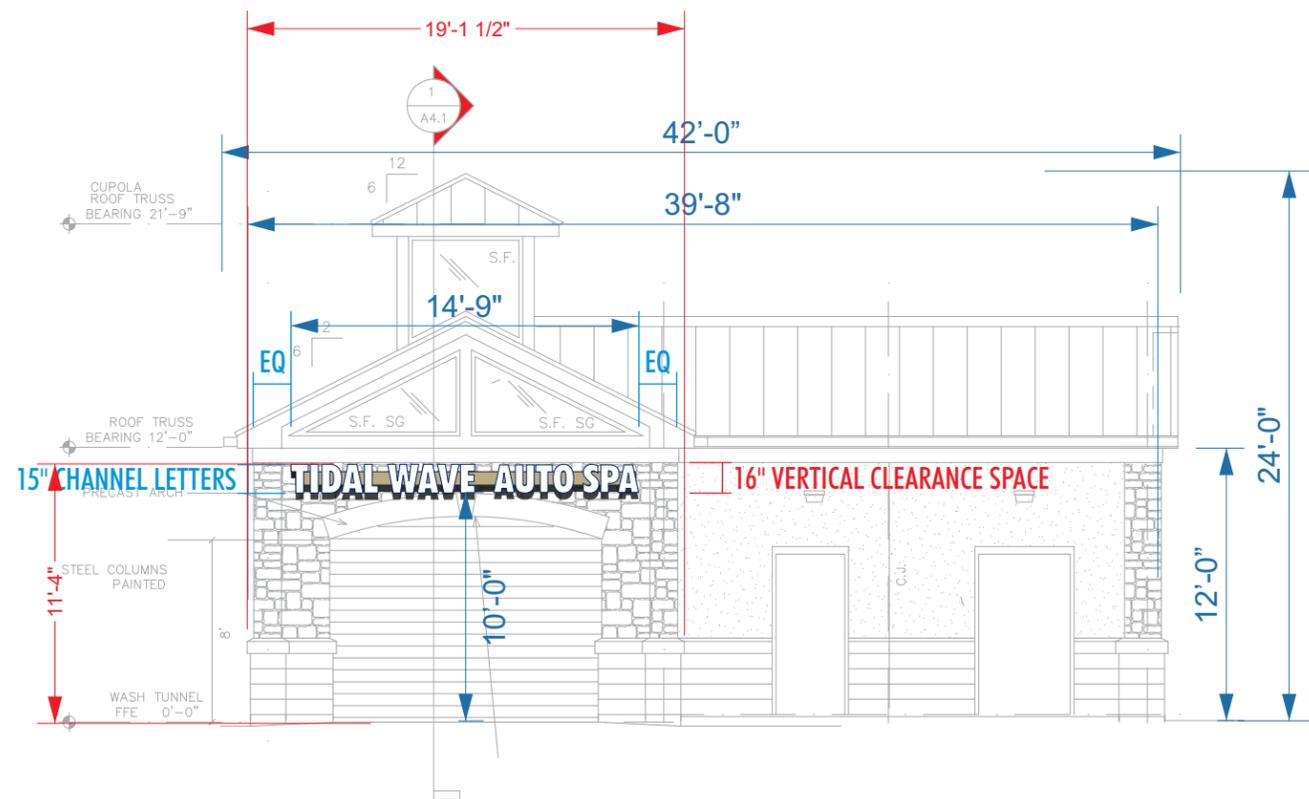
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SIGN TYPE B: FACE-LIT CHANNEL LETTERS
 QTY: 1 SET FOR TUNNEL ENTRANCE- **19.01 SQ. FT.**



WALL AREA 216.75 SQ. FT
 20% ALLOWED FOR SIGNAGE= 43.35 SQ. FT.

Pg.3
 a FRONT VIEW
 Scale: 3/4" = 1'



RACEWAY MOUNTED LED ILLUMINATED LETTERS

- 1) 1" RETAINER CAP (TO MATCH PANTONE 281 C)
- 2) 3/16" THICK WHITE ACRYLIC FACE
- 3) DURALED (UL APPROVED) WHITE LED MODULES
- 4) 4" DEEP ALUMINUM RETURN PAINTED TO MATCH PANTONE 281 C
- 5) 120 VOLT CIRCUIT TO SIGN INSIDE 1/2" UL APPROVED Sealite CONDUIT
- 6) DISCONNECT SWITCH ON RACEWAY END
- 7) MOUNTING METHOD VARIES WITH WALL CONDITIONS
- 8) STANDARD ELECTRICAL JUNCTION BOX
- 9) ALUMINUM RACEWAY PAINTED TO MATCH FASCIA COLOR
- 10) FULLY ENCLOSED JUSTIN, INC. POWER SUPPLY (UL APPROVED) MODEL #C2240-12

Pg.3
 C SECTION VIEW
 NOT TO SCALE

- P1 PAINT - MATCH PANTONE 281 C (GLOSS FINISH)
- P2 PAINT - MATCH BUILDING FACADE (GLOSS FINISH)

CHANNEL LETTERS (WHITE ILLUMINATION) WET LOCATION			
 	LED's JTSLSO65W-0.5 WHITE LED POWER SUPPLY BL-120-12-60 POWER SUPPLY		CONNECT THE AC LINE TO THE BLACK (LINE) AND WHITE (NEUTRAL) INPUT WIRES OF THE POWER SUPPLY USING 18-14 AWG TWIST ON WIRE CONNECTORS. GROUND POWER SUPPLY GREEN WIRE TO GROUNDING SCREW. REPLACE JUNCTION BOX COVER.
THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR APPLICABLE LOCAL CODES. THIS INCLUDES PROPER GROUNDING & BONDING OF THE SIGN.			

5198 NORTH LAKE DRIVE
 LAKE CITY, GA 30260
 404-361-3800
 FAX 404-361-7038
 WWW.CLAYTONSIGNS.COM

DATE - 10.5.20
 SIGN TYPE
 EXTERIOR SIGN PACKAGE

CLIENT
 TIDAL WAVE
 416 Brandilynn Blvd
 Cedar Falls, IA 50613

REVISIONS:
 REVISION # - DATE

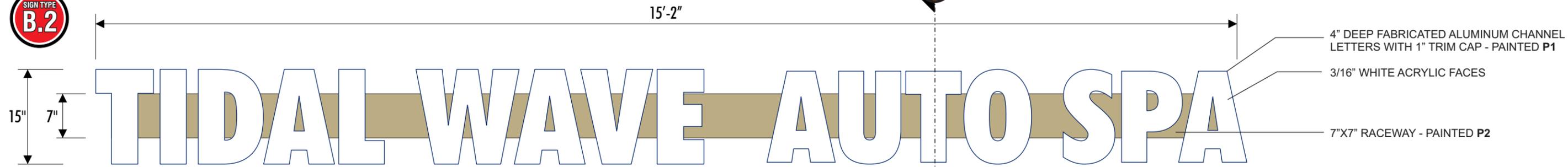
DESIGNER Ryan Mc
 ACCOUNT REP. NAME
 Todd Willis

SCALE: AS INDICATED
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 O:\RM\ TIDAL WAVE \ CEDAR FALLS, IA \
 TIDAL WAVE CEDAR FALLS IA SIGN PACKAGE -
 10.5.20 .CDR

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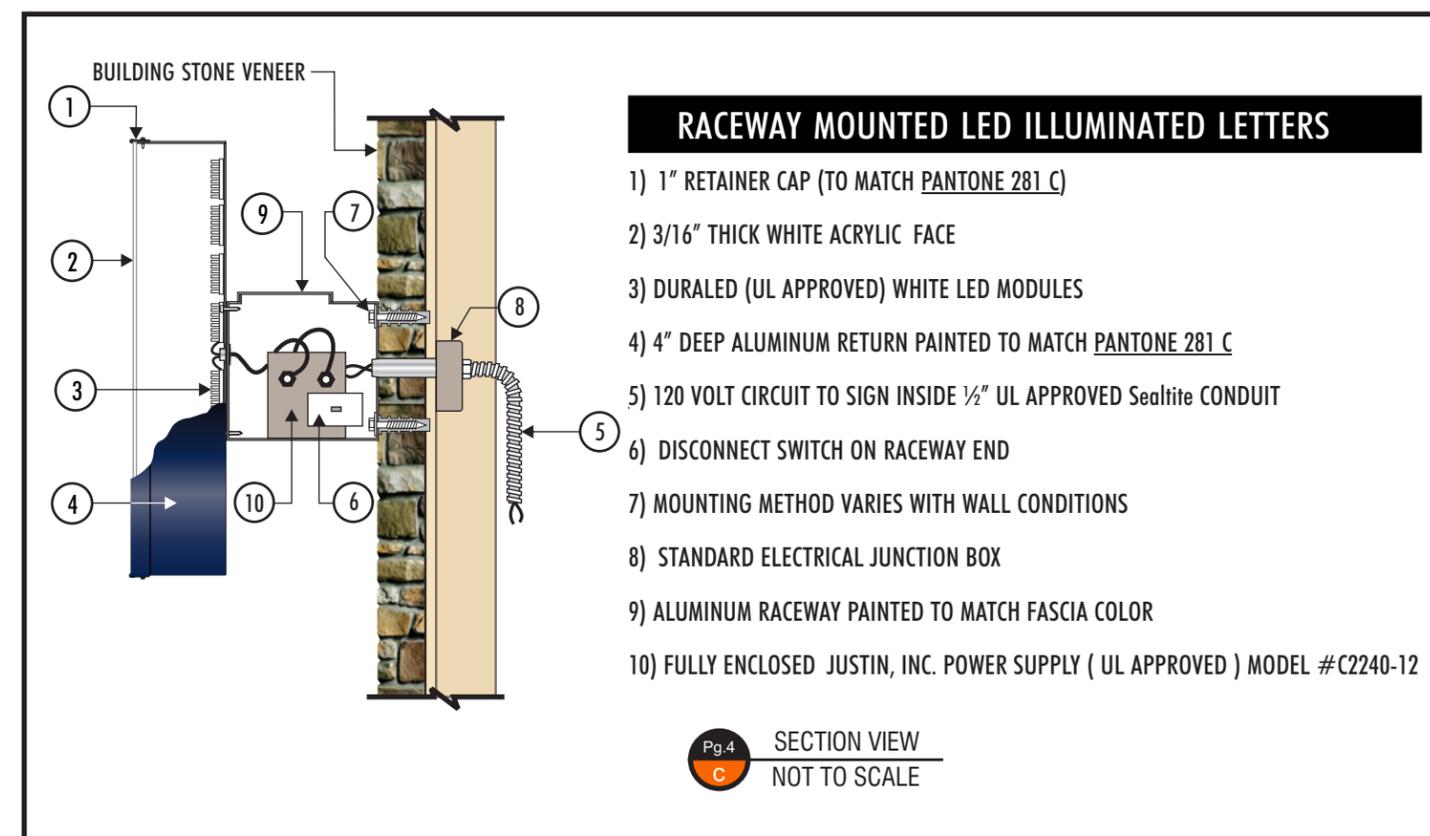
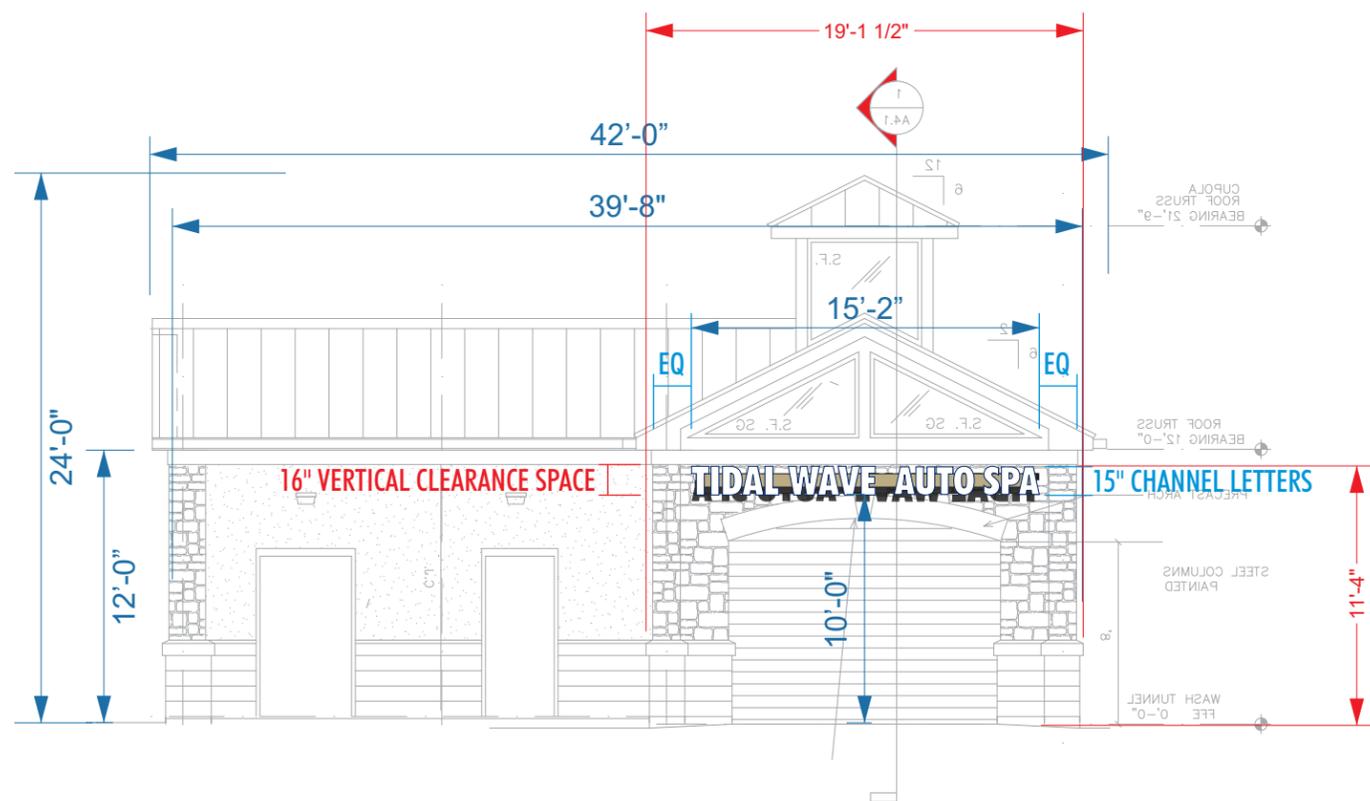
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SIGN TYPE B: FACE-LIT CHANNEL LETTERS
 QTY: 1 SET FOR TUNNEL EXIT- **19.01 SQ. FT.**



WALL AREA 216.75 SQ. FT
 20% ALLOWED FOR SIGNAGE= 43.35 SQ. FT.

Pg.4
 a FRONT VIEW
 Scale: 3/4" = 1'



Pg.4
 C SECTION VIEW
 NOT TO SCALE

- P1** PAINT - MATCH PANTONE 281 C (GLOSS FINISH)
- P2** PAINT - MATCH BUILDING FACADE (GLOSS FINISH)

CHANNEL LETTERS (WHITE ILLUMINATION) WET LOCATION

LED's JTSLSO65W-0.5 WHITE LED
 POWER SUPPLY BL-120-12-60 POWER SUPPLY

CONNECT THE AC LINE TO THE BLACK (LINE) AND WHITE (NEUTRAL) INPUT WIRES OF THE POWER SUPPLY USING 18-14 AWG TWIST ON WIRE CONNECTORS. GROUND POWER SUPPLY GREEN WIRE TO GROUNDING SCREW. REPLACE JUNCTION BOX COVER.

THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR APPLICABLE LOCAL CODES. THIS INCLUDES PROPER GROUNDING & BONDING OF THE SIGN.

5198 NORTH LAKE DRIVE
 LAKE CITY, GA 30260
 404-361-3800
 FAX 404-361-7038
 WWW.CLAYTONSIGNS.COM

DATE - 10.5.20
 SIGN TYPE
 EXTERIOR SIGN PACKAGE

CLIENT
 TIDAL WAVE
 416 Brandilynn Blvd
 Cedar Falls, IA 50613

REVISIONS:
 REVISION # - DATE

DESIGNER Ryan Mc
 ACCOUNT REP. NAME
 Todd Willis

SCALE: AS INDICATED
 DRAWING FILE NAME
 O:\RM\ TIDAL WAVE \ CEDAR FALLS, IA \ TIDAL WAVE CEDAR FALLS IA SIGN PACKAGE - 10.5.20 .CDR

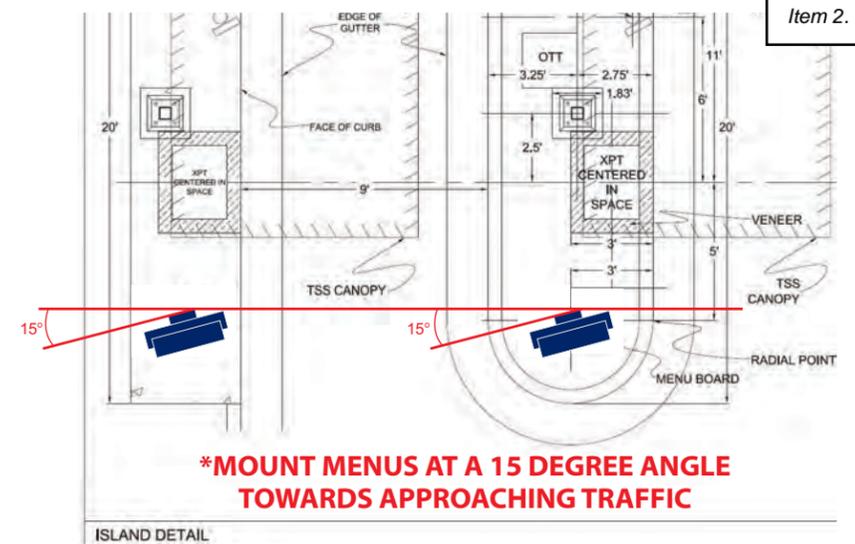
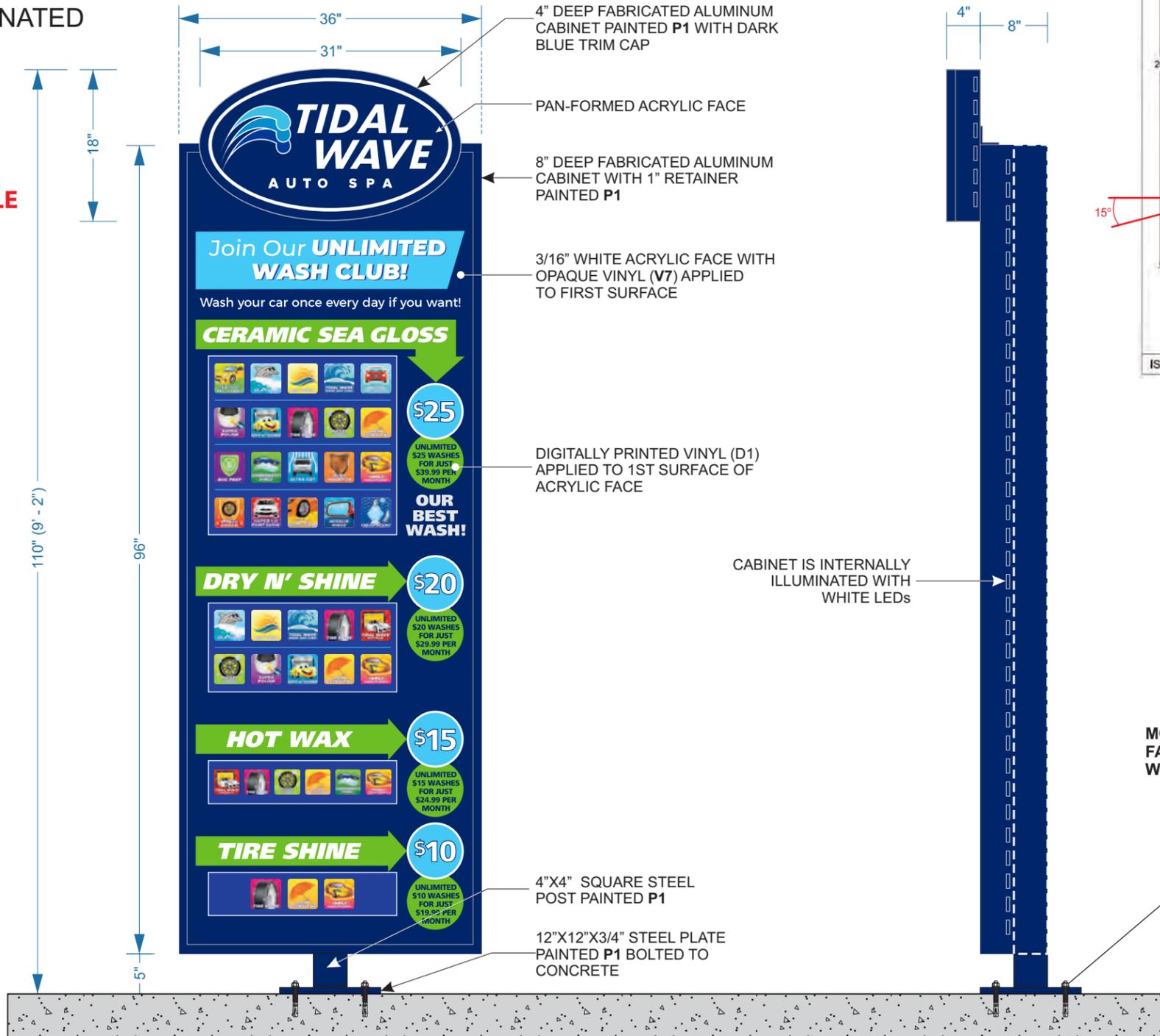
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SIGN TYPE C: MENUS
SINGLE-SIDED, INTERNALLY ILLUMINATED
QTY: 2



***MOUNT MENUS AT A 15 DEGREE ANGLE TOWARDS APPROACHING TRAFFIC**



Item 2.

P1 PAINT - MATCH PANTONE 281C

- D1 DIGITALLY PRINTED TRANSLUCENT VINYL**
- MATCH PANTONE 281C**
- MATCH PANTONE 2985C**
- MATCH PANTONE PROCESS BLUE**
- MATCH PANTONE 368C**

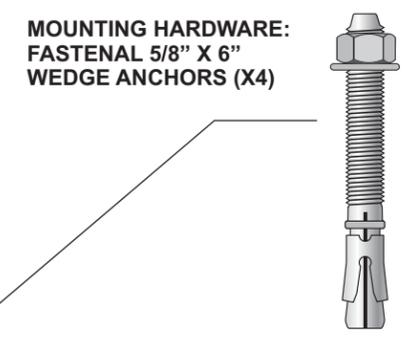
DOUBLE FACED CABINET		WET LOCATION	
	LED's GEWHDSR6-71K-DOUBLE SIDED		CONNECT THE AC LINE TO THE BLACK (LINE) AND WHITE (NEUTRAL) INPUT WIRES OF THE POWER SUPPLY USING 18-14 AWG TWIST ON WIRE CONNECTORS.
	POWER SUPPLY GEPS24-24V POWER SUPPLY		GROUND POWER SUPPLY GREEN WIRE TO GROUNDING SCREW. REPLACE JUNCTION BOX COVER.

THIS SIGN IS INTENDED TO BE INSTALLED IN ACCORDANCE WITH REQUIREMENTS OF ARTICLE 600 OF THE NATIONAL ELECTRICAL CODE AND/OR APPLICABLE LOCAL CODES. THIS INCLUDES PROPER GROUNDING & BONDING OF THE SIGN.

V7 OPAQUE VINYL - MATCH PANTONE 281 C

Pg.5 a FRONT ELEVATION
 Scale: 3/4" = 1'

Pg.5 b SIDE PROFILE
 Scale: 3/4" = 1'



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DATE - 10.5.20
 SIGN TYPE
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CLIENT
 TIDAL WAVE
 416 Brandilynn Blvd
 Cedar Falls, IA 50613

REVISIONS:
 REVISION # - DATE

DESIGNER Ryan Mc
 ACCOUNT REP. NAME
 Todd Willis

SCALE: AS INDICATED
 DRAWING FILE NAME
 O:\RM\ TIDAL WAVE \ CEDAR FALLS, IA \
 TIDAL WAVE CEDAR FALLS IA SIGN PACKAGE -
 10.5.20 .CDR

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SIGN TYPE D: DOUBLE FACED ILLUMINATED ENTRANCE SIGN

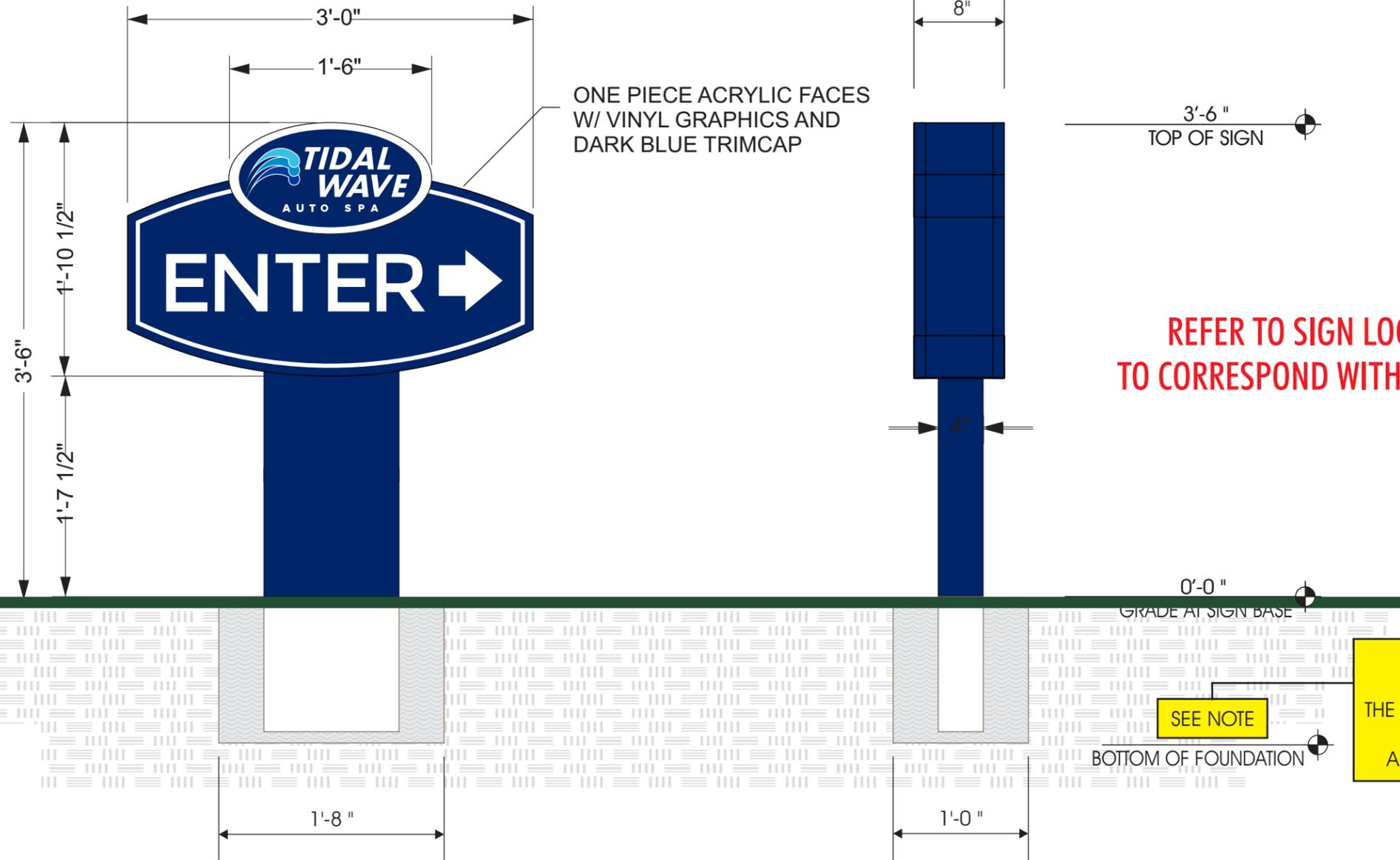
QTY: 1

Item 2.



SPECIFICATIONS

CABINET
 ALUMINUM CONSTRUCTION CABINET
 .090 ALUMINUM CLADDING,
 RETAINERS AND FILLER.
 GRAPHICS
 ACRYLIC FACES DECORATED
 WITH TRANSLUCENT VINYL
 FILM ON SURFACE OF ACRYLIC. FACES
 ARE BACKLIT BY HIGH OUTPUT LEDs
 .STEEL POLE
 4" x 12" STEEL POLE



REFER TO SIGN LOCATION SITE MAP (PAGE 1)
 TO CORRESPOND WITH COLORED CIRCLE INDICATORS

IN LOCATIONS NORTH OF
 TENN. & N. CAROLINA
 THE FOOTINGS MUST BE 3'-0" DEEP
 DUE TO THE FROST LINE
 ALL OTHERS TO BE 2'- 0" DEEP

PAINT COLOR FOR SIGN CABINET

 TO MATCH PANTONE 281 C. GLOSS FINISH



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SIGN TYPE E: DIRECTIONAL POST & PANEL SIGNS
QTY 3

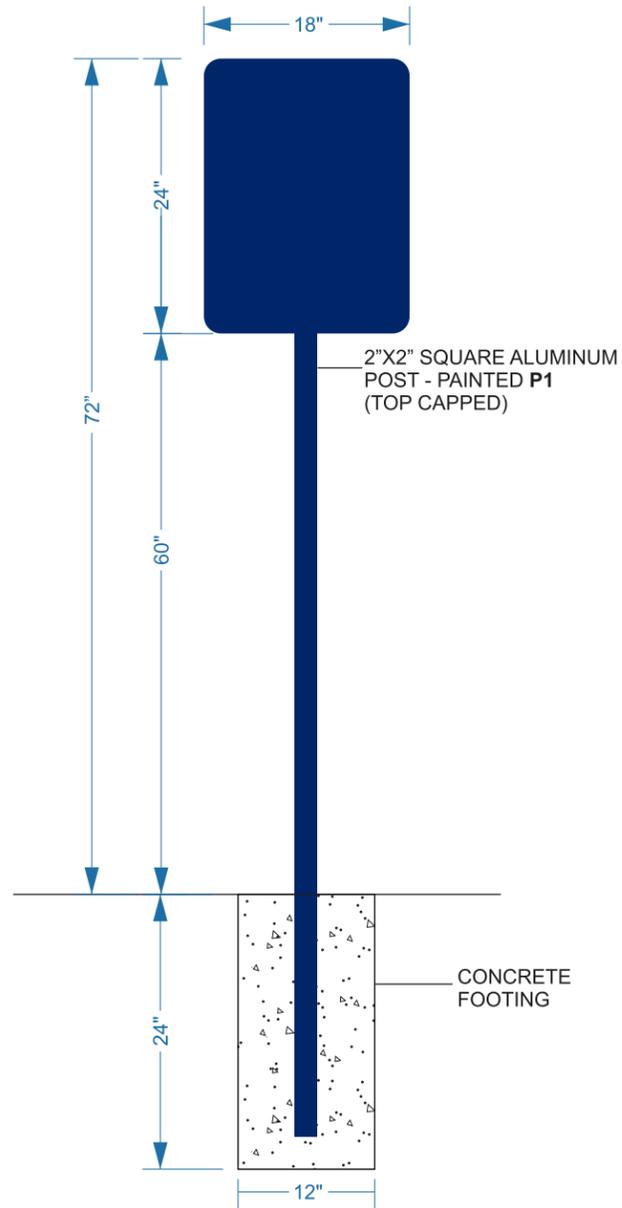


.080" ALUMINUM
SIGN PANEL PAINTED P1

CONTOUR-CUT VINYL (V1)

THIS COPY FOR REFERENCE
ONLY. SEE MESSAGE SCHEDULE
FOR ACTUAL LAYOUTS

Pg.7
a FRONT VIEW
Scale: 1 1/2" = 1'



2"X2" SQUARE ALUMINUM
POST - PAINTED P1
(TOP CAPPED)

CONCRETE
FOOTING

Pg.7
b MOUNTING DETAIL
Scale: 3/4" = 1'

SIGN TYPE E: DIRECTIONAL POST & PANEL SIGNS
MESSAGE SCHEDULE

Item 2.



SIGN #1 S/F



SIGN #2 S/F



SIGN #3 S/F

P1 PAINT - MATCH PANTONE 281C

V1 VINYL - AVERY 700-101-0 WHITE



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ACCOUNT REP. NAME
Todd Willis

SCALE: AS INDICATED
DRAWING FILE NAME
O:\RM\ TIDAL WAVE \ CEDAR FALLS, IA \
TIDAL WAVE CEDAR FALLS IA SIGN PACKAGE -
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DRAWING
STATUS:



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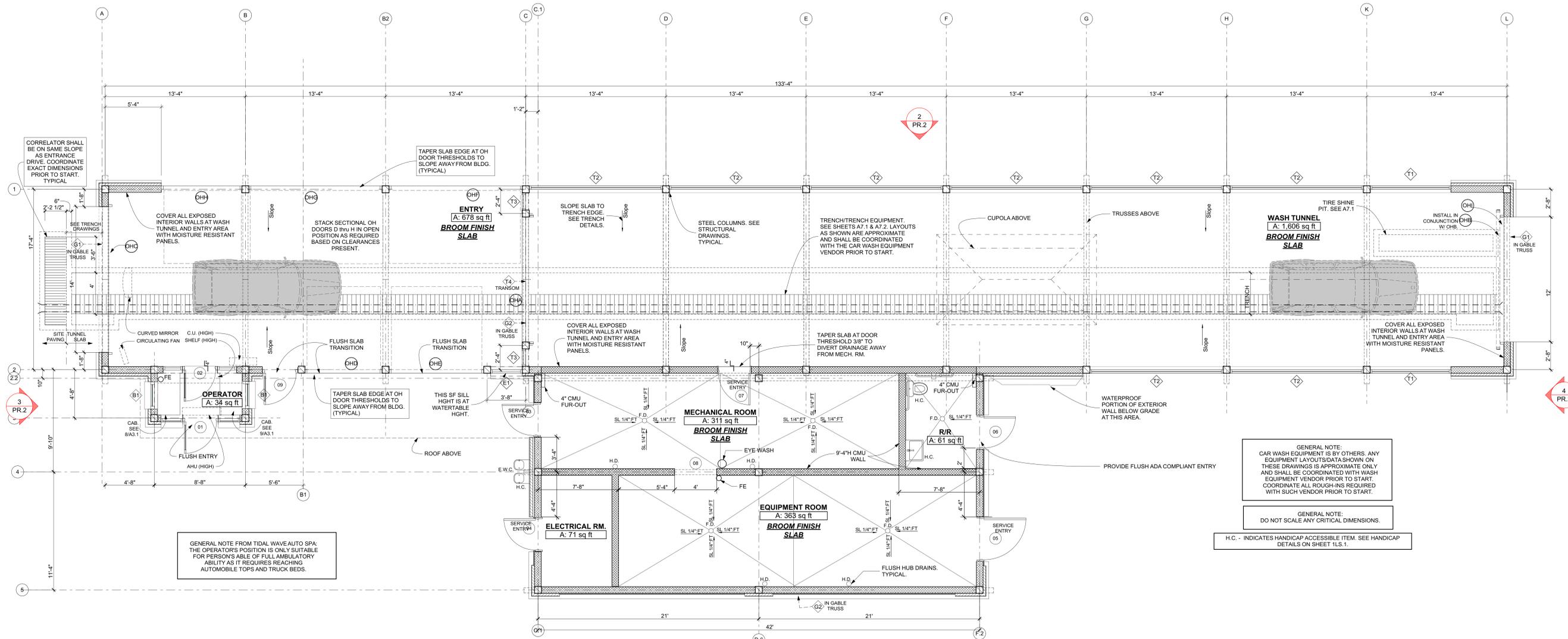
SIGN TYPE F: PAINTED PANELS W/ GRAPHICS
 FOR DUMPSTER GATE
 QTY: 1

Item 2.



**VERIFY DUMPSTER DOOR FRAME
 DIMENSIONS WITH G.C. PRIOR
 TO PRODUCTION**





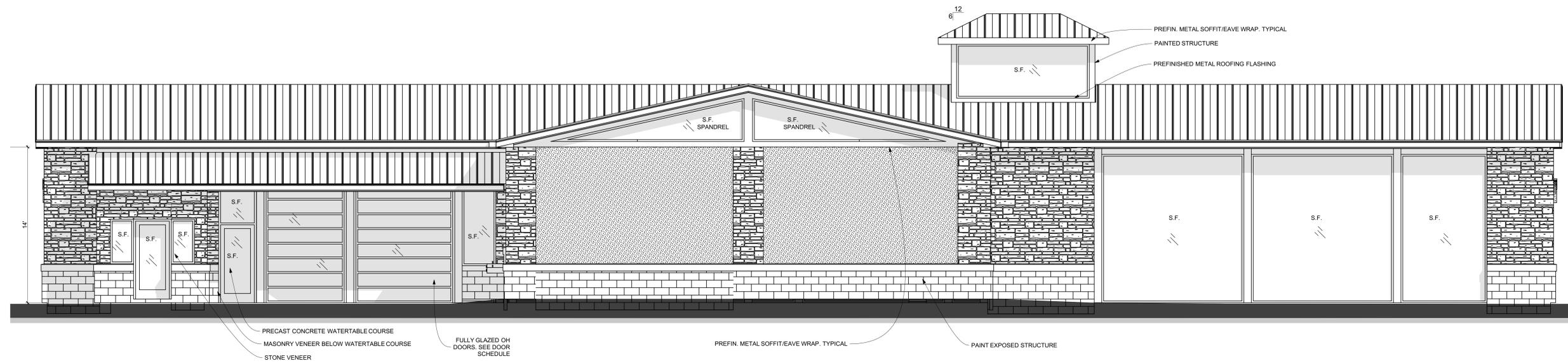
1 PRELIMINARY FLOOR PLAN
SCALE: 1/4" = 1'-0"

TIDAL WAVE AUTO SPA - EXTERIOR FINISH SCHEDULE:

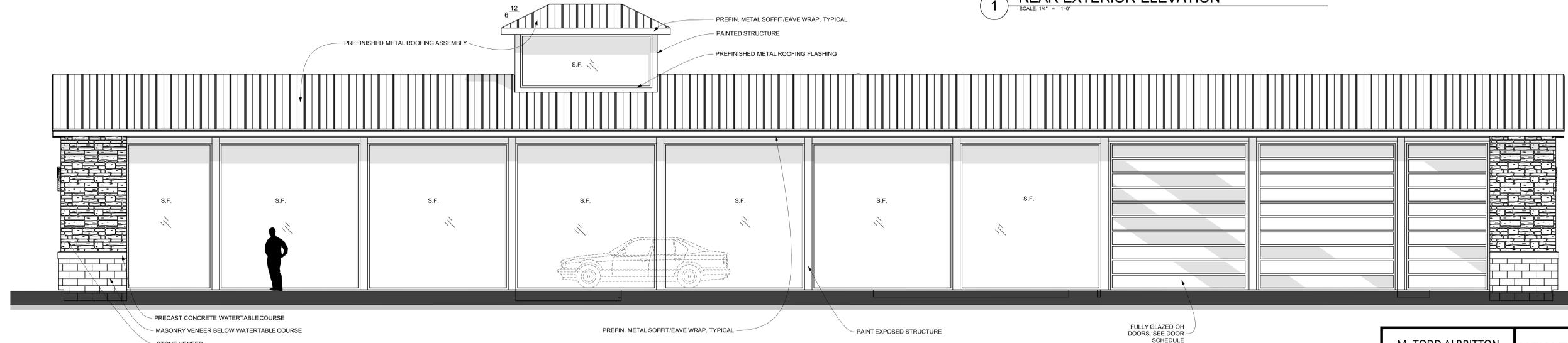
<p>GLAZING - CLEAR (PROVIDE SPANDREL GLAZING IN GLAZED GABLE TRUSSES LOCATED AT ENCLOSED ATTICS)</p>	<p>PREFINISHED METAL ROOF COLOR - REGAL BLUE (PREFIN SOFFIT, FASCIA, FLASHING COLOR-MATCH ROOF COLOR)</p>	<p>PRECAST WATERTABLE COURSE OLD CASTLE - WHITE</p>
<p>STONE VENEER KENTUCKY LEDGESTONE DO NOT PAINT BOTTOM SIDE OF ANY EXPOSED METAL ROOF PANEL</p>	<p>STOREFRONT FRAME COLOR - BRONZE</p>	<p>SPLIT FACE MASONRY OLD CASTLE -</p>
<p>EXPOSED STRUCTURAL STEEL TRUSSES AND COLUMNS. PAINT - BLACK</p>	<p>HM DOORS/FRAMES PAINT SW 2735 ROCKWEED</p>	

<p>M. TODD ALBRITTON ARCHITECT</p> <p>202 EAST MAIN STREET THOMASTON, GEORGIA 30286 PH 770-550-3275 mtoddalbrittonarchitect@gmail.com</p>		<p>NEW TIDAL WAVE AUTO SPA</p> <p>### ### ##### #####, #####</p> <p>OWNER: TIDAL WAVE AUTO SPA EAST THOMPSON STREET THOMASTON GEORGIA 30286</p>
<p>THESE DESIGNS AND DRAWINGS AS INSTRUMENTS OF SERVICE ARE THE PROPERTY OF THE ARCHITECT AND SHALL NOT BE COPIED OR REUSED IN ANY FORM WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT. THESE DRAWINGS SHALL BE USED ON THE ORIGINAL PROJECT LOCATION ONLY AND SHALL NOT BE REUSED AT OTHER LOCATIONS.</p>		
<p>TIDAL WAVE A U T O S P A</p>		
<p>MARK DATE DESCRIPTION</p>		
<p>SHEET TITLE PRELIMINARY</p>		
<p>PROJECT DATE: #####</p>		
<p>PROJECT NUMBER: ##</p>		
<p>DRAWN BY: Name</p>		
<p>PR.1</p>		

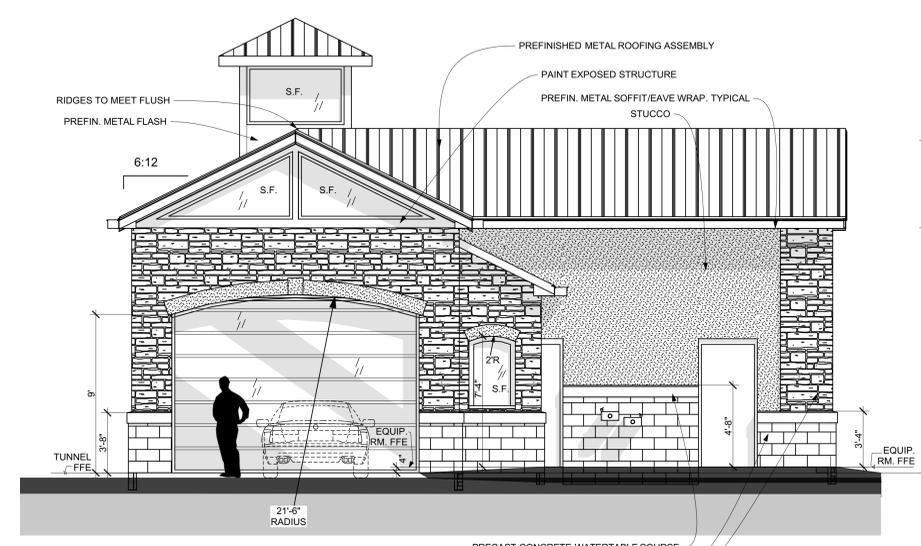
NOT RELEASED FOR CONSTRUCTION



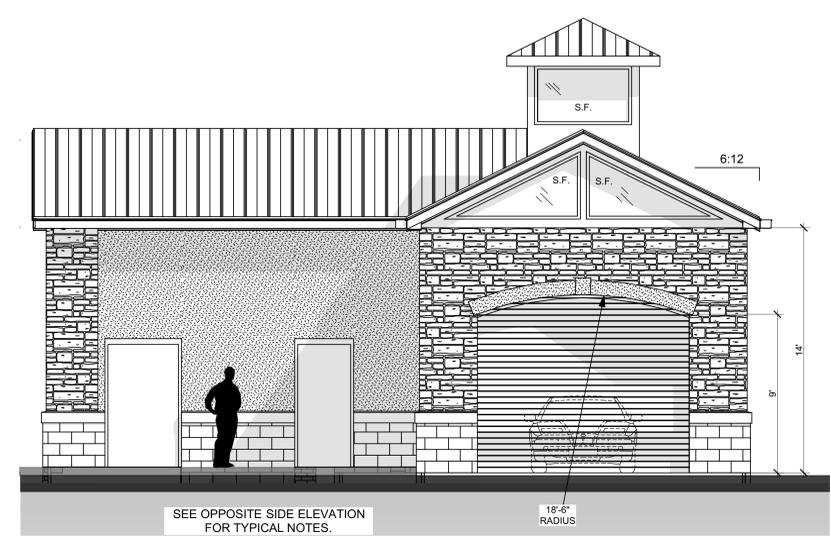
1 REAR EXTERIOR ELEVATION
SCALE: 1/4" = 1'-0"



2 FRONT ELEVATION
SCALE: 1/4" = 1'-0"



3 SIDE ELEVATION - ENTRY
SCALE: 1/4" = 1'-0"



4 SIDE ELEVATION - EXIT
SCALE: 1/4" = 1'-0"

M. TODD ALBRITTON
ARCHITECT
202 EAST MAIN STREET
THOMASTON, GEORGIA
30286
PH 770-550-3275
mtoddalbrittonarchitect@gmail.com

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NEW TIDAL WAVE
AUTO SPA

#####, #####
OWNER:
TIDAL WAVE AUTO SPA
EAST THOMPSON STREET
THOMASTON GEORGIA
30286



MARK	DATE	DESCRIPTION

SHEET TITLE
PRELIMINARY

PROJECT DATE: #####
PROJECT NUMBER: ##
DRAWN BY: Name

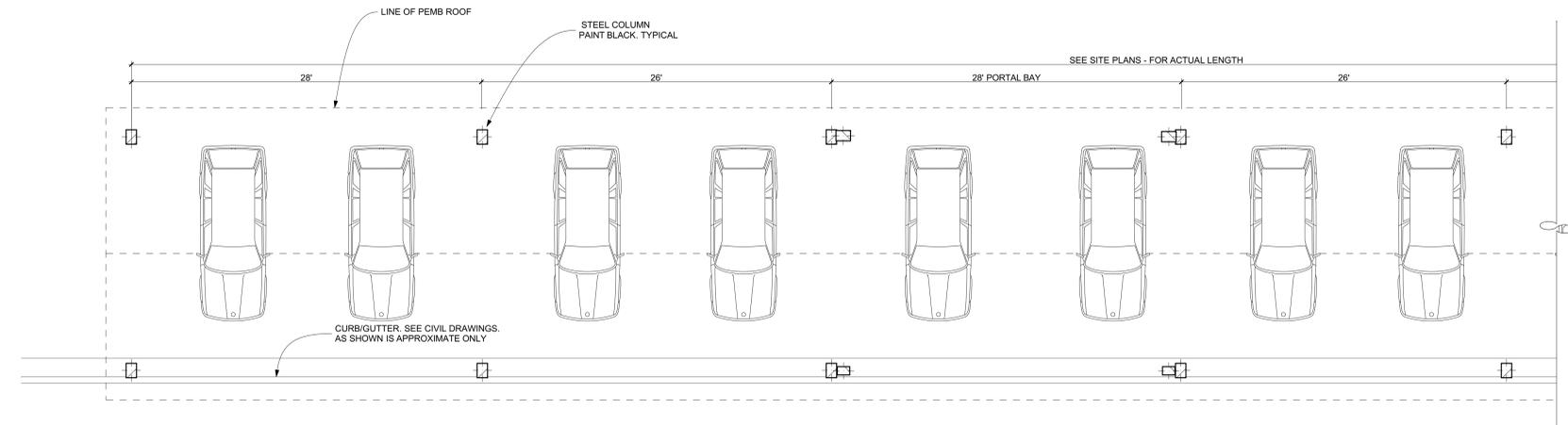
PR.3

NOT RELEASED FOR CONSTRUCTION

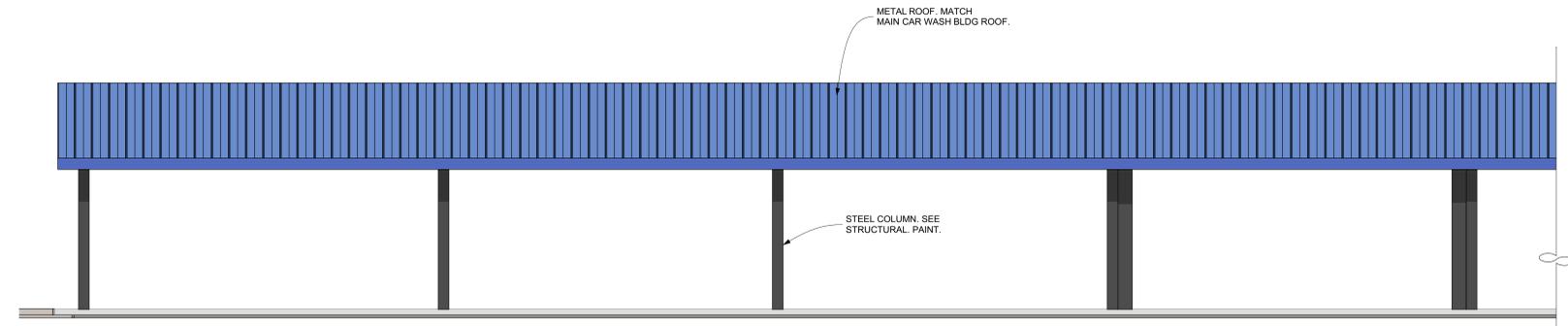
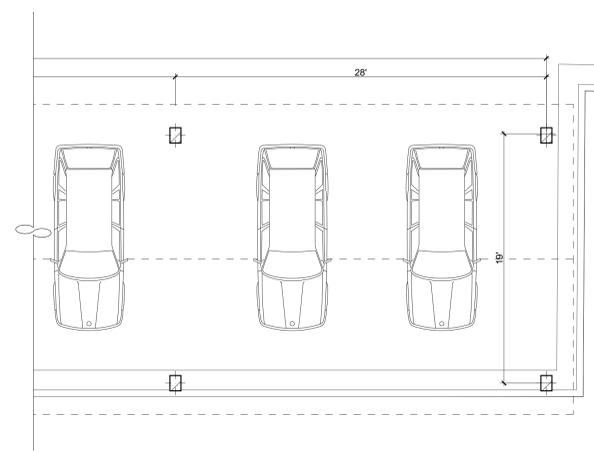
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NEW TIDAL WAVE AUTO SPA
 CEDAR FALLS, IA

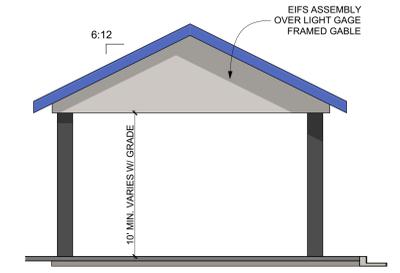
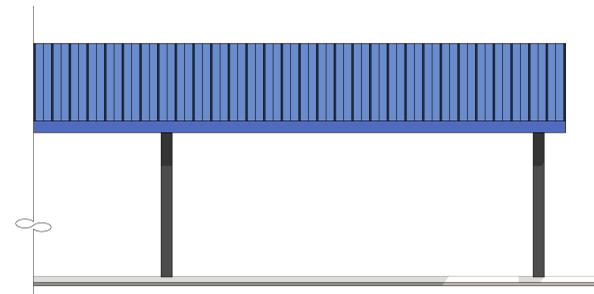
OWNER:
TIDAL WAVE AUTO SPA
 EAST THOMPSON STREET
 THOMASTON GEORGIA
 30286



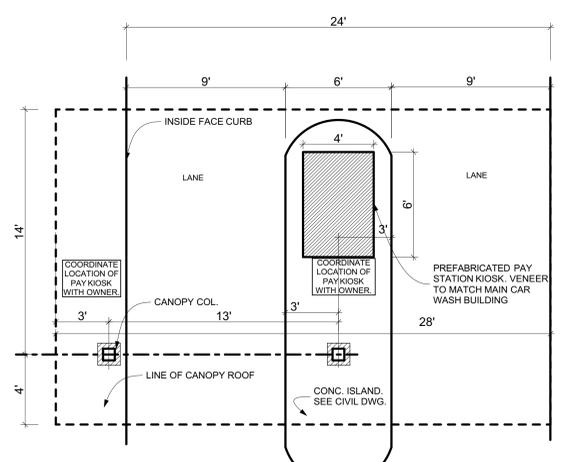
1 VAC CANOPY
 SCALE: 3/16" = 1'-0"



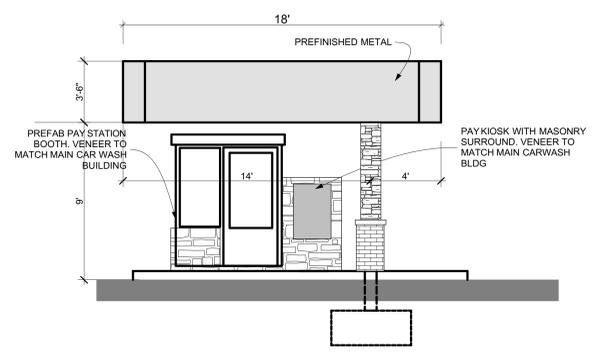
2 FRONT/REAR ELEVATION VAC CANOPY
 SCALE: 3/16" = 1'-0"



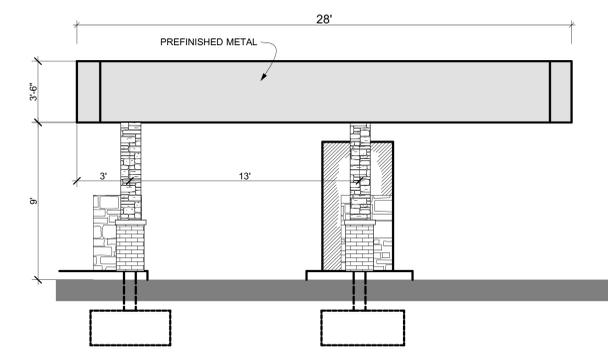
3 END ELEVATION - VAC CANOPY
 SCALE: 3/16" = 1'-0"



PAY STATION - PLAN



PAY STATION CANOPY SIDE ELEVATION



PAY STATION CANOPY FRONT/REAR ELEVATION

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COLORS:
 COLORS TO MATCH MAIN CAR WASH BUILDING. COORDINATE WITH TIDAL WAVE, INC PRIOR TO START

MARK	DATE	DESCRIPTION

SHEET TITLE

AUXILIARY BUILDINGS

PROJECT DATE: 2/18/20 PERMIT SET

PROJECT NUMBER: ##

DRAWN BY: Name

AB.2

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