



AGENDA

OF A REGULAR MEETING
OF THE
CITY OF COACHELLA
PLANNING COMMISSION

May 01, 2024 6:00 PM

In-Person Meeting Location:

City Council Chamber 1515 6th Street, Coachella, CA If you would like to attend the meeting via Zoom, here is the link:

 $\underline{https://us02web.zoom.us/j/84544257915?pwd} = \underline{VTdHWitpYVdOUk1NQW8vZ1pqUm0zQT}$

<u>09</u>

Or One tap mobile: +16694449171,,84544257915#,,,,*380084#

Or Telephone:

US: +1 669 900 6833 **Webinar ID: 845 4425 7915**

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abajo de la pantalla

• Public comments may be received **either in person, via email, telephonically, or via Zoom** with a limit of **250 words, or three minutes:**

o <u>In Real Time</u>:

If participating in real time via Zoom or phone, during the Public Comment Period, use the "raise hand" function on your computer, or when using a phone, participants can raise their hand by pressing *9 on the keypad.

In Writing:

Written comments may be submitted to the City Council electronically via email to cityclerk@coachella.org. Transmittal **prior to the start** of the meeting is required. All written comments received will be forwarded to the City Council and entered into the record.

- o If you wish, you may leave a message at (760) 262-6240 before 5:30 p.m. on the day of the meeting.
- The **live stream** of the meeting may be **viewed online** by accessing the city's website at www.coachella.org, and clicking on the "Watch Council Meetings" tab located on the home page, and then clicking on the "live" button.

CALL TO ORDER:

PLEDGE OF ALLEGIANCE:

ROLL CALL:

APPROVAL OF AGENDA:

"At this time the Commission may announce any items being pulled from the agenda or continued to another date or request the moving of an item on the agenda."

APPROVAL OF THE MINUTES:

1. Planning Commission Meeting Minutes April 17, 2024

WRITTEN COMMUNICATIONS:

PUBLIC COMMENTS (NON-AGENDA ITEMS):

"The public may address the Commission on any item of interest to the public that is not on the agenda, but is within the subject matter jurisdiction thereof. Please limit your comments to three (3) minutes."

REPORTS AND REQUESTS:

NON-HEARING ITEMS:

2. Code Enforcement Update - Commercial Centers

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

- 3. <u>CUP No. 254 (Modification)</u>, <u>AR No. 24-02 Coachella Gateway</u> a modification to add a drive-thru multitenant commercial building within the Coachella Gateway shopping center approved under CUP No. 254 at the southwest corner of Ave 48 and Grapefruit Boulevard, adjacent to the Dillon Bridge. Applicant: Chandi & Karan LLC
- 4. <u>CUP 375 AR No. 23-14 Coachella Islamic Center Parking Lot</u> for the construction of a parking lot to serve the existing building at 84650 Ave 49. Applicant: Islamic Society of Palm Springs. Recommendation: Continue to May 15, 2024.

INFORMATIONAL:

ADJOURNMENT:

Complete Agenda Packets are available for public inspection at the City Clerk's Office at 53-462 Enterprise Way, Coachella, California, and on the City's website www.coachella.org.

THIS MEETING IS ACCESSIBLE TO PERSONS WITH DISABILITIES





AGENDA

DE UNA REUNIÓN DE LA

COMISIÓN DE PLANIFICACIÓN PLANNING COMMISSION

1 de Mayo, 2024 6:00 PM

https://us02web.zoom.us/j/84544257915?pwd=VTdHWitpYVdOUk1NQW8vZ1pqUm0zQT09

O one tap mobile:

Us: +16699006833,, 84544257915#,,,,* 380084# US

O teléfono:

Us: +1 669 900 6833

ID del webinar: 845 4425 7915 Código de acceso: 380084

Español: El idioma español está disponible en Zoom seleccionado la opción en la parte de abajo de la pantalla

Los comentarios públicos se pueden recibir por correo electrónico, por teléfono o por zoom con un límite de 250 palabras o tres minutos:

En vivo:

Si participa en vivo a través de zoom o teléfono, durante el período de comentarios públicos, use la función "levantar la mano" en su computadora, o cuando use un teléfono, los participantes pueden levantar la mano presionando *9 en el teclado.

Por escrito:

Los comentarios escritos pueden enviarse a la comisión electrónicamente por correo electrónico a gperez@coachella.org. Se requiere la transmisión antes del inicio de la reunión. Todos los comentarios escritos recibidos serán enviados a la comisión e ingresados en el registro.

SI LO DESEA, PUEDE DEJAR UN MENSAJE EN EL (760) 398-3102, EXTENSIÓN 122, ANTES DE LAS 4:00 P.M. DEL DÍA DE LA REUNIÓ

LLAMADO AL ORDEN:

JURAMENTO A LA BANDERA:

PASE DE LISTA:

APROBACIÓN DE LA

AGENDA:

"En este momento, la Comisión puede anunciar cualquier punto que está siendo retirado de la agenda o continuado a otra fecha o solicitar el traslado de un punto de la agenda".

APROBACION DE LAS ACTAS:

1. Borrador de las Actas de la Comisión de Planificación – 17 de Abril 2024.

COMUNICACIONES ESCRITAS:

COMENTARIOS DEL PÚBLICO (PUNTOS QUE NO ESTÁN EN LA AGENDA):

"El público puede dirigirse a la Comisión sobre cualquier tema de interés para el público que no esté en la agenda, pero que esté dentro de la jurisdicción de la materia de la misma. Por favor limite sus comentarios a tres

(3) minutos".

INFORMES Y SOLICITUDES:

PUNTOS QUE NO SON DE AUDIENCIA:

2. Actualización sobre la aplicación del código: centros comerciales

CALENDARIO DE AUDIENCIAS PÚBLICAS (CUASI-JUDICIAL):

- 1. CUP No. 254 (Modificación), AR No. 24-02 Coachella Gateway: una modificación para agregar un edificio comercial de múltiples inquilinos con servicio de autoservicio dentro del centro comercial Coachella Gateway aprobado bajo CUP No. 254 en la esquina suroeste de Ave 48 y Grapefruit Boulevard, adyacente al puente Dillon. Solicitante: Chandi & Karan LLC
- 2. <u>AR No. 23-14 CUP 375 Estacionamiento del Centro Islámico de Coachella</u> para la construcción de un estacionamiento para servir al edificio existente de la Sociedad Islámica de Coachella de Palm Springs en 84650 49th Avenue. Solicitante: Sociedad Islámica de Palm Springs

INFORMATIVO:

SE SUSPENDE LA SESIÓN:

Los paquetes completos de la agenda están disponibles para inspección pública en el Departamento de Servicios de Desarrollo en 53-990 Enterprise Way, Coachella, California, y en el sitio web de la ciudad <u>www.coachella.org</u>.

ESTA REUNIÓN ES ACCESIBLE PARA PERSONAS CON DISCAPACIDAD



City Council Chamber 1515 6th Street, Coachella, CA (760) 398-3502 ◆ www.coachella.org

MINUTES

OF A REGULAR MEETING
OF THE
CITY OF COACHELLA
PLANNING COMMISSION

April 17, 2024 6:00 PM

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IF YOU WISH, YOU MAY LEAVE A MESSAGE AT (760) 398-3102, EXTENSION 122, BEFORE 4:00 P.M. ON THE DAY OF THE MEETING

CALL TO ORDER: 6:02 PM

PLEDGE OF ALLEGIANCE:

Alternate Commissioner Fonseca.

ROLL CALL:

Commissioners Present: Commissioner Arvizu, Chair Hernandez, Vice Chair Gonzalez, Commissioner

Ramirez, Alternate Commissioner Fonseca.

Commissioners Absent: Commissioner Murillo.

Staff Present: *Gabriel Perez, Development Services Director.

*Adrian Moreno, Associate Planner. *Andrew Simmons, City Engineer

*Jesus Medina, Information Technology Technician

APPROVAL OF AGENDA:

"At this time the Commission may announce any items being pulled from the agenda or continued to another date or request the moving of an item on the agenda."

IT WAS MOVED BY COMMISSIONER RAMIREZ AND SECONDED BY COMMISSIONER ARVIZU TO APPROVE THE AGENDA AND TABLE ITEMS THREE (3) AND FOUR (4).

Approved by the following roll call vote:

AYES: Commissioner Ramirez, Vice Chair Gonzalez, Chair Hernandez, Commissioner Arvizu, Alternate Commissioner Fonseca.

NOES: None. ABSTAIN: None.

ABSENT: Commissioner Murillo.

APPROVAL OF THE MINUTES:

1. Planning Commission Meeting Minutes – March 20, 2024.

IT WAS MOVED BY COMMISSIONER RAMIREZ AND SECONDED BY ALTERNATE COMMISSIONER FONSECA TO APPROVE THE MINUTES.

Approved by the following roll call vote:

AYES: Vice Chair Gonzalez, Commissioner Ramirez, Alternate Commissioner Fonseca, Commissioner Arvizu, and Chair Hernandez.

NOES: None. ABSTAIN: None.

ABSENT: Commissioner Murillo.

WRITTEN COMMUNICATIONS:

None.

PUBLIC COMMENTS (NON-AGENDA ITEMS):

"The public may address the Commission on any item of interest to the public that is not on the agenda, but is within the subject matter jurisdiction thereof. Please limit your comments to three (3) minutes."

REPORTS AND REQUESTS:

None.

NON-HEARING ITEMS:

2. <u>CUP 364 1-Year Time Extension – AMPM – Type 21 Alcohol Sales</u> for the 1-year time extension for CUP 364 for (ABC License Type 21, Off-Sale General) at the "AMPM" convenience store at 48055 Grapefruit Blvd. Applicant: GSC & Son Corporation

Adrian Moreno, Associate Planner, informed the Planning Commission that this item will be tabled due to CUP 364 not needing a 1-year time extension.

3. <u>Coachella Valley Apartments 1st 1-Year Time Extension Architectural Review No. 22-12</u>. The project consists of a proposed development of a 242-unit multi-family residential gated community with 15 buildings totaling 223,740 square feet on 9.7 acres of vacant land located near the southwest corner of Van Buren Street and Avenue 48. HCM Development (Applicant).

Item tabled.

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

4. <u>CUP 375 AR No. 23-14 – Coachella Islamic Center Parking Lot</u> for the construction of a parking lot to serve the existing building at 84650 Ave 49. Applicant: Islamic Society of Palm Springs.

Item tabled.

5. <u>La Mango – Type 40 ABC License</u> Conditional Use Permit No. 378 to allow liquor sales (ABC Type 40, On-Sale Beer) within a 1,711 SF restaurant located at 51704 Cesar Chavez Street. Applicant: Arlent Irani Torres Cardenas.

Adrian Moreno, Associate Planner, gave a brief presentation for the item. A copy of the presentation is on file in the Planning Division.

Public Hearing Opened at 6:14pm by Chair Hernandez

Arlent Irani Torres Cardenas, Applicant, spoke and provided comments.

Public Hearing Closed at 6:20pm by Chair Hernandez

IT WAS MOVED BY VICE CHAIR GONZALEZ AND SECONDED BY COMMISSIONER ARVIZU ADOPT RESOLUTION NO. PC 2024-09 APPROVING CONDITIONAL USE PERMIT NO. 378 TO ALLOW LIQUOR SALES (ABC TYPE 40, ON-SALE BEER) WITHIN A 1,711 SF RESTAURANT LOCATED AT 51704 CESAR CHAVEZ STREET WITH THE FOLLOWING REQUEST:

• REPORT BACK TO THE PLANNING COMMISSION IN 6 MONTHS ON CODE ENFORCEMENT EFFORTS TO BRING SITE CONDITIONS INTO COMPLIANCE.

Approved by the following roll call vote:

AYES: Commissioner Arvizu, Alternate Commissioner Fonseca, Commissioner Ramirez, Vice Chair Hernandez, Chair Gonzalez.

NOES: None. ABSTAIN: None.

ABSENT: Commissioner Murillo.

6. Encanto Project Change of Zone No. 24-01, Tentative Tract Map No 38429, Conditional Use Permit No. 376, Architectural Review No. 22-11, Environmental Assessment No. 22-04 a proposal to subdivide 19.2 acres into 111 single family lots with a minimum lot size of 4,500 square feet with seven single family residential production model homes located west of Van Buren Street, between Avenue 51 and Avenue 52, APN 779-360-001; Applicant: Joseph Rivani, Global Investment and Development.

Gabriel Perez, Development Services Director, gave a brief presentation for the item. A copy of the presentation is on file in the Planning Division.

Public Hearing Opened at 7:02pm by Chair Hernandez

Rich Malacoff, Applicant representative, spoke and provided comments.

Public Hearing Closed at 7:05pm by Chair Hernandez

IT WAS MOVED BY ALTERNATE COMMISSIONER FONSECA AND SECONDED BY VICE CHAIR GONZALEZ TO ADOPT RESOLUTION NO. PC 2024-10 APPROVING CHANGE OF ZONE NO. 24-01, TENTATIVE TRACT MAP NO. 38429, CONDITIONAL USE PERMIT NO. 376, ARCHITECTURAL REVIEW NO. 22-11, ENVIRONMENTAL ASSESSMENT NO. 22-04 A PROPOSAL TO SUBDIVIDE 19.2 ACRES INTO 111 SINGLE FAMILY LOTS WITH A MINIMUM LOT SIZE OF 4,500 SQUARE FEET WITH SEVEN SINGLE FAMILY RESIDENTIAL PRODUCTION MODEL HOMES LOCATED WEST OF VAN BUREN STREET BETWEEN AVENUE 51 AND AVENUE 52, APN: 779-360-001 WITH THE FOLLOWING CONDITION:

 NO OWNER SHALL BE PERMITTED TO RENT OR LEASE SUCH OWNER'S LOT FOR TRANSIENT OR HOTEL PURPOSES, OR FOR A PERIOD OF LESS THAN 30 DAYS UNLESS PERMITTED BY THE CITY TWO YEARS AFTER THE FINAL OCCUPANCY PERMIT IS ISSUED FOR TR. 38557, CUP 372, AND AR 23-13.

Approved by the following roll call vote:

AYES: Commissioner Arvizu, Alternate Commissioner Fonseca, Vice Chair Hernandez, Chair Gonzalez, Commissioner Ramirez.

NOES: None. ABSTAIN: None.

ABSENT: Commissioner Murillo.

INFORMATIONAL:

Director's Development Updates

- City of Coachella is recommended for a Community Resilience Centers Implementation Grant award. Final award decisions will be made at the SGC Council Meeting on April 24, 2024.
- City of Coachella will be receiving an Inland Empire Section APA award for the pre-approved ADU plan effort.
- Farmchella will be taking place Thursday, April 18, 2024.

• Carin Leon will be receiving the Key to the City on Saturday, April 20, 2024 at 10:30am.

Respectfully Submitted by,

Gabriel Perez

Planning Commission Secretary

ADJOURNMENT: 7:45 PM

Complete Agenda Packets are available for public inspection in the Development Services Department at 53-990 Enterprise Way, Coachella, California, and on the City's website www.coachella.org.

THIS MEETING IS ACCESSIBLE TO PERSONS WITH DISABILITIES



STAFF REPORT 5/1/2024

To: Planning Commission Chair and Commissioners

FROM: Adrian Moreno, Associate Planner

SUBJECT: <u>CUP No. 254 (Modification)</u>, AR No. 24-02 Coachella Gateway – a modification

to add a drive-thru multi-tenant commercial building within the Coachella Gateway shopping center approved under CUP No. 254 at the southwest corner of Ave 48 and Grapefruit Boulevard, adjacent to the Dillon Bridge. Applicant:

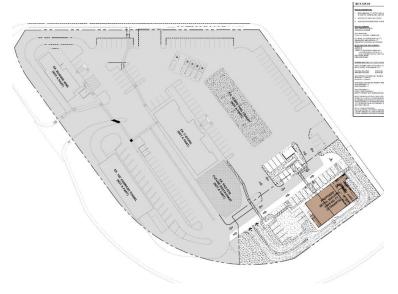
Chandi & Karan LLC

STAFF RECOMMENDATION:

Staff recommends that the Planning Commission adopt Resolution No. PC2024-11 adopting CUP 254 (MOD) and AR No. 24-02, subject to the findings and conditions attached to this staff report.

BACKGROUND:

On December 12, 2012, the Planning Commission approved Conditional Use Permit Nos. 254 and 256, Tentative Parcel Map 36481, and Architectural Review No. 12-07 for the refurbishment of an existing service station/mini-market and the construction of four new commercial buildings on 4.85 acres on the southwest corner of Avenue 48 and Grapefruit Boulevard. On June 3, 2015, Planning approved a Commission 24-month retroactive time extension for the project. **Planning** October 21, 2015, Commission approved a modification to the original approved project to add a new drive-thru restaurant and other site



modifications. On June 7, 2017, Planning Commission approved a sign program for the site. On April 19, 2023, Planning Commission approved Conditional Use Permit No. 364 to allow ABC License Type 21 at the AMPM at the subject site at 48055 Grapefruit Blvd.

The applicant requests a second modification to the site to add a drive-thru for a multi-tenant commercial building originally approved as a stand-alone sit-down restaurant under CUP No. 254.

The building was originally approved for a sit-down restaurant. The building is on Grapefruit Boulevard, adjacent to the Dillon Bridge.

DISCUSSION/ANALYSIS:

The surrounding land uses and zoning designations are as follows:

North: Tower Market (City of Indio).

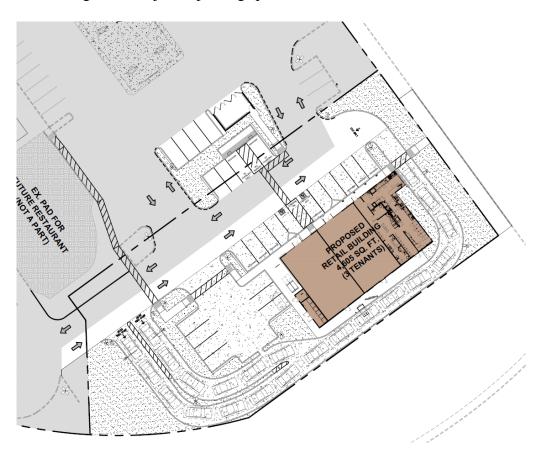
South: Dillon Bridge (C-G, General Commercial).East: Grapefruit Boulevard and Union Pacific Railroad

West: Dillon Road/Bridge and vacant lot/old Glenroy Hotel Project site (C-G, General

Commercial)

Site Plan

The exhibit below illustrates the proposed modification to the site plan layout for the proposed 4,605 SF drive-thru restaurant a modification from the original plan for a sit-down restaurant. A double-drive thru entrance is proposed that merges into one drive thru-lane and wraps around the south and east sides of the building. There project will construct 26 new off-street parking spaces, a total of 20 are at the north and west ends of the building. There is a new landscape island to the north of the building with 6 adjacent parking spaces, and a new trash enclosure.



The building is proposed as a 4,605 SF retail shell building consisting of 2 retail tenants and 1 quick-stop retail tenant. There is no proposed floor plans for the 2 retail tenants at this time, however the floor plan for the 1 quick stop retail tenant proposes 1,299 SF of non-customer area and 107 SF of customer area. The drive-thru serves the quick-stop retail tenant, which includes a walk-up window intended for quick stop rather than a customer area with seating and tables customary of other Coachella drive thru fast food establishments.





Figure 1 East Elevation - Facing Grapefruit Boulevard

Figure 2 Northeast Elevation

The applicant proposes high quality elevations for the project with both horizontal and vertical modulation, a compatible desert color scheme, and textured exterior finishes. The applicant proposes a metal canopy over the drive-thru window and building entrances at the north elevation. The applicant also proposes an 8 foot tall trash enclosure structure with steel doors and CMU wall. The trash enclosure provides a high quality design with a color scheme that is compatible with buildings on-site.

LANDSCAPING

The project proposes a new conceptual landscape plan for the parcel of the subject building. The on-site landscaping includes landscaping around the building, landscape medians, and landscape common area. Landscaping is composed of various shrubs, palm trees, shade trees adjacent to parking, vines adjacent to the trash enclosure, and a landscaped median between the proposed double drive-thru lane. All landscape areas around the subject building provide 3/4 crushed rock with several desert shrub varieties that include red bird of paradise and desert accents that include red yucca. Palm trees are proposed at common area landscape areas adjacent to the public rightof-way to match existing, and 24" box mulga trees provide shade to on-site parking near the subject building. There is a landscape median at the two entry drive-thru entrance proposed with 34 inch gravel and lantana shrubs. Staff will require additional on-site landscape improvements that include vines adjacent to new structures, increasing the landscaped area that adjoins the existing car wash, and landscape improvements to the entire center to provide a unified look that may include adding shade trees, 34 inch rock, and/or large stones. Staff discussed adding conditions of approval for the project to improve the bridge dirt berm area. Instad staff request the applicant provide complete access to the City to south berm adjacent to the Dillon Bridge to ensure improvements can be made by the City.

As a condition of approval, a final landscape, lighting, and irrigation plan will be required for the

project. As a condition of approval, Staff will require landscape improvements for the entire center along landscape areas visible from the public R.O.W.

SIGNAGE

The applicant proposes a modification to the sign program previously approved for the center. The proposed modified sign program provides a new modified site plan, and new elevations that show signage locations for the modified 4,605 SF subject building and modified pylon directory sign. The sign program is amended to add additional regulations pertaining to roof signs, exterior merchandise signs, electronic message board signs, and more regulations that are pulled from the Coachella Sign Ordinance. There are specific regulations on pylon directory signs prohibiting flat signs, moving electronic signs, and that lights shall only shine through individual letters.

The previously approved 25 foot tall directory sign incorporated a plastic and aluminum design at approximately 135 SF of tenant signage at each sign face and was never completed. The new proposed 25 foot tall directory sign proposes a stucco textured finish and a cement board finish with a wooden appearance. There is approximately 86 SF of tenant signage at each sign face, with approximately 38 SF of sign face dedicated to City of Coachella branded signage. The improved signage materials, color scheme, and reduction of tenant signage at each sign face provides a high quality design that is an improvement from the original design. Also, as a condition of approval, the existing on-site billboard sign adjacent to Grapefruit Boulevard shall be removed.

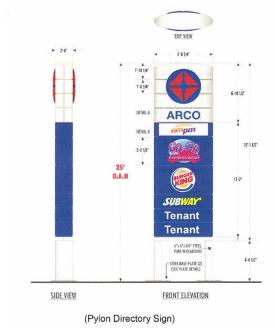


Figure 2 Old Design Directory Sign

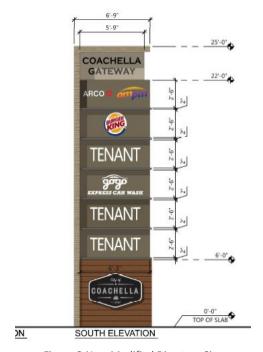


Figure 3 New Modified Directory Sign

PARKING

The applicant proposes 26 new parking spaces to serve the subject commercial building. The project proposes 3 tenants, with 1 of those tenants being a quick stop retail (QSR) restaurant. The

restaurant requires 10 parking spaces. The other two tenants parked at the standard commercial retail ratio will require 12 parking spaces. The building requires 22 parking spaces, and 26 are provided, the amount of proposed parking is sufficient for the proposed use. There may be tenants in the future that require a higher parking ratio, for this reason the center is conditioned to allow the Director of Development Services to require parking improvements that meet the parking needs of the site.

ENVIRONMENTAL REVIEW:

The Planning Division has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as "In-Fill Development" (CEQA Guidelines, Section 15301). The proposed project consists of the development within city limits on a project site of no more than five acres substantially surrounded by urban uses. This was a previously approved project, and this is a modification of that approved project.

ALTERNATIVES

- 1) Adopt Resolution No. PC2024-11 approving Conditional Use Permit 254 (Modification) and Architectural Review No. 24-02 with the findings as recommended by Staff.
- 2) Adopt Resolution No. PC2024-11 with the findings and conditions as recommended by Staff with <u>modifications</u> as proposed by the Planning Commission.
- 3) <u>Not</u> approve Resolution No. PC 2024-11 and request that staff prepare a Planning Commission Resolution for denial of Conditional Use Permit 254 (Modification) and Architectural Review No. 24-02.
- 4) Continue this item and provide staff and the applicant with direction.

RECOMMENDED ALTERNATIVE(S):

Staff recommends that the Planning Commission approve Alternative #1.

Attachments:

- 1. Resolution PC2024-11
 - a. Exhibit A Conditions of Approval
 - b. Exhibit B 2015 Conditions of Approval
 - c. Exhibit C Modified Development Plan Set
 - d. Exhibit D Original Development Plan Set
- 2. Aerial Photo
- 3. Outside Agency Comments

RESOLUTION NO. PC 2024-11

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF COACHELLA, CALIFORNIA APPROVING CONDITIONAL USE PERMIT MODIFICATION AND ARCHITECTURAL REVIEW FOR THE ADDITION OF A DRIVEWAY AND FOR DEVELOPMENT OF THE 4,605 SF BUILDING LOCATED AT THE SOUTHWEST CORNER OF GRAPEFRUIT BOULEVARD AND AVENUE 48, ADJACENT TO THE DILLON BRIDGE. CHANDI & KAREN LLC, APPLICANT.

- **WHEREAS**, Chandi and Karan LLC filed an application for a Conditional Use Permit Modification and Architectural Review for the addition of a driveway and for development of the 4,605 SF building located at the southwest corner of Grapefruit Boulevard and Avenue 48, adjacent to the Dillon Bridge (APN: 603-220-056, 603-220-057, 603-220-058, 603-220-059); and,
- **WHEREAS,** on November 7, 2012, the Planning Commission conducted a duly noticed public hearing on the originally approved project.
- **WHEREAS,** on October 21, 2015, the Planning Commission conducted a duly noticed public hearing on the modification on the approved project and adopted, with the conditions as set forth in those resolutions attached as Exhibit "B".
- **WHEREAS**, on May 1, 2024, the Planning Commission conducted a duly noticed public hearing on the proposed second modification to the project in the subject application, considered written and oral comments, and facts and evidence presented by the applicant, City staff, and other interested parties; and
- **WHEREAS**, the Applicant and members of the public were present and were afforded an opportunity to testify regarding the Project; and,
- **WHEREAS**, the proposed use is necessary or desirable for the development of the community, is consistent with the objectives of the City's General Plan, and is not detrimental to the existing uses or the uses specifically permitted in the zone in which the proposed use is to be located; and,
- **WHEREAS**, the proposed site is adequate in size and shape to accommodate the proposed development; and,
- **WHEREAS**, the site for the proposed use relates properly to streets which are designed to carry the type and quantity of traffic to be generated by the proposed use; and,
- **WHEREAS**, the proposed project is exempt from the California Environmental Quality Act, as amended; and,
- **WHEREAS**, the conditions of approval as stipulated by the City are necessary to protect the public health, safety and welfare of the community.

NOW, THEREFORE, BE IT RESOLVED, THE PLANNING COMMISSION OF THE CITY OF COACHELLA, CALIFORNIA DOES HEREBY FIND DETERMINE AND RESOLVE AS FOLLOWS:

SECTION 1. Recitals. The above recitals are true and correct and incorporated herein as findings of fact.

SECTION 2. CEQA Findings.

Based upon its review of the entire record, including the Staff Report, any public comments or testimony presented to the Planning Commission, and the facts outlined below, the Planning Commission hereby finds and determines that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as an "In-Fill Development" project (CEQA Guidelines, Section 15332) as the proposed project consists of the development within city limits on a project site of no more than five acres substantially surrounded by urban uses. This was a previously approved project, and this is a modification of that approved project and architectural review for new proposed elevations.

SECTION 3. Conditional Use Permit, Architectural Review Findings

With respect to Conditional Use Permit No. 254 Modification and Architectural Review No. 24-02, the Planning Commission finds as follows for the addition of a driveway and for development of the 4,605 SF building located at the southwest corner of Grapefruit Boulevard and Avenue 48, adjacent to the Dillon Bridge.

- 1. The proposed conditional use permit modification and architectural review are consistent with the General Plan and the City of Coachella Official Zoning Map governing the site. The subject site is a 4.85-acre site on four parcels with adequate access and lot dimensions to allow for the intended commercial development in a manner consistent with the Suburban Retail District land use designation of the General Plan and General Commercial Zoning Designation.
- 2. The proposed use will be located, designed, constructed, operated and maintained so as to be compatible with the existing or intended character of the general vicinity and shall not change the essential character of the same area. The modification to the project design to include addition of a driveway and development of the 4,605 SF building would be located in the General Commercial area and is compatible with existing adjacent uses that are similar land uses.
- 3. Consideration was given to harmony in scale, bulk, coverage and density, to the availability of public facilities and utilities, to harmful effect, in any, upon desirable neighborhood character, to the generation of traffic and the capacity of surrounding streets, and to any other relevant impact of the development. The proposed project uses are compatible uses with existing similar uses in the area.

4. Where the proposed use may be potentially hazardous or disturbing to existing or reasonable expected neighboring uses, it must be justified by the common public interest as a benefit to the community as a whole. The Development Services Department does not anticipate any potentially hazardous or disturbing impacts on existing or neighboring uses. The proposed use is anticipated to add an amenity to City residents and to the entire Coachella Valley in providing an anchor for the commercial corridor which would provide jobs and increase the City's sales tax revenues.

SECTION 4. Location and Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings are based are located at Coachella Civic Center. The Development Services Director is the custodian of the record of proceedings.

SECTION 5. Execution of Resolution. The Chairman shall sign this Resolution and the City Clerk shall attest and certify to the passage and adoption thereof.

SECTION 6. Planning Commission Approval Based on the foregoing recitals and findings above, and the written and oral comments, facts and evidence presented, the City of Coachella Planning Commission hereby approves Conditional Use Permit No. 254 Modification and Architectural Review No. 24-02 for the Coachella Gateway project subject to and amended by conditions of approval in "Exhibit A" and includes conditions of approval in "Exhibit B", and the modified plans as set forth in "Exhibit C", a modification of the original approved plans set forth in "Exhibit D".

PASSED APPROVED and ADOPTED this 1st day of May 2024.

Coachella Planning Commission ATTEST: Gabriel Perez Planning Commission Secretary APPROVED AS TO FORM: Carlos Campos	Jason Hernandez, Chairperson	
Gabriel Perez Planning Commission Secretary APPROVED AS TO FORM:	Coachella Planning Commission	
Planning Commission Secretary APPROVED AS TO FORM:	ATTEST:	
Planning Commission Secretary APPROVED AS TO FORM:		
APPROVED AS TO FORM:	Gabriel Perez	
	Planning Commission Secretary	
Carlos Campos	APPROVED AS TO FORM:	
Carlos Campos		
Carros Campos	Carlos Campas	
City Attorney	•	

I HEREBY CERTIFY that the foregoing Resolution No. PC-2024-11, was duly adopted at a regular meeting of the Planning Commission of the City of Coachella, California, held on the 1st day of May, by the following roll call vote:

AYES:
NOES:
ABSENT:
ABSTAIN:
Gabriel Perez
Planning Commission Secretary

Exhibit A – Resolution No. PC2024-11

CONDITIONS OF APPROVAL FOR CONDITIONAL USE PERMIT 254 (MODIFICATION) AND ARCHITECTURAL REVIEW NO. 24-02 COACHELLA GATEWAY

General Conditions

- 1.) The applicant shall comply with <u>all</u> requirements and conditions of approval as included in CUP 254, CUP 256, CUP 364, Tentative Parcel Map No. 36481, AR 12-07, and any modifications or extensions thereof, as listed in those Notice of Actions for each respective approval and City Council Resolution 2012-49 and Reso. No. PC2023-08.
- 2.) The applicant shall defend, indemnify and hold harmless the City of Coachella, its officials, officers, employees, and agents from and against any claim, action, or proceeding against the City, its officials, officers, employees or agents to attack, set aside, void or annul any project approval or condition of approval of the city concerning this project, including but not limited to any approval or condition of approval or mitigation measure imposed by the City Council or Planning Commission. The City shall promptly notify the applicant of any claim, action, or proceeding concerning the project and the City shall cooperate fully in the defense of the matter. The City reserves the right, at its own option, to choose its own attorney to represent the City, its officials, officers, employees and agents in the defense of the City Attorney, within five days of the effective date of this approval
- **3.**) Prior to Certificate of Occupancy, missing offsite improvements shall be installed along Grapefruit Boulevard. Applicant shall install and continue sidewalk along the southeastern portion of the commercial center and continued under Dillon Bridge.
- **4.**) Shade Trees shall be implemented along public right of way to the satisfaction of the Development Services Director.
- **5.) General Landscaping**. The applicant shall submit a final landscape, lighting, and irrigations plans for review and approval by the Development Services Department with added low intensity landscape lighting. The final landscape plan shall be for the entire center, including existing and new landscape common areas along public R.O.W, landscape medians on Grapefruit Boulevard, site landscaping for the 4,605 SF retail building adjacent to the Dillon Bridge, etc.
- **6.) New Landscaping**. Applicant shall provide ¾ inch rock for all new proposed landscape areas. Shade trees shall be implemented along the public R.O.W. Up lighting shall be provided for new trees. Landscape areas shall include a diversity of shrubs including flowering varieties. Additionally, the site landscaping west of the building 4,605 SF building shall extend to landscape the unlandscaped areas adjacent to the existing car wash site. Vines (wired) shall be attached to each CMU wall face of the trash enclosure. Vines wired to the building shall be implemented on the south side of the 4,605 SF building to the satisfaction of the Development Services Director.

- **7.) Berm Landscaping**. The applicant shall provide the City with complete access for entry to the entire south berm area adjacent to the Dillon Bridge for purposes of maintenance and improvements to the berm area. The applicant shall include the berm area enter into project's Landscape Maintenance Agreement with the City of Coachella for the maintenance of the berm to include weed abatement.
- 8.) Existing Landscaping. Applicant shall rehabilitate existing landscape areas. Landscape areas shall be rehabilitated along all landscape areas visible from the public R.O.W along Grapefruit Boulevard and Avenue 48. To the satisfaction of the Development Services Director, shade trees shall be implemented along the public R.O.W, and up lighting shall be provided for new trees. Applicant shall replace missing, dead, or decaying landscaping, refresh decorative gravel where sparse, and shall add matching 34 inch gravel in all landscape areas visible from the public R.O.W along Grapefruit Boulevard and Avenue 48 to the satisfaction of the Code Enforcement Manager and Development Services Director prior to Certificate of Occupancy. The applicant shall submit a final landscape plan for the entire center, to be approved by the Development Services Director, that create consistency across the entire center, which may include large stone in street-adjacent corner areas, and some 34 inch gravel along common area landscape areas.
- **9.) General Landscaping**. All landscaping, lighting, and irrigation related conditions of approval shall be met to the satisfaction and discretion of the Development Services Director and City Engineer.
- **10.**) Landscape Maintenance Agreement. Applicant shall enter into a Landscape Maintenance Agreement with the City of Coachella for the commercial center landscape areas prior to Certificate of Occupancy for the multi-tenant commercial building.
- 11.) The pad owner of the proposed 4,695 sq. ft. building shall address any traffic back-up issues that is observed by the City Engineer to regularly conflict with/obstruct on-site circulation from the commercial center to the satisfaction of the City Engineer and Fire Department. Such improvements may include reconfiguration of the queuing drive through lanes or other means that is acceptable to the City Engineer.
- **12.**) Applicant shall submit the updated sign program for stamped approval by the Development Services Department. The applicant shall update the sign program to be complete that includes complete signage details for the center. The site plan on the sign program shall show the repurposed pylon sign and the existing monument price signage on site.
- **13.**) Existing on-site billboard on Grapefruit Blvd shall be removed prior to Certificate of Occupancy.
- **14.)** The construction improvements to the existing abandoned pylon directory sign, pursuant to the approved updated sign program, shall be completed prior to final inspection of new commercial multi-tenant building.
- **15.)** At the discretion of the Development Services Director, the Director may require parking improvement measures if it is determined that the parking impacts/demand from the

- businesses at the Coachella Gateway Center do not meet the current on-site or off-site parking supply.
- **16.)** Completion of Landscape Improvements on-site and removal or rehabilitation of the pylon directory sign may satisfy Condition of Approval #4 of PC Resolution PC2023-08.
- 17.) The owner will be required to consult with the Utilities Manager and Environmental Programs Coordinator regarding water and sewer connections, and utility connection impact fees, and any proposed sewer dump connections.
- **18.)** If it is observed by the City Engineer that the drive through lane capacity is consistently exceeded so as to create a conflict with on-site circulation, the applicant upon notice by the City Engineer shall propose measures to reduce such impacts such as placement of employees to expedite drive through orders, reconfiguration of the existing drive through lane to improve capacity, or other measures that best mitigate such conflicts.

Building Department

- 19.) Provide access from building to Public Right of Way on Grapefruit Blvd.
- **20.**) Proposed conceptual drawing are missing California Green Building Standard Code Requirements for the following features:
 - 5.106.4.1.1 Short-Term Bicycle Parking
 - 5.106.4.1.2 Long-Term Bicycle Parking
 - 5.106.12 Shade Trees for both (5.106.12.1) Surface Parking Areas, (5.106.12.2) Landscape Areas and (5.106.12.3) Hardscape Areas.
- **21.**) Provide a second egress door at the electrical room.

Utilities

- **22.**) The applicant shall submit water and sewer plans for approval. The proposed project shall be required to connect to the City's public water and sewer system.
- 23.) Water & Sewer impact fees to be paid prior to final approval of plans.
- **24.**) The project will require a Water Quality Management Plan (WQMP).
- **25.**) Based on findings of the Source Control application, all modifications shall be completed prior to issuance of a certificate of occupancy.
- **26.**) The applicant shall install above ground "Double Check Detector Assembly" DCDA for fire system to protect water supply from contamination or pollution.
- **27.**) Backflow devices will require Reduced Pressure Principle Device (RP) within 12 inches of water service installed to protect water supply from contamination or pollution.
- **28.**) A separate AMI 4-G metering system for each building shall be installed.

- **29.**) A separate AMI 4-G water service meter for the irrigation system shall be installed.
- **30.**) All facilities will be required to submit a source control survey.
- **31.**) Detailed plumbing and mechanical plans shall be submitted.
- **32.**) The project must implement the State's drought mandate which prohibits irrigation with potable water outside newly constructed homes and buildings that is not delivered by drip or microspray systems.
- **33.**) If there is an existing sewer connection it will have to be inspected via camera at the project's expense to determine acceptance.
- **34.**) Water and sewer design engineer shall check in with the Utilities Department for material approvals
- **35.**) All water services shall be copper tubing
- **36.**) Plumbing blueprints depicting water, sanitary, and grease waste line. (Grease waste lines if applicable)
- **37.**) Water and sewer point of connection to the City's utilities.
- **38.**) Plumbing code fixture schedule with total DFU's listed.
- **39.**) Complete wastewater discharge survey.
- **40.**) Pretreatment equipment/Sample box: (if applicable) Provide manufacture scarifications and sizing chart.
 - o Pretreatment Equipment Minimum Requirements:
 - 750 gal minimum
 - Clean outs and vents before and after pretreatment equipment
 - Sample box immediately downstream of the pretreatment equipment
 - Multi suite buildings requesting pretreatment installation require separate sewer lateral connection per pretreatment device.
- **41.**) Stormwater runoff must be contained within the property.

Engineering

PRIOR TO APPROVAL OF ENGINEERING PLANS or ISSUANCE OF ENGINEERING PERMITS:

GENERAL:

- 42.) A focused Traffic Analysis (TA) shall be prepared for the project by an appropriately licensed professional engineer. Prior to the preparation of the TA, the engineer shall submit a scoping letter for the TA for the City Engineer's approval. The TA shall include but not limited to identification of trip generation, traffic distribution and impact on existing transportation facilities and at time of General Plan build-out, all relevant, ingress and egress movements, lines of sight, queuing analysis, and alignment studies (preliminary signing and striping plan). Applicant shall obtain approval of site access and circulation from the Fire Marshall.
- **43.**) A preliminary soils report shall be prepared for the project by an appropriately licensed professional engineer. At a minimum, the soils report shall provide specific analyses and recommendations for grading, pavement structural sections, and infiltration.
- **44.**) A comprehensive drainage report, prepared by California Registered Civil Engineer, shall be submitted for review and approval by the City Engineer prior to issuance of any permits. The report shall contain pre- and post-development hydrology maps showing on-site and off-site tributary drainage areas and shall be prepared in accordance with the requirements of the Riverside County Flood Control District. Adequate provisions shall be made to accept and conduct the existing tributary drainage flows around or through the site in a manner which will not adversely affect adjacent or downstream properties. If the design of the project includes a retention basin, it shall be sized to contain the runoff resulting from a 10-year storm event and the runoff from a 100-year storm event shall be contained within basin with shallow ponding (3.5' max.). The basin shall be designed to evacuate a 10-year storm event within 72 hours. The size of the retention basin(s) shall be determined by the hydrology report and be approved by the City Engineer. Retention basin shall be provided with a minimum of 2.00 feet sandy soil if determined to contain silt or clay materials. Maximum allowable percolation rate for design shall be 10 gal./s.f./day unless otherwise approved by the City Engineer. A percolation test for this site is required to be submitted. A combination drywell vertical drain field shall be constructed at all points where runoff enters the retention basin. Drywell & vertical drain field design shall be based on soils borings made at the proposed drywell locations after the retention basins have been rough graded. Minimum depth shall be 45-feet. A log that includes sieve analysis for each strata of the borings shall be submitted to the City Engineer for confirmation of depth of the vertical drain fields. Underground retention under the proposed parking area will be considered as an alternative to surface retention subject to the approval of the City Engineer.
- **45.**) Prepare and record necessary drainage easements to implement the project in accordance with drainage law.
- **46.**) A storm water quality management plan shall be prepared for the project by California Registered Civil Engineer in compliance with NPDES and State Water Quality Control

- Board regulations. The project shall be designed to specify preferential use of Low Impact Development Best Management Practices that reduce pollutants and runoff volume.
- **47.)** Applicant shall comply with the valley wide NPDES permit requirements including but not limited to submittal of a WQMP for plan review accompanied by a \$3,000 plan check deposit for approval including executed maintenance agreement. All unused plan check fees will be refunded to the applicant upon approval of the Final WQMP.
- **48.**) The developer shall submit a Fugitive Dust Control and Erosion Control plan in accordance with Guidelines set forth by CMC and SCAQMD to maintain wind and drainage erosion and dust control for all areas disturbed by grading. Exact method(s) of such control shall be subject to review and approval by the City Engineer. No sediment is to leave the site. Additional securities, in bond form, in amount of \$2,000.00 per acre of gross area, and a one-time cash deposit of \$2,000.00 are required to insure compliance with this requirement. No work may be started on or off site unless the PM-10 plan has been approved, the original plans, and executed dust control agreement, are filed in the engineering department at the City of Coachella.
- **49.**) Applicant shall submit for review and approval by the City Engineer all documents related to any existing and proposed on-site and off-site easements that may affect the development of the site. All easements shall be identified on the engineering plans.
- **50.**) Site access improvements shall be in conformance with the requirements of Title 24 of the California Administrative Code. This shall include access ramps for off-site and on-site streets as required.
- **51.**) Applicant shall obtain approval of site access and circulation from Fire Marshall.
- **52.**) The applicant shall provide necessary utility easements for IID and underground overhead distribution lines within the project boundaries. Applicant shall submit to the City a letter from IID that satisfies this requirement.
- **53.**) The applicant shall pay all necessary plan check, permit and inspection fees. Fees will be determined when plans are submitted to the City Engineering Department for plan check.

ROUGH GRADING:

- **54.**) Prepare and submit rough grading and erosion control plans for the project.
- 55.) The project's soils engineer shall certify to the adequacy of the grading plan.
- 56.) All projects developing one (1) acre or more of total land area, or which are part of a larger phased development that will disturb one acre of land, are required to obtain coverage under the State Water Resources Control Board's (SWRCB) General Permit for storm water discharges associated with construction activity. Proof of filing a Notice of Intent (NOI) with the SWRCB for coverage under this permit is required. The Waste Discharger's Identification Number (WDID), issued by the SWRCB, must be shown on the grading plans. The project's Storm Water Pollution Prevention Plan shall be submitted for the City's review and approval.

PRECISE GRADING:

- **57.)** A precise grading/improvement plan, prepared by a California Registered Civil Engineer, showing building footprints, pad elevations, finished grades, drainage routes, retaining walls, erosion control, slope easements, and all other pertinent information shall be submitted for review and approval by the City Engineer.
- **58.**) Rough grading shall be certified by the project soils engineer prior to issuance of a permit for precise grading or building construction.
- **59.**) Provide and record a reciprocal use and maintenance agreement to assure common ingress and egress and joint maintenance of all common access, parking areas and drives.
- **60.**) If applicant is planning to build a wall, separate permits shall be required for wall construction. The maximum height of any wall shall be limited to six (6) feet as measured from an average of the ground elevations on either side.
- **61.**) Applicant shall evaluate existing conditions of Storm Drain Structures and retention basins located on the project, and if applicant needs to change the location or size of any Storm Drain structures or retention basins a Drainage Report, prepared by California Registered Civil Engineer, shall be submitted for review and approval by the City Engineer prior to issuance of any permits.

STREET IMPROVEMENTS:

- **62.**) Street improvement plans prepared by a California Registered Civil Engineer shall be submitted for review and approval by the City Engineer. All street improvements including street lights shall be designed and constructed in conformance with City Municipal Code, General Plan, and Standards and Specifications. Street flow line grade shall have a minimum slope of 0.35 %.
- **63.**) Applicant shall construct all off-site and on-site improvements including street pavement, curb, gutter, sidewalk, street trees, perimeter walls, perimeter landscaping and irrigation, storm drain, street lights, and any other incidental works necessary to complete the improvements. Driveways shall conform to City of Coachella standards for commercial driveways with a minimum width of 24.00 feet and curbed radius entrances.
- **64.**) Applicant shall construct and dedicate the following streets and street improvements to conform to the General Plan and/or requirements of Traffic Study.
 - 1) Grapefruit Boulevard- Public Roadway as shown on the RAC and per these comments shall include the following:
 - a. Dedication of land along Southbound lane within project limits is required. This street is classified as Major Arterial with Bicycle Lanes with 118 feet of right-of-way as per City of Coachella General Plan.
 - b. Existing street measured at Center line to westerly curb is 46-foot.
 - c. Applicant shall install all sidewalk, curb and gutter transitions to uniformly connect to existing adjacent improvements and coordinate installation and/or relocation of fire hydrants, water

- meters, storm drain, wells, streetlights, landscape and all other appurtenances as required to the satisfaction of the City Engineer.
- d. Applicant shall construct all appurtenant roadway components within project limits such as, but not limited to: sidewalk, ADA ramps, Traffic control striping, legends, Traffic control signs, Street Lights and street name signs to the satisfaction of the City Engineer.
- e. Applicant shall extend new sidewalk on Grapefruit Blvd. southbound lane from project limits to south side of Dillon Bridge abutment to the satisfaction of the City Engineer.
- f. Applicant shall installed hardscape on exiting Grapefruit Blvd. in front of project limits.
- g. Applicant shall landscape Dillon Bridge slope that is located at southerly side of project location to the satisfaction of the City Engineer.

SEWER and WATER IMPROVEMENTS:

- **65.**) Sewer & Water Improvement Plans prepared by a California Registered Civil Engineer shall be submitted for engineering plan check and City Engineer approval.
- **66.**) Applicant shall construct all off-site and on-site water improvements and any other incidental works necessary to complete the improvements. Size and location of sewer and water improvements shall be approved by the City Engineer.

PRIOR TO ISSUANCE OF BUILDING PERMITS:

- **67.)** A final soils report, compaction report and rough grading certificate shall be submitted and approved prior to issuance of any building permits.
- **68.**) Prior to issuance of building permits, all required public improvements, including landscaping and lighting of the retention basins, and landscaped areas along the exterior streets, shall be completed or secured with appropriate sureties to the satisfaction of the City Engineer. An engineering final inspection is required. "As-built" plans shall be submitted to and approved by the City Engineer. Prior to acceptance of the improvements by the City, such plans, once approved, shall be given to the city on compact disk in AutoCad format. All off-site and on-site improvements shall be completed to the satisfaction of the City Engineer prior to acceptance of improvements for maintenance by the City.
- **69.**) The applicant's Civil Engineer shall field verify and certify that all BMPs are designed, constructed, and functional in accordance with the approved WQMP.

PRIOR TO RELEASE OF OCCUPANCY PERMITS/ACCEPTANCE OF PUBLIC IMPROVEMENTS:

70.) Prior to issuance of certificate of occupancy, all public improvements, including landscaping and lighting of the retention basins, and landscaped areas along the exterior streets, shall be completed to the satisfaction of the City Engineer. An engineering final inspection is required. "As-built" plans shall be submitted to and approved by the City Engineer. Prior to acceptance of the improvements by the City, such plans, once approved, shall be given to the city on compact disk in AutoCad format. All off-site and on-site

improvements shall be completed to the satisfaction of the City Engineer prior to acceptance of improvements for maintenance by the City.

FIRE CONDITIONS

- **71.**) Fire Protection Water Supplies/Fire Flow Minimum fire flow for the construction of all buildings is required per CFC Appendix B. Prior to building permit issuance for new construction, the applicant shall provide documentation to show a (existing/proposed) water system capable of delivering the required fire flow. Specific design features may increase or decrease the required fire flow.
 - a. Will Serve Letters from the responsible water purveyor are required prior to a map recordation.
- 72.) Fire Protection Water Supplies/Hydrants The minimum number of fire hydrants required, as well as the location and spacing of fire hydrants, shall comply with CFC Appendix C and NFPA 24. Fire hydrants shall be located no more than 400 feet from all portions of the exterior of the building along an approved route on a fire apparatus access road, unless otherwise approved by the Fire Department. Where new water mains are extended along streets where hydrants are not needed for protection of structures, standard fire hydrants shall be provided at spacing not to exceed 1000 feet along streets for transportation hazards. Fire hydrants shall be at least 40 feet from the building it is serving. A fire hydrant shall be located within 20 to 100 feet of the fire department connection for buildings protected with a fire sprinkler system. The size and number of outlets required for the approved fire hydrants are 4" x 2 ½" x 2 ½" (super hydrant). Reference CFC as amended and NFPA 24.
- 73.) Fire Department Access Fire apparatus access roads shall be provided to within 150 feet of all exterior portions of buildings, unless otherwise approved by the Fire Department. Fire apparatus access roads shall have an unobstructed width of not less than 24 feet. Dead-end fire apparatus access roads in excess of 150 feet shall be provided with an approved turn around. The minimum required turning radius of a fire apparatus access road is 38 feet outside radius and 14 feet inside radius. The construction of the fire apparatus access roads shall be all weather and capable of sustaining 75,000 lbs. Unless otherwise approved, the grade of a fire apparatus access road shall not exceed 16 percent and the cross slope shall not exceed 2.5 percent. The angles of approach and departure for fire apparatus access roads shall be a maximum of 6 percent grade change for 25 feet of approach/departure. Reference CFC as amended and Riverside County Fire Department Policies and Standards.
 - a. Fire Lane marking: Identification and marking of fire lanes, including curb details and signage shall be in compliance with Riverside County Fire Department Standards.
- **74.**) Fire Department Building Construction Plan Review Submittal of construction plans to the Fire Department will be required. Final fire and life safety conditions will be addressed when the Fire Department reviews the plans. These conditions will be based on California Fire Code, California Building Code (CBC), and related codes/standards adopted at the time of construction plan submittal. Reference CFC as amended.
- **75.**) Fire Sprinkler System All new commercial buildings and structures 3,600 square feet or larger will be required to install a fire sprinkler system. Reference CFC as amended.

- **76.**) Fire Alarm and Detection System A water flow monitoring system and/or fire alarm system may be required as determined at time of building construction plan review. Reference CFC as amended.
- 77.) Traffic Calming Devices Requests for installation of traffic calming designs/devices on fire apparatus access roads shall be submitted for evaluation purposes, resulting in denial or approval by the Fire Code Official. Reference CFC as amended.
- **78.**) Gate Access All electronically operated gates shall be provided with Knox key switches and automatic sensors for access. These gates shall be provided with access to gate equipment or another method to open the gate if there is a power failure. (Manual gates shall not be locked unless a Knox padlock or Knox Box containing the key to the lock is installed in an approved location on the approach side of the gate). A pedestrian gate, if used to provide access, shall be a minimum 3 feet wide and provided with a Knox Box/Padlock if locked. Reference CFC as amended.
- **79.**) Water Plans If fire hydrants are required to be installed, applicant/developer shall furnish the water system fire hydrant plans to the Fire Department for review and approval prior to building permit issuance. Plans shall be signed by a registered civil engineer, and shall confirm hydrant type, location, spacing, and minimum fire flow. Once plans are signed and approved by the local water authority, the originals shall be presented to the Fire Department for review and approval. Reference CFC as amended.

CHANDI GROUP USA – GATEWAY PROJECT

CONDITIONS OF APPROVAL FOR CONDITIONAL USE PERMIT NO. 254 (MODIFICATION):

- 1. This Conditional Use Permit 254 (modification) hereby relinquishes, and replaces in their entirety, the prior approved phasing plan and related conditions of approval, in order to allow two drive-thru restaurants and a single phasing plan for the proposed project. A building permit must be obtained and construction diligently pursued for all phases of the development within 12 months of the effective date, unless a time extension is granted by the Planning Commission.
- 2. The applicant shall incorporate an outdoor dining area with permanent seating and shade structures to serve the patrons of the drive-thru restaurant pad located at the northern portion of the site, subject to review and approval by the Development Services Director.
- 3. The applicant shall provide a lighting plan showing adequate illumination of all exterior parking lots, and common areas of the site, subject to review by the Development Services Department.
- 4. A maximum of 20% of window area may be used for signs. All banners and temporary signs shall be prohibited except as allowed under the City's sign regulations for special sales and promotional events.
- 5. All improvements for the car wash tunnel building, subject to Architectural Review No. 12-07, shall be constructed before the operation of the facility. Operational restrictions of the car wash imposed by Conditional Use Permit No. 256, are incorporated herein as listed below.
 - a. The car wash facility and vacuum area may operate between the hours of 5 AM to 10 PM daily.
 - b. An on-site car wash attendant shall be available during regular business hours of the car wash.
 - c. Wash water from the car wash must be recycled and cannot be discharged to the sewer system.
 - d. The car wash and vacuum area shall be operated as an automated car wash and maintained regularly, free of trash and debris.
- 6. Trash receptacles shall be provided in areas convenient for customers and shall be emptied daily.

CONDITIONS OF APPROVAL FOR ARCHITECTURAL REVIEW 12-07 (MODIFICATION):

- 1. This Architectural Review 12-07 (modification) relinquishes, and replaces in their entirety, the prior conditions of approval for Architectural Review No12-07 and allows a modified placement of the car wash tunnel, two drive-thru restaurants and an augmented service station canopy and new mini-market building. A building permit must be obtained and construction diligently pursued for all phases of the project within 12 months of the effective date, unless a time extension is granted by the Planning Commission.
- 2. The construction shall be in conformance with the plans submitted and conditions imposed herein for Architectural Review No. 12-07. This shall include the materials and colors as shown on the approved plans. The applicant or successor in interest shall submit detailed plans to the Building Division for review and approval.
- 3. A comprehensive sign program shall be submitted for review by the Planning Commission prior to issuance of a Certificate of Occupancy.
- 4. The project shall comply with all applicable codes, laws and regulations, regardless of whether they are listed in these conditions.
- 5. The applicant shall pay all applicable school impact fees to the Coachella Valley Unified School District prior to the issuance of building permit.
- 6. Additional cabling will be required to provide electric service from Imperial Irrigation District. Existing cable will require upgrading to include switches and other subsurface structures.

Grading and Drainage

- 7. A preliminary geological and soils engineering investigation shall be conducted by a registered soils engineer, and a report submitted for review with the grading plan and shall include pavement recommendations (on-site & off-site). The report recommendations shall be incorporated into the grading plan design prior to grading plan approval. The soils engineer and/or the engineering geologist shall certify to the adequacy of the grading plan.
- 8. A grading plan, prepared by a California Registered Civil Engineer, shall be submitted for review and approval by the City Engineer prior to issuance of any permits. A final soils report, compaction report and rough grading certificate shall be submitted and approved prior to issuance of any building permits.
- 9. A Drainage Report, prepared by California Registered Civil Engineer, shall be submitted for review and approval by the City Engineer prior to issuance of any permits. 100% of the post development runoff shall be retained onsite. The report shall contain a Hydrology Map showing on-site and off-site tributary drainage areas and shall be prepared in accordance with the requirements of the Riverside County Flood Control District. Adequate provisions

shall be made to accept and conduct the existing tributary drainage flows around or through the site in a manner which will not adversely affect adjacent or downstream properties. If the design of the project includes a retention basin, it shall be sized to contain the runoff resulting from a 10-year storm event and the runoff from a 100-year storm event shall be contained within basin with shallow ponding (2.5' max.) and within the public streets. The basin shall be designed to evacuate a 10-year storm event within 72 hours. The size of the detention basin(s) shall be determined by the hydrology report and be approved by the City Engineer. Detention basin shall be provided with a minimum of 2.00 feet sandy soil if determined to contain silt or clay materials. Maximum allowable percolation rate for design shall be 10 gal./s.f./day unless otherwise approved by the City Engineer. A percolation test for this site is required to be submitted. A combination drywell vertical drain field shall be constructed at all points where runoff enters the retention basin. Drywell & vertical drain field design shall be based on soils borings made at the proposed drywell locations after the retention basins have been rough graded. Minimum depth of borings shall be 45-feet. A log that includes sieve analysis for each strata of the borings shall be submitted to the City Engineer for confirmation of depth of the vertical drain fields.

- 10. Site access improvements shall be in conformance with the requirements of Title 24 of the California Administrative Code. This shall include the replacement or installation of access ramps for off-site and on-site streets as required.
- 11. Applicant shall obtain approval of site access and circulation from Fire Marshall and trash disposal company.
- 12. Applicant shall construct all off-site and on-site improvements including street pavement, curb, gutter, sidewalk, street trees, perimeter walls, perimeter landscaping and irrigation, storm drain, street lights, and any other incidental works necessary to complete the improvements. Driveways shall be in accordance with City standard drawing S-18. Damaged curb, gutter and sidewalk shall be replaced as required.

Sewer and Water Improvements

- 13. Sewer & Water Improvement Plans prepared by a California Registered Civil Engineer shall be submitted for Engineering plan check and City Engineer approval.
- 14. Applicant shall construct all off-site and on-site water improvements and any other incidental works necessary to complete the improvements. Size and location of sewer and water improvements shall be approved by the City Engineer.
- 15. Minimum depth of sewer manholes shall be 5.00 feet (top of pipe to top of rim). Size and slope of sewer mains shall be approved by the City Engineer. The minimum slope for sewer main shall be as follows: (1) 8" 0.33 percent, (2) 10" 0.24 percent, (3) 12" 0.19 percent, (4) 15", 18", 24", 27" & 33" 0.14 percent.

- 16. Applicant shall connect to the existing sewer main in the relocated 48 grade separation via six inch laterals with a clean out at the property line. On site sewer shall be constructed according to City Standards and shall be subject to inspection by the City.
- 17. Applicant shall connect to the existing 12 inch water main in Grapefruit Blvd. and the 48 Grade separation as required to provide adequate pressure to serve both domestic and fire services, including all fire hydrants, valves, 4 inch blow offs, fittings and, any other incidental works necessary to complete the improvements according to City standards.
- 18. Applicant shall provide the City with bonding for all off-site improvements, such bonding amounts shall be based on an Engineer's Estimate provided by the Applicant.

General

- 19. A composite utility plan showing all utilities shall be submitted for review and approval by the City Engineer. The applicant shall construct all other utilities such as gas, telephone, television cable, electrical, and any other incidental works necessary to complete the utility improvements. All utilities will be constructed underground and extended to the tract boundary. Existing overhead utilities within the limit of construction shall be relocated underground and behind sidewalk. Street improvement plans shall not be approved until this plan is submitted and deemed substantially complete and correct.
- 20. The developer shall submit a Fugitive Dust Control and Erosion Control plan in accordance with Guidelines set forth by CMC and SCAQMD to maintain wind and drainage erosion and dust control for all areas disturbed by grading. Exact method(s) of such control shall be subject to review and approval by the City Engineer. No sediment is to leave the site. Additional securities, in bond form, in amount of \$2,000.00 per acre of gross area, and a one-time cash deposit of \$2,000.00 are required to insure compliance with this requirement. No work may be started on or off site unless the PM-10 plan has been approved, the original plans, and executed dust control agreement, are filed in the engineering department at the City of Coachella.
- 21. The applicant shall pay all necessary plan check, permit and inspection fees. Fees will be determined when plans are submitted to the City Engineering Department for plan check.
- 22. The applicant shall comply with the valley wide NPDES permit requirements including but not limited to submittal of a Preliminary WQMP and a Final WQMP for final approval including executed maintenance agreement.

Completion

23. "As-built" plans or composite utility plans shall be submitted to and approved by the City Engineer prior to acceptance of the improvements by the City. All off-site and on-site improvements shall be completed to the satisfaction of the City Engineer prior to acceptance of improvements by the City.

- 24. Submit plumbing plans for facility. Facility will need oil/grease interceptors and oil/water clarifiers and sample wyes installed.
- 25. Install above ground "double check detector assembly" DCDA for fire system to protect water supply from contamination or pollution
- 26. Backflow devices shall be installed and will require reduced pressure principle device (RP) installed to protect water supply from contamination or pollution.
- 27. Submit a site plan for stormwater including basin retention plan
- 28. Submit a spill containment and response plan to prevent non-stormwater spills to storm system; spill response kit to be maintained in the immediate area along with a record's log to document occurrences of spill events.
- 29. Building height shall not exceed thirty (35) feet.
- 30. Separate permits shall be required for wall or fence construction. The maximum height of any wall shall be limited to eight (8) feet as measured from an average of the ground elevations on either side.
- 31. Concrete decorative paving shall be installed at the full driveway width, and with a depth of 20 feet at each driveway entrance to the project site.
- 32. The applicant shall install and maintain all landscaping and irrigation within the landscape parkway and the sidewalk on frontage areas along Grapefruit Blvd, Dillon Road, and Avenue 48 and shall assume landscape maintenance responsibilities of all parkway areas through a maintenance agreement with the City. The owner may enter into an agreement with the City for landscape installation and maintenance agreement on the south berm area adjacent to the Dillon Bridge prior to issuance of a building permit.
- 33. The developer must submit a landscape plan following the guidelines of the adopted Landscape Development Guidelines and Specifications prepared by VC2 Group dated September 2000. Landscaping shall also conform to Chapter 17.54 of the Coachella Municipal Code.
- 34. The Development Services Department may request minor substitutions of plant materials or request additional sizing or quantity of materials during plan check.
- 35. The landscape and irrigation plan shall show compliance with the following development standards for the parking areas.
 - a) Where any parking area abuts a street there shall be a minimum setback of ten (10) feet from the public right of way, which shall be fully landscaped and irrigated unless a greater setback is required by any other provision of the title.

- b) Internal landscaping equal to a minimum of five percent (5%) of the parking area and driveway area is required and shall be established throughout the parking area. The landscape plan shall include common and botanical names and include size of each species. The applicant shall submit an irrigation plan for review and approval by Directors of Community Development and Engineering. Landscaping and automatic irrigation system must be completed prior to occupancy of the first tenant space.
- c) All landscape planter beds in interior parking areas shall be not less than five (5) feet in width and bordered by a concrete curb not less than six (6) inches nor more than eight (8) inches in height adjacent to the parking surface.
- d) Where a drive aisle abuts the side of a parking space a landscaped planter shall separate the parking space from the drive aisle. The masonry wall proposed to screen the drive-thru aisle along the Avenue 48 and Dillon Road frontage shall be a minimum of four feet in height and vertical shrubs shall be used to further screen the drive-thru aisle from view to the street.
- e) At least one (1) fifteen (15) gallon tree shall be provided within the parking area for every ten (10) parking spaces, with size, height and species acceptable to staff.
- f) All internal landscape planters shall have permanent and automatic sprinklers or drip irrigation systems.
- 36. Mature Date Palm trees shall be planted with a minimum trunk height of 12 feet.
- 37. All roof mounted mechanical equipment shall be obscured from view by parapet walls that exceed the height of the equipment.
- 38. All new lighting shall be low pressure sodium and shall be directed away from adjacent properties. Prior to issuance of a building permit, a lighting plan shall be required to be submitted for review and approval by the Planning Director showing lighting standards, locations, and light levels throughout the project site. Lighting shall also be provided along all walkways between, building #1, car wash building, and Arco AM/PM building. Landscape accent lighting shall be used in all perimeter landscape areas.
- 39. A minimum of five (5) trash enclosures shall be constructed; this shall include doors and a concrete foundation within the enclosure, and a concrete apron for the area in the swing of the gates, and include space for recycling containers. Trash enclosures shall be compatible with the architecture of the project. These bins shall be of a size and design that is acceptable to Burrtec. The trash enclosures shall be maintained in a clean and sanitary manner.
- 40. The applicant shall submit plans that demonstrate existing billboard sign will not interfere with on-site vehicle circulation.
- 41. Tenant infill improvement plans shall be required to be approved by the Planning, Building and Fire Departments prior to the issuance of building permits. All buildings shall be subject

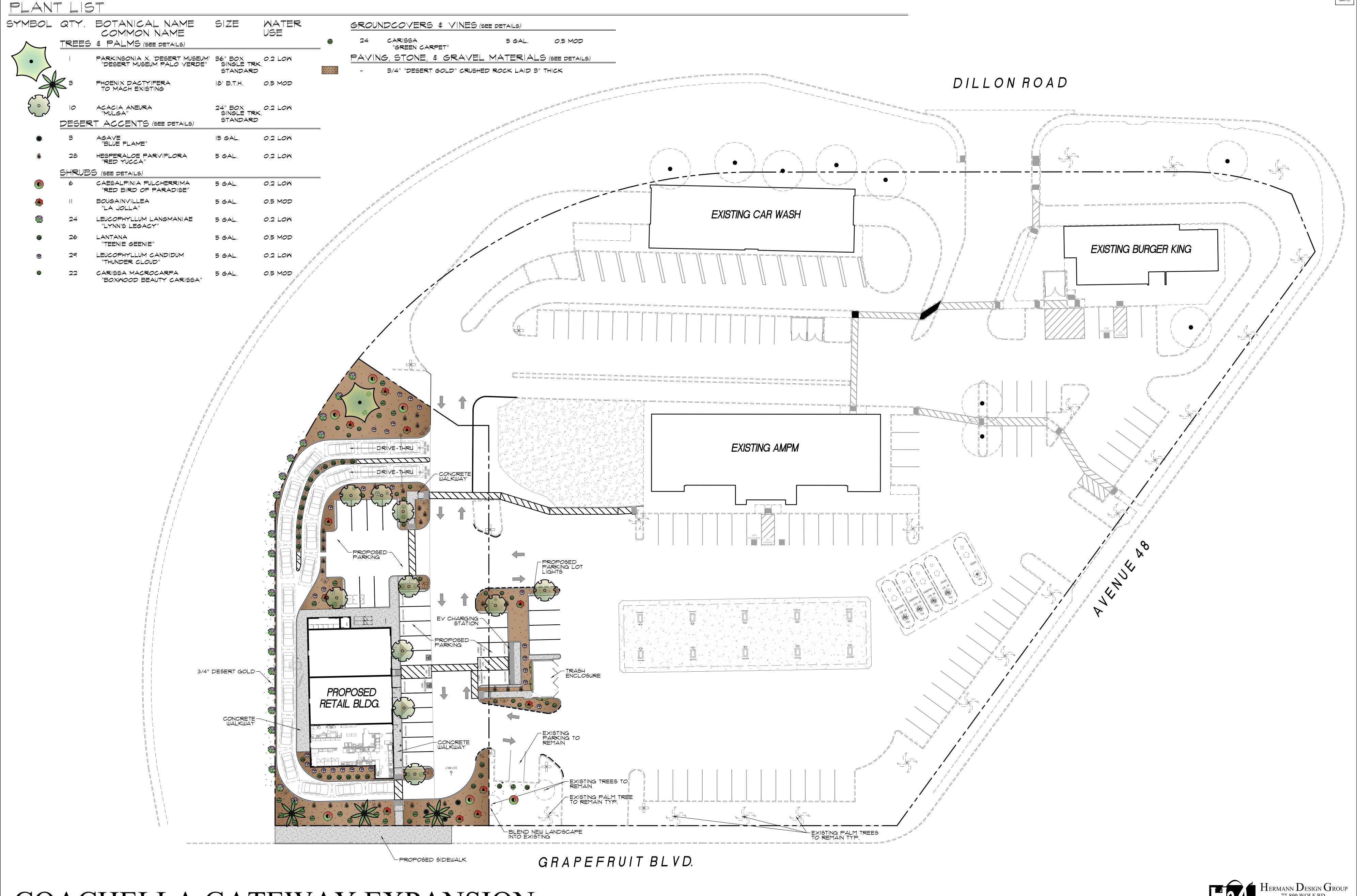
to conformance with applicable building codes including the Uniform Building and Plumbing Codes.

Fire Department

- 42. Provide or show there exists a water system capable of delivering a fire flow 1500 gallons per minute for a 2 hours duration at 20 psi residual operating pressure, which must be available before any combustible material is placed on the construction site. (Fire flow may be adjusted, based on building use, sprinkler installation, and/or building size)
- 43. Approved accessible on-site fire hydrants shall be located not to exceed 500 feet apart in any direction. Any portion of the facility or of an exterior wall of the first story of the building shall not be located more than 150 feet from fire apparatus as measured by an approved route around the complex, exterior of the facility or building. No portion of a building shall be further than 400 feet from a fire hydrant. Fire hydrants shall provide the required fire flow.
- 44. (If an onsite fire hydrant is necessary) Prior to building plan approval and construction, applicant/developer shall furnish two copies of the water system fire hydrant plans to Fire Department for review and approval. Plans shall be signed by a registered civil engineer, and shall confirm hydrant type, location, spacing, and minimum fire flow. Once plans are signed and approved by the local water authority, the originals shall be presented to the Fire Department for review and approval.
- 45. Prior to issuance of building permits, the water system for fire protection must be provided as approved by the Fire Department and the local water authority. If a water system already exists; a letter from the local water District, verifying an adequate water flow, will need to be submitted for review.
- 46. Blue dot retro-reflectors pavement markers on private, public streets and driveways to indicated location of the fire hydrant shall be per Standard 06-11 if an onsite fire hydrant is necessary.
- 47. Driveway loops, fire apparatus access lanes and entrance curb radius should be designed to adequately allow access of emergency fire vehicles. The applicant or developer shall include in the building plans the required fire lanes and include the appropriate lane printing and/or signage
- 48. Approved fire apparatus lanes shall be provided for every commercial/ public use facility, building or portion of a building. All access lanes shall extend to within 150 feet of all portions of the buildings.
- 49. Access lane shall be constructed with a surface so as to provide all weather driving capabilities.
- 50. An approved Fire Department access key lock box (Minimum Knox Box 3200 series model) shall be installed next to the approved Fire Department access door to the building. If the

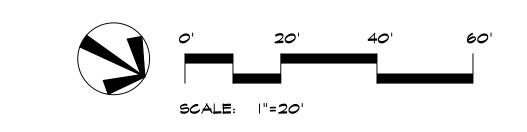
- buildings are protected with an alarm system, the lock box shall be required to have tampered monitoring. Required order forms and installation standards may be obtained at the Fire Department.
- 51. Display street numbers in a prominent location on the address side of building(s) and/or rear access if applicable. Numbers and letters shall be a minimum of 12" in height for building(s) up to 25' in height. In complexes with alpha designations, letter size must match numbers. All addressing must be legible, of a contrasting color, and adequately illuminated to be visible from street at all hours.
- 52. Install a complete commercial fire sprinkler system (per NFPA 13 1999 Edition). Fire sprinkler system(s) with pipe sizes in excess of 4" in diameter will require the project Structural Engineer to certify with a "wet signature", that the structural system is designed to support the seismic and gravity loads to support the additional weight of the sprinkler system. All fire sprinkler risers shall be protected from any physical damage. The PIV and FCD shall be located to the front, within 25 to 50 feet of hydrant, and a minimum of 25 feet from the building(s). Sprinkler riser room must have indicating exterior and/or interior door signs. A C-16 licensed contractor must submit plans, along with current permit fees, to the Fire Department for review and approval prior to installation.
- 53. Install an alarm monitoring system for fire sprinkler system(s) with 20 or more heads A C-10 licensed contractor must submit plans along with the current permit fees to the Fire Department for review and approval prior to installation.
- 54. Install a portable fire extinguisher, with a minimum rating of 2A-10BC, for every 3,000 sq. ft. and/or 75 feet of travel distance. Fire extinguishers shall be mounted 3.5 to 5 ft. above finished floor, measured to the top of the extinguisher. Where not readily visible, signs shall be posted above all extinguishers to indicate their locations. Extinguishers must have current CSFM service tags affixed.
- 55. Where applicable; a UL 300 hood/duct fire extinguishing system must be installed over the cooking equipment. A C-16 licensed contractor must submit plans, along with the current permit fee, to the Fire Department for review and approval prior to installation.
- 56. No hazardous materials shall be stored and/or used within the building, which exceeds quantities listed in 2007 California Building Code. No class I, II or IIIA of combustible/flammable liquid shall be used in any amount in the building.
- 57. Exit designs, exit signs, door hardware, exit markers, exit doors, and exit path marking shall be installed per the 2007 California Building Code.
- 58. Electrical room doors, FAC, fire riser, roof access if applicable shall be labeled as per use.
- 59. Access shall be provided to all mechanical equipment located on the roof as per 2007 California Mechanical Code.

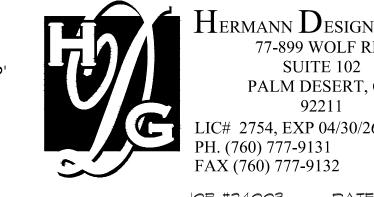
- 60. Air handling systems supplying air in excess of 2000 cubic feet per minute to enclosed spaces within buildings shall be equipped with an automatic shut off as per 2007 California Mechanical Code.
- 61. The Proposed project may have a cumulative adverse impact on the Fire Department's ability to provide an acceptable level of service. These impacts include an increase in the number of emergency and public service calls due to the increased presence of structures, traffic and population. The project proponents/developers will be expected to provide for a proportional mitigation to these impacts via capital improvements and/or impact fees.
- 62. The project will need to comply with the City's Industrial Pretreatment Program. All proposed discharges that are generated as a part of operations (garage, car washing) must meet discharge requirements.
- 63. The applicant is encouraged utilize energy efficient design in the construction of the proposed building to address issues such as water conservation/reuse, ventilation, insulation, building materials, energy production (solar, wind), and site planning.
- 64. The applicant shall construct a bus shelter or dedicate right-of-way for a future shelter at the Sunline transit stop on Grapefruit Boulevard. Bus shelter plans or plans for the dedication of right-of-way shall be submitted by the applicant for review by the Sunline Transit Agency, City Engineer and the Development Services Director.
- 65. The applicant shall install bicycle racks in accessible areas to each building, excluding the car wash service building.
- 66. A path of travel shall be provided with access to all tenant spaces from Grapefruit Blvd, Dillon Road and Avenue 48.
- 67. The project shall comply with the City of Coachella Art in Public Places Program by the dedication of an approved artwork at the project site, payment of an in-lieu fee to the Public Arts Fund, the posting of a financial security or the donation of approved artwork in conformance with The Ordinance No. 1034.
- 68. The applicant shall defend, indemnify and hold harmless the City and its officers, agents and employees from any claim, action, or proceedings against the City or its officers, agents or employees. The applicant shall further defend, indemnify and hold harmless the City, its officers, agents and employees from any damages, liabilities, claims, suits, or causes of auction of any kind or form, whether for personal injury, death or property damage, arising out of or in connection with the activities or performance of the applicant, its agents, employees, licensees, contractors, subcontractors or independent contractors, pursuant to the approval issues by the City. The applicant shall submit an indemnification agreement in a form acceptable to the City Attorney.

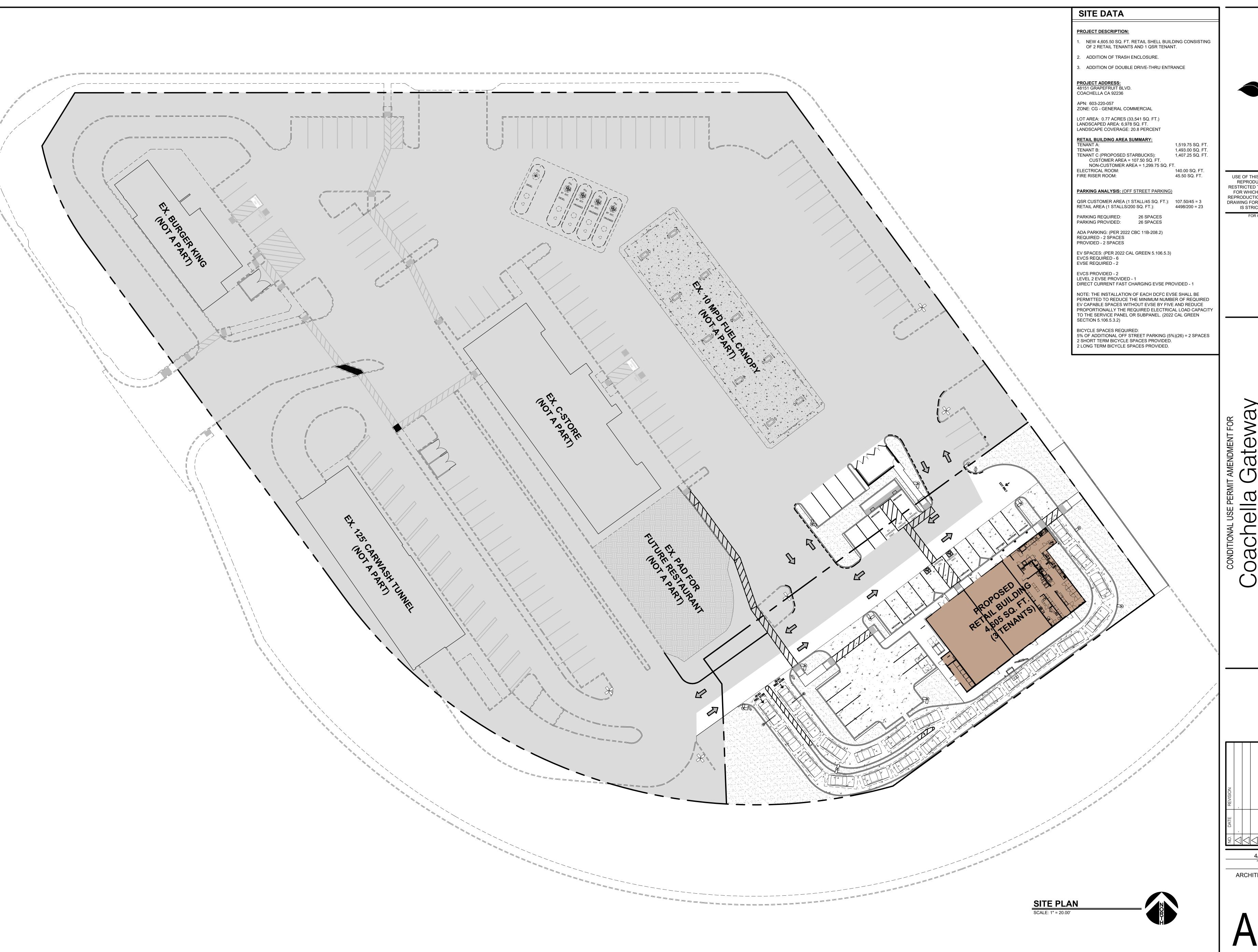


COACHELLA GATEWAY EXPANSION PRELIMINARY LANDSCAPE PLAN

RIVERSIDE COUNTY, CA







BLACK GOLD ENGINEERING

IL ENGINEERING - LAND SURVEYING - DESIGN
270 SPECTRUM STREET, INDIO, GA 92203
(760) 396-9260 - FX: (760) 396-5245

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Coachella Gateway
Retail Building

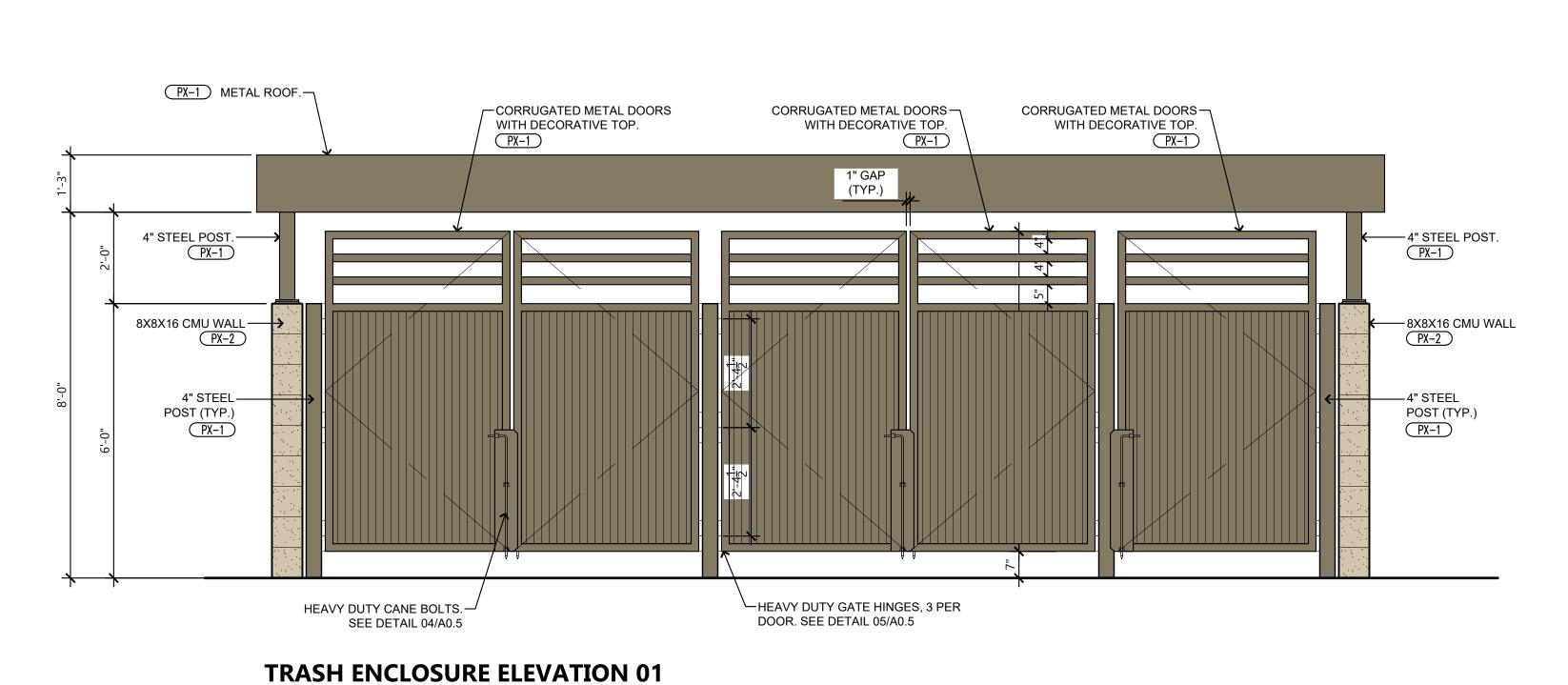
NO SINGLE A/18/2024
PROJECT NUMBER
121 04

4/18/2024
PROJECT NUMBER
121-04

ARCHITECTURAL SITE
PLAN

A0.1





-4x4 ROOF DRAIN 2" STEEL POST (TYP.) PX-1 4" STEEL POST.— 8X8X16 CMU-HEAVY DUTY GATE HINGES, 3 PER TRASH ENCLOSURE ELEVATION 03 SCALE: 1/2" = 1'-0"

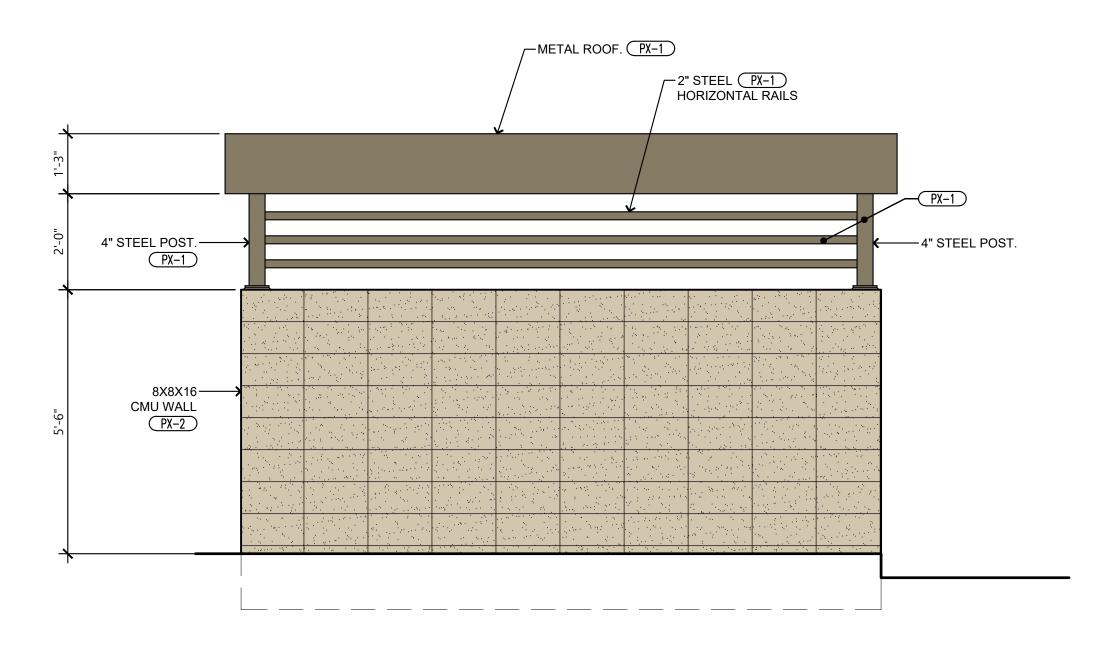
EXTERIOR MATERIAL /FINISH SCHEDULE **EXTERIOR FINISH NOTES**

. THE INTERIOR WALL SURFACES OF THE TRASH ENCLOSURE ARE TO BE SMOOTH, SEALED AND WASHABLE. APPLY ONE COAT EPOXY FILLER/SEALER AND ONE COAT GLOSS POLYURETHANE.

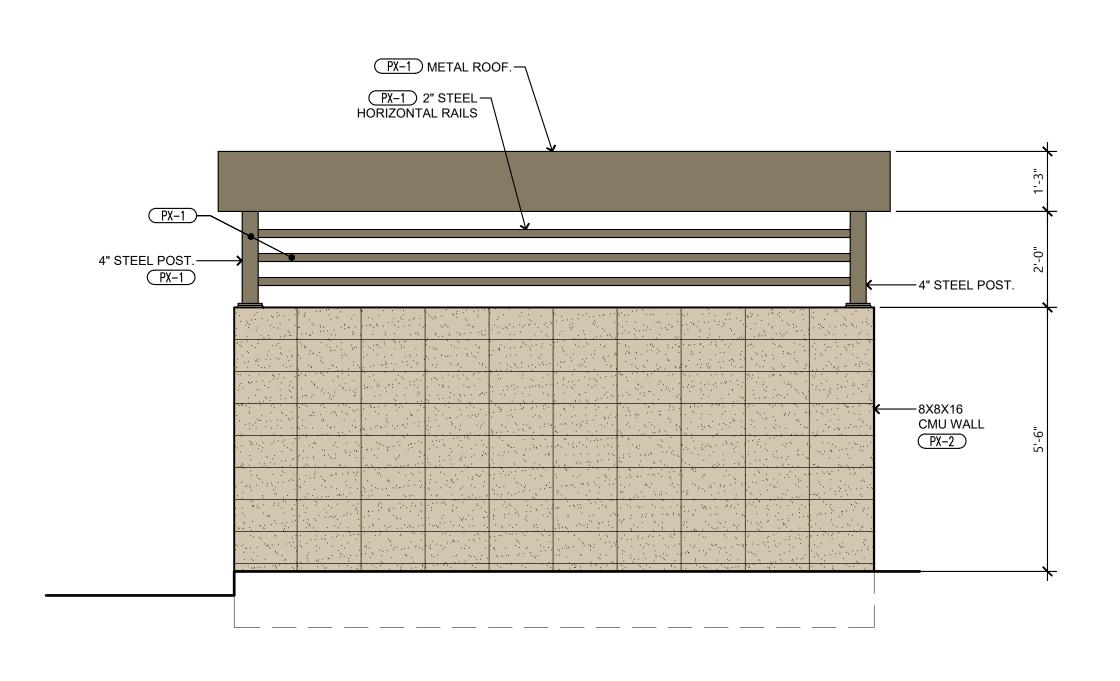
2. CMU TO BE FINISHED PX-2.

PX-1 MANUFACTURER: DUNN EDWARDS COLOR: MISSION TRAIL NOTES: EPOXY FINISH TO BE APPLIED TO METAL ROOF (15 YEAR FILM COATING & 5 YEAR FADE

WARRANTY MIN.) MANUFACTURER: ORCO BLOCK PRODUCT: SPLIT FACE BLOCK COLOR: TAN MW



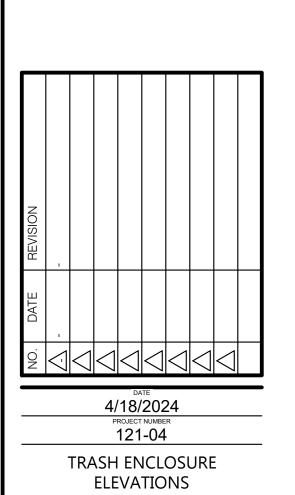
TRASH ENCLOSURE ELEVATION 02



TRASH ENCLOSURE ELEVATION 04

SCALE: 1/2" = 1'-0"

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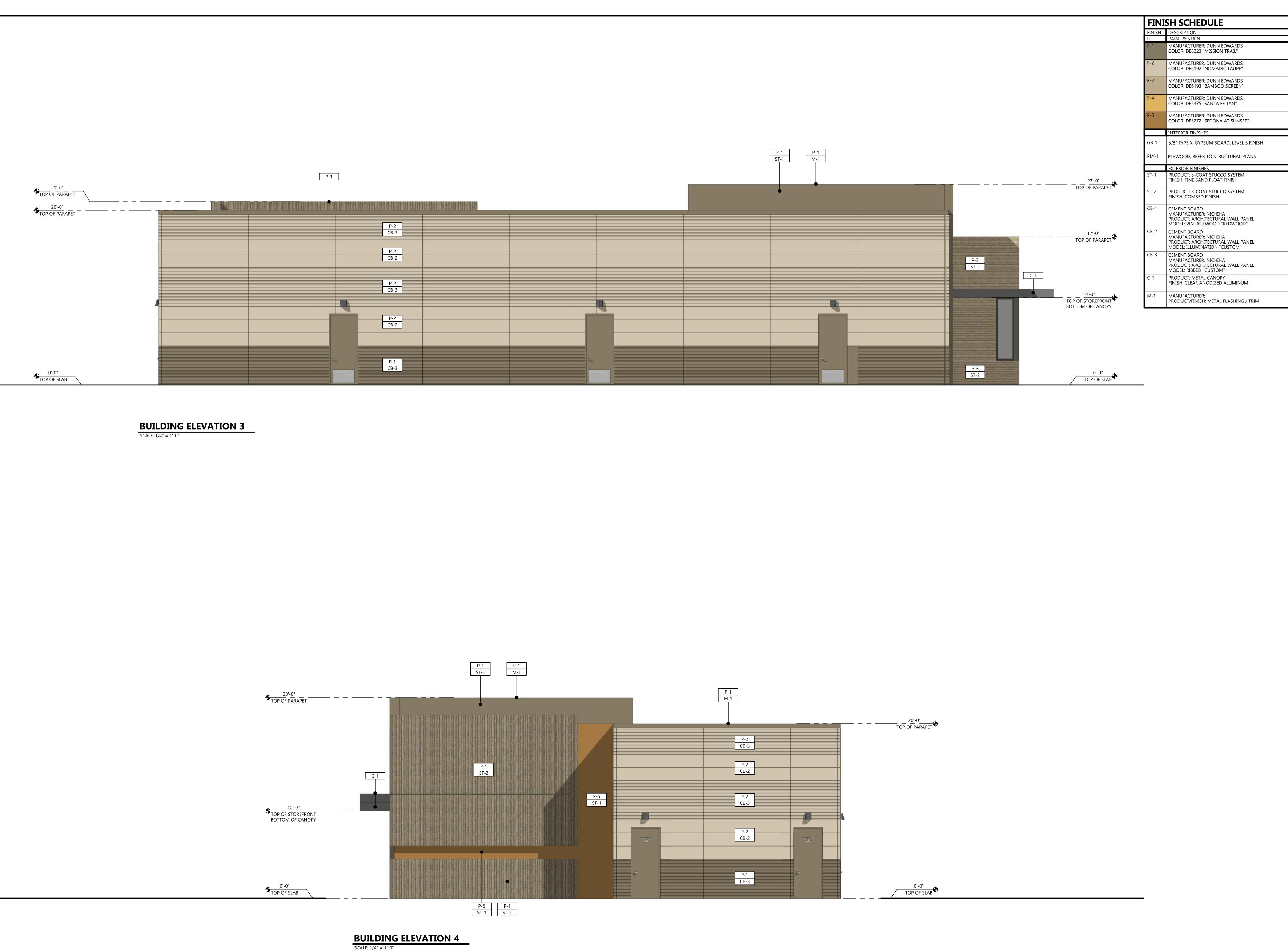




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4/16/2024 PROJECT NUMBER 121-04 EXTERIOR BUILDING ELEVATIONS



4/16/2024
PROJECT NUMBER
121-04

EXTERIOR BUILDING
ELEVATIONS

A1.3

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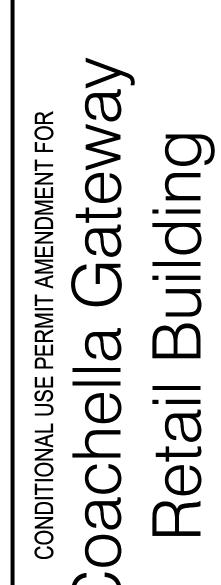
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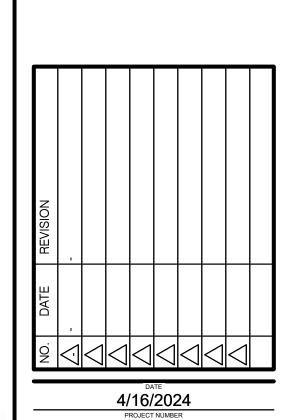






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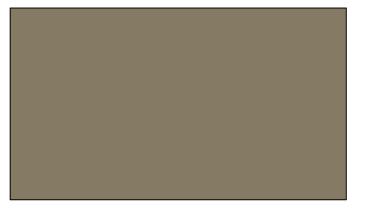


4/16/2024

PROJECT NUMBER
121-04

BUILDING
PERSPECTIVE VIEWS

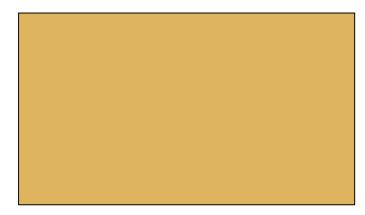
A1.4



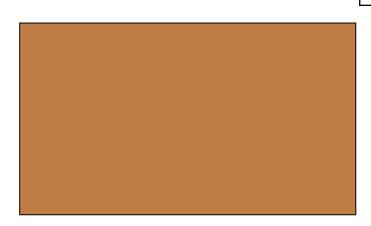
DUNN EDWARDS DE6223 MISSION TRAIL



DUNN EDWARDS DE6192 NOMADIC TAUPE



DUNN EDWARDS DE5375 SANTA FE TAN



DUNN EDWARDS DE5272 SEDONA AT SUNSET



DUNN EDWARDS DE6223 MISSION TRAIL WITH COMBED FINISH



DUNN EDWARDS DE6193 BAMBOO SCREEN WITH COMBED FINISH



NICHIHA RIBBED PANEL CUSTOMIZED COLOR TO MATCH DUNN EDWARDS DE6192 NOMADIC TAUPE



NICHIHA IUUMINATION PANEL CUSTOMIZED COLOR TO MATCH DUNN EDWARDS DE6192 NOMADIC TAUPE



NICHIHA IUUMINATION PANEL CUSTOMIZED COLOR TO MATCH DUNN EDWARDS DE6223 MISSION TRAIL



NICHIHA VINTAGEWOOD SERIES WITH REDWOOD COLOR

Item 3.

COACHELLA GATEWAY SIGN PROGRAM AMENDMENT

PROJECT DIRECTORY

OWNER: CHANDI & KARAN LLC

42270 SPECTRUM ST INDIO, CA, 92203 PH: (760) 396-9260

DESIGNER: BLACK GOLD ENGINEERING INC.

42270 SPECTRUM ST INDIO, CA, 92203 PH: (760) 396-9260

SIGN CONSULTANT: MCID

3941 Park Drive. Suite 20-516 El Dorado Hills, CA. 95762 PH: (916) 790-4719

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- 8 CHANNEL LETTERING
- 9 SITE PLAN

BUILDING SIGNS

10 TENANT

GROUND SIGNS

11 PYLON DIRECTORY SIGN

A. INTRODUCTION

The intent of this sign criteria is to provide the guidelines necessary to achieve a visually coordinated, balanced and appealing sign environment, harmonious with the architecture of the project, while maintaining provisions for individual graphic expression.

Performance of this sign criteria shall be rigorously enforced and any nonconforming sign shall be removed by the tenant or his sign contractor at their expense, upon demand by the owner.

Exceptions to these standards shall not be permitted without the approval from the Landlord and will require approval of a modification to the sign program application by the city.

Accordingly, the Landlord will retain full rights of approval for any sign used in the center.

No sign shall be installed without the written Landlord approval and the required City permits.

This sign program is designed in coordination with the Coachella Municipal Code Zoning Ordinance (Section 17.56.010)

B. GENERAL LANDLORD/TENANT REQUIREMENTS:

- 1. Proir to city submittal or sign fabrication, each tenant shall submit to landlord for written approval, three (3) copies of the fully detailed shop drawings of their proposed sign, indicating conformance with the sign criteria herein outlined.
- 2. The Landlord shall determine and approve the availability and position of a tenant name on any ground sign(s).
- 3. The tenant shall pay for all signs, related materials and installation fees (including all inspection costs).
- 4. The tenant shall obtain all necessary permits.
- 5. The tenant shall be responsible for fulfillment of all requirements of this sign criteria.
- 6. It is the responsibility of the Tenants sign company to verify all conduit and transformer locations and service access prior to fabrication.
- 7. Should a sign be removed, it is the Tenant's responsibility to patch and paint all holes to match finish and color of adjacent surface.
- 8. The Landlord may, at his sole discretion and at the Tenant's expense. correct, replace, or remove any sign that is installed without written approval and/or that is deemed unacceptable pertaining to this sign program.
- 9. If the Tenant chooses to change his exterior sign at anytime during the term of his lease, then the Tenant must comply with the requirements set forth herein and any future modifications, amendments, revisions or changes which have been made to this sign program for this center after the execution of his lease agreement.

C. GENERAL SIGN CONSTRUCTION REQUIREMENTS:

- 1. All signs and their installation shall comply with all local building and electrical codes.
- 2. All electrical signs will be fabricated by a U.L. approved sign company, according to U.L. specifications and bear U.L. Label.
- 3. Sign company to be fully licensed with the City and State and shall provide proof of full Workman's Compensation and general liability insurance.
- 4. All penetrations of building exterior surfaces are to be sealed, waterproof, and in color & finish to match existing exterior.
- 5. Internal illumination to be 30 milliamp neon, installed and labeled in accordance with the "National Board of Fire Underwriters Specifications".Or L.E.D.'S which ever sign company decied.
- 6. Painted surfaces to have a semi gloss finish. Only paint containing acrylic polyurethane products can be used.
- 7. Logo and letter heights shall be as specified and shall be determined by measuring the normal capital letter of a type font exclusive of swashes, ascenders, and descenders.
- 8. All sign fabrication work shall be of excellent quality. All logo images and type-styles shall be accurately reproduced. Lettering that approximates type-styles shall not be acceptable. The Owner reserves the right to reject any fabrication work deemed to be below standard.

- 9. All lighting must match the exact specification of the approved working drawings. No exposed conduits or race ways will be allowed.
- 10. Signs must be made of durable rust -inhibited materials that are appropriate and complimentary to the building.
- 11. Color coatings shall exactly match the colors specified on the approved plans.
- 12. Joining of materials (e.g., seams) shall be finished in such a way as to be unnoticeable. Visible welds shall be continuous and ground smooth. Rivets, screws, and other fasteners that extend to visible surfaces shall be flush, filled, and finished so as to be unnoticeable.
- 13. Finished surfaces of metal shall be free from canning and warping. All sign finishes shall be free from dust, orange peel, drips, and runs and shall have a uniform surface conforming to the highest standards of the industry.
- 14. In no case shall any manufacturer's label be visible from the street from normal viewing angles.
- 15. Exposed raceways are not permitted unless they are incorporated into the overall sign design and acceptable to the Landlord.
- 16. Exposed junction boxes, lamps, tubing, or neon crossovers of any kind are prohibited.
- 17. All materials used in signage construction and installation must be new. No used materials will be allowed.

D. SIGNAGE SPECIFICATIONS:

The intent of this criteria is to encourage creativity to ensure the individuality of each tenant sign as opposed to similar sign design, construction, and colors repeated throughout the project.

The following types of construction will be allowed:

Acrylic face channel letters
Through face and halo channel letters
Reverse pan channel letters
Open pan channel letters
Push thru letters and logos in aluminum cabinets
Logo modules with applied vinyl graphics.
Flat cut out dimensional shapes and accents
Metal screen mesh

The use of at least two types of the above to be incorporated into each sign design is encouraged unless otherwise approved by Landlord. The idea of using dissimilar materials and creating signs with varying colors, layers and textures will create an exciting and appealing retail environment.

Stacked copy is permitted

Ascending and descending shapes shall not be included in allocated square footage except for the area they occupy.

E. ABANDONMENT OF SIGNS:

Any tenant sign left after thirty (30) days from vacating premises shall become the property of Landlord. Cost for the removal of abandoned sign shall be in accordance with lease agreement.

F. INSPECTION:

Landlord reserves the right to hire an independent electrical engineer at the Tenant's sole expense to inspect the installation of all Tenant's signs and to require the Tenant to have any discrepancies and/or code violations corrected at the Tenant's expense.

G. PROHIBITED SIGNS:

1. Signs constituting a Traffic Hazard

No person shall install or maintain, or cause to be installed or maintained, any sign which simulates or imitates in size, color, lettering or design any traffic sign or signal, or which makes use of the words "STOP", "LOOK", "DANGER" or any words, phrases, symbols, or characters in such a manner as to interfere with, mislead or confuse traffic.

2. Signs in Proximity to Utility Lines:

Signs which have less horizontal or vertical clearance from authorized communication or energized electrical power lines that are prescribed by the laws of the State of California are prohibited.

- 3. Painted letters will not be permitted.
- 4. Wall signs may not project above the top of a parapet, the roof line at the wall, or roof line.
- 5. Electronic message board signs shall not include flashing, blinking, audible, scrolling images or messages. Electronic message board signs shall not be placed within 1,000 feet of any other existing electronic message board sign.
- 6. Signs must be architecturally compatible with the entire center.
- 7.No sign shall project above or below the sign-able area. The sign-able area is defined in the attached Exhibit for minor and major tenants.
- 8. Vehicular Signs.

Vehicles of any type or trailers displaying advertising for a business, services, merchandise or products may not be parked adjacent to or along a public right-of-way in such a way as to be used for advertising purposes.

9. Roof Signs.

Signs painted or mounted on a roof, including a mansard roof, are prohibited. Signs that extend above the peak of the roof or the parapet are prohibited. Signs that are mounted on a mansard or other roof type and are not integrated into the design of the building are also prohibited.

10. Sandwich or "A" Frame Signs.

The use of sandwich or "A" frame type signs shall be prohibited.

11. Light Bulb Strings:

External displays, other than temporary decorative holiday lighting or holiday displays, which consist of unshielded light bulbs, festoons, and/or strings of open light bulbs are prohibited.

12. Banners, Pennants & Balloons Used for Advertising Purposes:

Temporary flags, banners, or pennants, or a combination of same constituting an architectural feature which is an integral part of the design character of a project may be permitted subject to Municipal Code requirements, Landlord's, and City approval. Temporary signs that are torn, misshapen, broken, or generally in disrepair shall be removed, replaced, or repaired.

- 13. Billboard Signs are not permitted.
- 14. The use of permanent sale signs is prohibited. The temporary use of these signs are limited to a thirty-day period and is restricted to signs affixed to the interior of windows which do not occupy more that 20% of the window area. Each business is permitted a total of not more than ninety (90) days of temporary window sale signs per calendar year.

15. Obscene or Unlawful Signs.

Signs displaying obscene, indecent, immoral or unlawful matter are not permitted.

16. Signs Affixed to Windows or Doors.

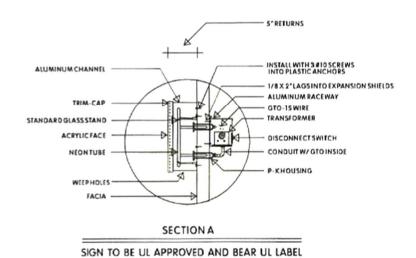
Any sign, merchandise, or graphic, otherwise affixed to windows or doors, interior or exterior shall be prohibited. Allowable signs affixed to windows shall in no case exceed 20% of the occupancy frontage window area.

17. Exterior Merchandise Signs.

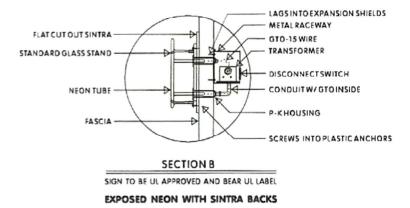
Signs attached, affixed to or from the interior or exterior of any merchandise or products displayed outside of permanent buildings.

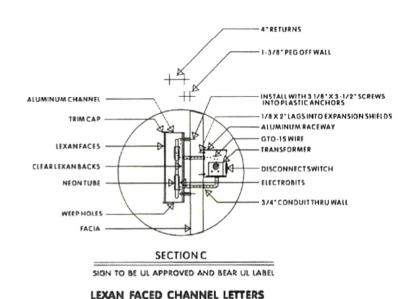
18. Pylon Directory Sign:

- a. Flat signs shall be prohibited. Signs shall be individual channel letters or embossed.
- b. Moving electronic signs shall be prohibited.
- c. Light should only shine through individual letters.



ACRYLIC FACED CHANNEL LETTERS





THROUGH FACE AND HALO ILLUMINATION

SECTION A

New single faced internally illuminated Lexan faced channel letter display.

Use standard aluminum construction with Matthews (or equivalent) semi gloss acrylic polyurethane finish. Faces use acrylic with 3/4" bronze trim cap, Illuminate

Paint 5" deep returns duronodic bronze.

with 30 ma neon or LED.

SECTION B

New single faced illuminated wall display.
Use multi layered flat cut out 1/2" Sintra graphics with applied neon OR LED overlays.

Flush mount Sintra to wall. Paint Sintra Matthews (or equivalent) semi gloss acrylic polyurethane.
Use neon OR LED overlays with PK type housings.

SECTION C

New single faced internally illuminated Lexan faced channel letter display with through face and halo illumination.

Use standard aluminum construction with Matthews (or

equivalent) semi gloss acrylic polyurethane finish, Faces use Lexan with 3/4" trim cap. Illuminate with 30 ma neon OR LED through face and halo.

Paint returns duronodic bronze.

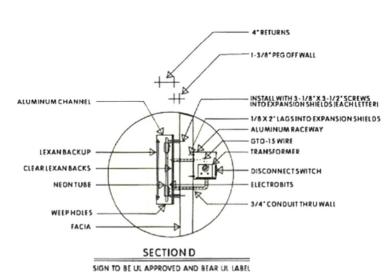
SECTION D

New single faced internally illuminated aluminum faced channel letter display with through face and halo illumination

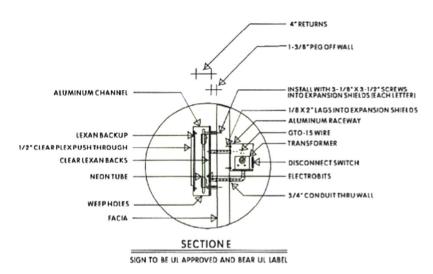
Use standard aluminum construction with Matthews (or equivalent) semi gloss acrylic polyurethane finish. Route out where graphics occur and back up with Lexan. Illuminate with 30 ma neon or LED through face and halo. Paint aluminum face and returns.

SECTION E

Same as "D" except route out where graphics occur and push through 1/2" clear plex with applied vinyl overlays. Note: All signs may utilize digital vinyl printing on faces of illuminated or non illuminated surfaces.



REVERSE PAN CHANNELS
THROUGH FACE AND HALO ILLUMINATION



REVERSE PAN CHANNELS
THROUGH FACE AND HALO ILLUMINATION
PUSH THROUGH GRAPHICS

——— MAXIMUM 75% — OF ADJACENT SURFACE

60% OF ADJACENT SURFACE OR 6'-0" MAX LETTER (WHICHEVER IS LESS)

MAJOR TENANT

MAJOR TENANTS (7,501 TO 99,999 SQUARE FEET)

MAXIMUM 70%____OF ADJACENT SURFACE

60% OF ADJACENT SURFACE OR 4'-0" MAX LETTER (WHICHEVER IS LESS) PAD TENANT

_____ MAXIMUM 70% ___ OF ADJACENT SURFACE

60% OF ADJACENT SURFACE OR 3'-0" MAX LETTER (WHICHEVER IS LESS) SHOP TENANT

SHOP TENANTS (UP TO 7,500 SQUARE FEET)

SIZE:2 SQUARE FOOT OF SIGN AREA PER LINEAR FOOT OF LEASED PREMISES, MAXIMUM OF 350 SQUARE FEET TOTAL PER ELEVATION.

MATERIALS: VARIETY OF TYPES PER SIGN CRITERIA, SINGLE OR MULTIPLE TYPES OF CONSTRUCTION ALLOWED.

ILLUMINATION: YES

COPY: TENANT NAME AND/OR LOGO

HEIGHT: SIXTY PERCENT OF ADJACENT SURFACE

LENGTH: SEVENTY FIVE PERCENT OF ADJACENT SURFACE

TYPEFACE: CUSTOM LOGO AND TYPE OK

COLORS: CUSTOM COLORS OK

SECONDARY SIGNS: YES (NOT TO EXCEED 25% OF TOTAL ALLOWABLE SIGN AREA). TOTAL COMBINED SIGNAGE NOT TO EXCEED MAXIMUM SQUARE FOOTAGE PER FRONTAGE.

SIZE:1.5 SQUARE FOOT OF SIGN AREA PER LINEAR FOOT OF LEASED PREMISES, MAXIMUM OF 250 SQUARE FEET TOTAL PER ELEVATION.

MATERIALS: VARIETY OF TYPES PER SIGN CRITERIA, TWO TYPES OF CONSTRUCTION REQUIRED UNLESS OTHERWISE APPROVED BY LANDLORD

ILLUMINATION: YES

COPY: TENANT NAME AND/OR LOGO

HEIGHT: SIXTY PERCENT OF ADJACENT SURFACE

LENGTH: SEVENTY PERCENT OF ADJACENT SURFACE

TYPEFACE: CUSTOM LOGO AND TYPE OK

COLORS: CUSTOM COLORS OK

SECONDARY SIGNS: YES (NOT TO EXCEED 25% OF TOTAL ALLOWABLE SIGN AREA). TOTAL COMBINED SIGNAGE NOT TO EXCEED MAXIMUM SQUARE FOOTAGE PER FRONTAGE.

SIZE:1.5 SQUARE FOOT OF SIGN AREA PER LINEAR FOOT OF LEASED PREMISES, MAXIMUM OF 150 SQUARE FEET TOTAL PER ELEVATION.

MATERIALS: VARIETY OF TYPES PER SIGN CRITERIA, TWO TYPES OF CONSTRUCTION REQUIRED UNLESS OTHERWISE APPROVED BY LANDLORD

ILLUMINATION: YES

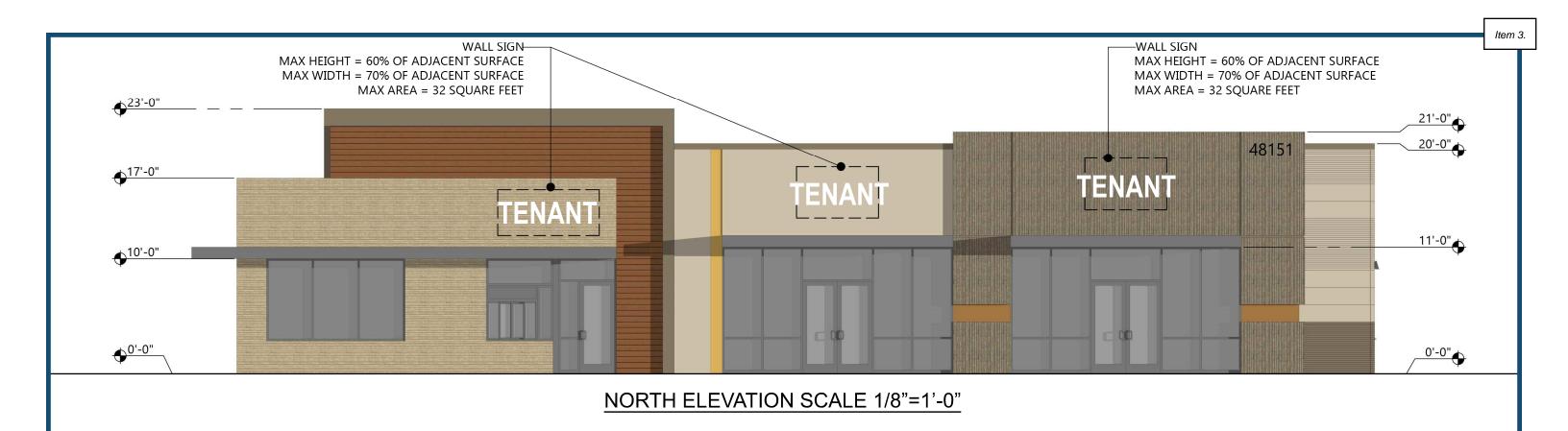
COPY: TENANT NAME AND/OR LOGO

HEIGHT: SIXTY PERCENT OF ADJACENT SURFACE LENGTH: SEVENTY PERCENT OF ADJACENT SURFACE

TYPEFACE: CUSTOM LOGO AND TYPE OK

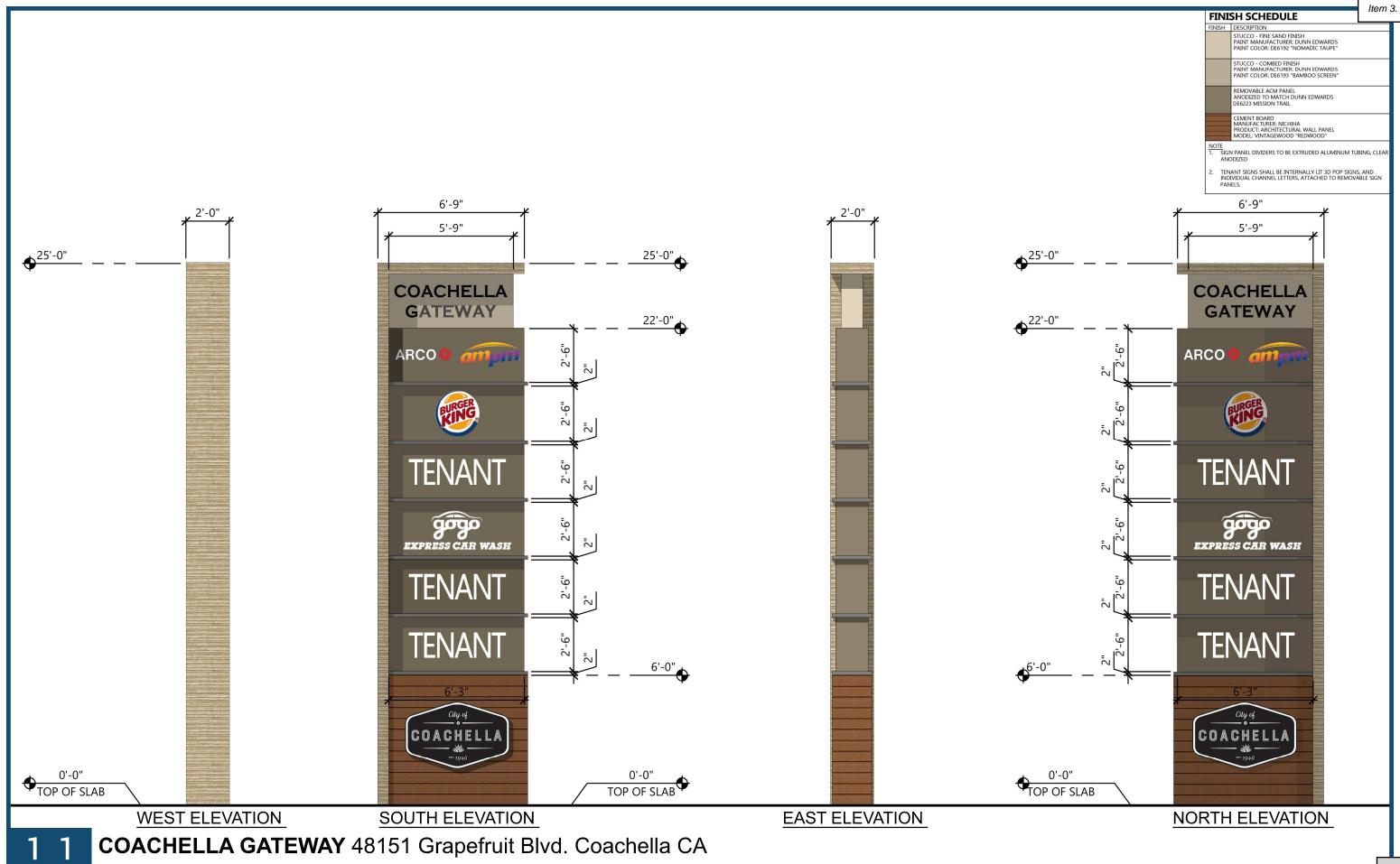
COLORS: CUSTOM COLORS OK SECONDARY SIGNS: NO

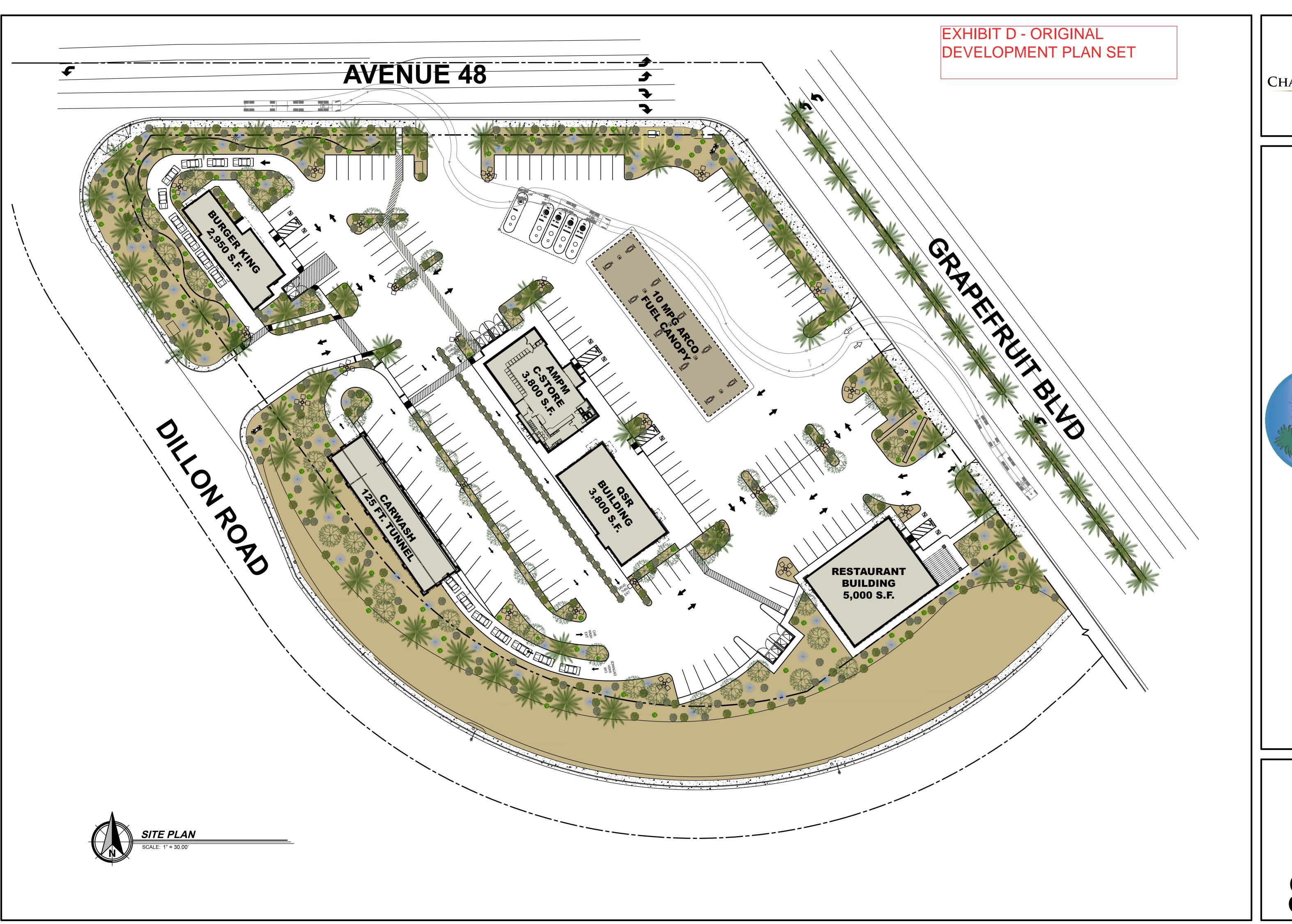






EAST ELEVATION SCALE 1/8"=1'-0"

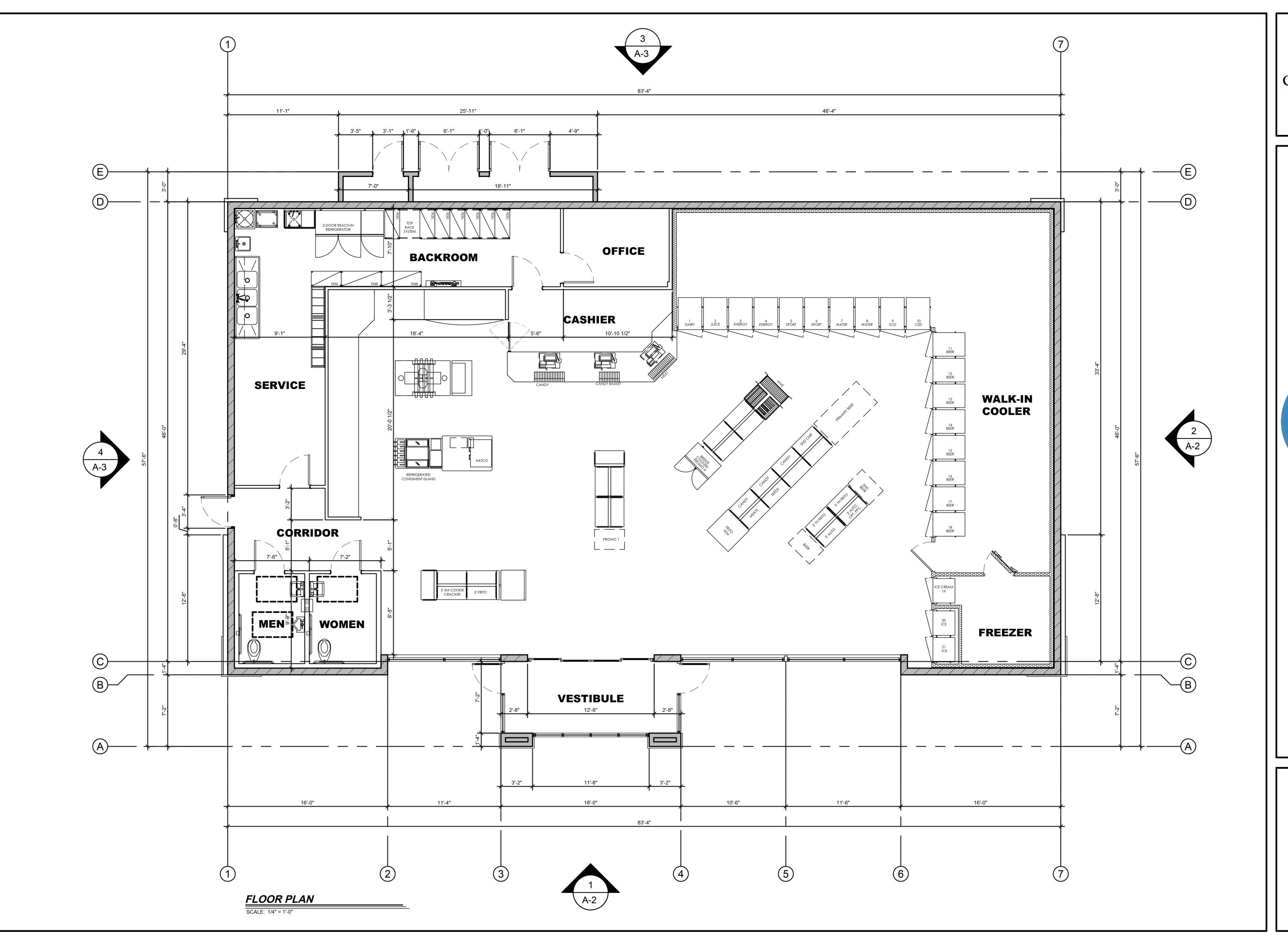






SITE PLAN

S-1

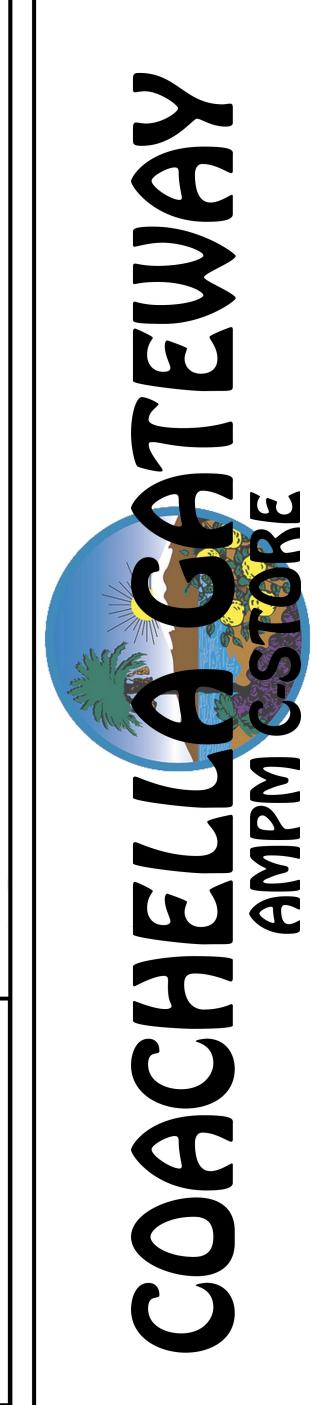




AMPM C-STORE FLOOR PLAN

A-1





KEYED NOTES:

(1) SIGNAGE (SEE BELOW NOTE)

1" EXPANSION JOINT CORNICE MOLDING

(6) FRONT METAL CANOPY

ALL EXTERIOR SIGNAGE

DECORATIVE MEDALLION

SERVICE DOOR AND FRAME

ALUMINUM STOREFRONT SYSTEM

8 ALUMINUM STOREFRONT ENTRANCE DOORS
9 8" HIGHT ILLUMINATE ADDRESS NUMBER

NOTE:
1. A SEPARATE PERMIT WILL BE REQUIRED FOR

2. ALL ROOF EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW INCLUDING FREEWAY.

3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING

STUCCO 7/8" SAND FINISH STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROVIDE CONTROL JOINTS AS REQUIRED

VENEER CULTURED STONE VENEER, PRO-LEDGESTONE. COLOR: ALASKAN

C-01 MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 7678 COTTAGE CREAM

C-02 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7680 LANYARD

C-03 MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 6888 PIZAZZ PEACH

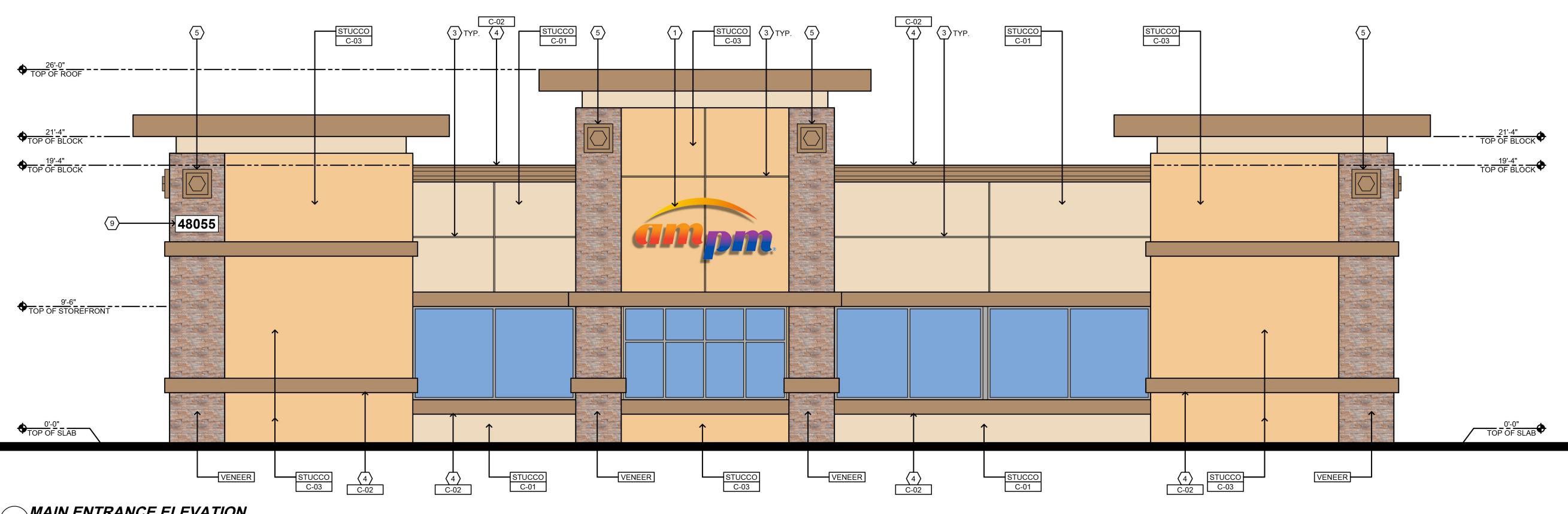
EXTERIOR FINISHES

SUNSET.

EXTERIOR PAINTS

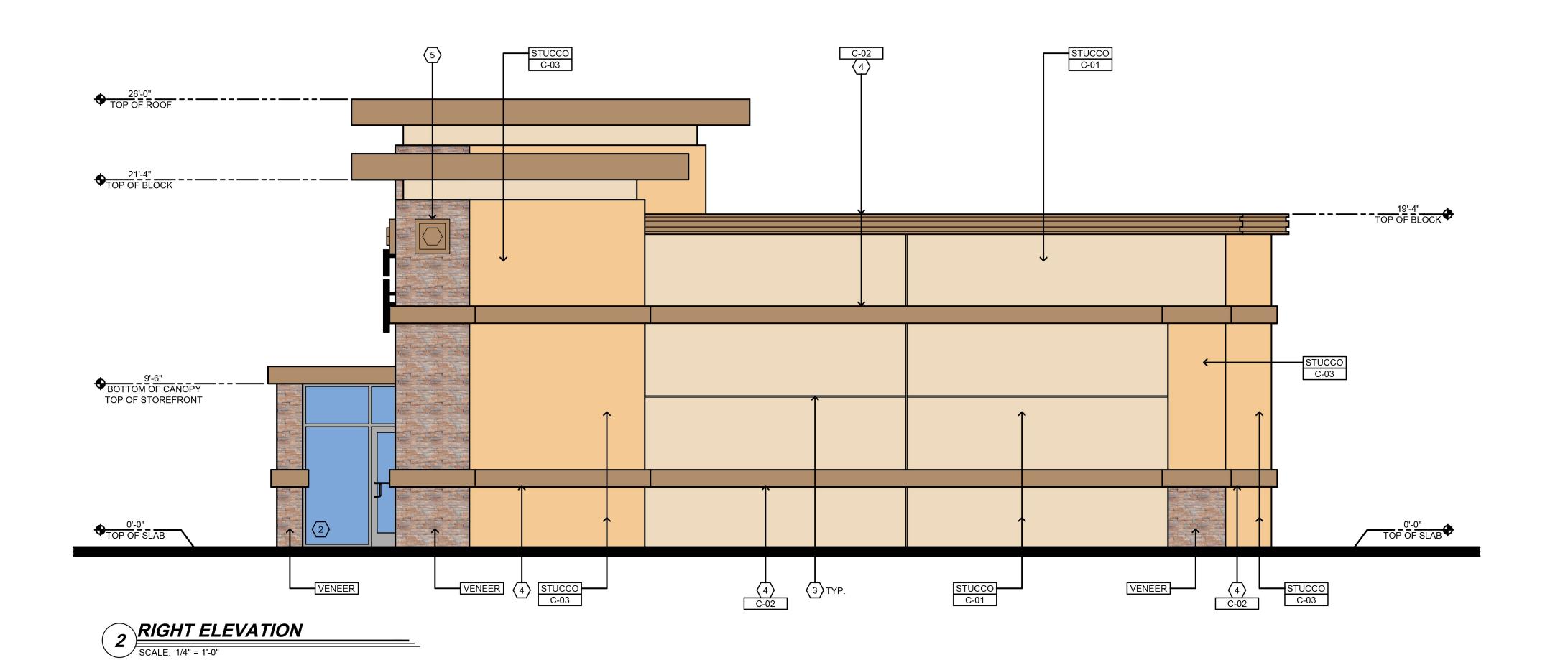
AMPM C-STORE ELEVATIONS

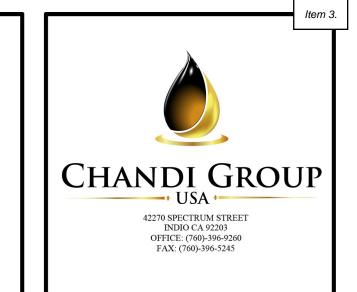
A-2

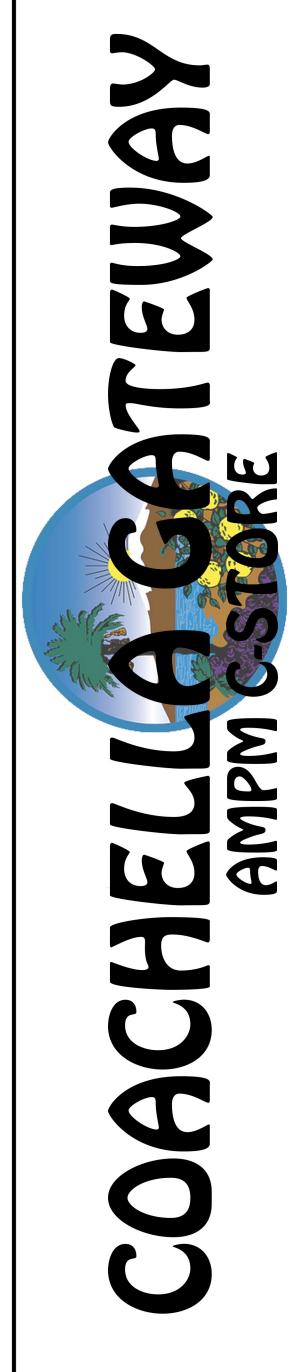


MAIN ENTRANCE ELEVATION

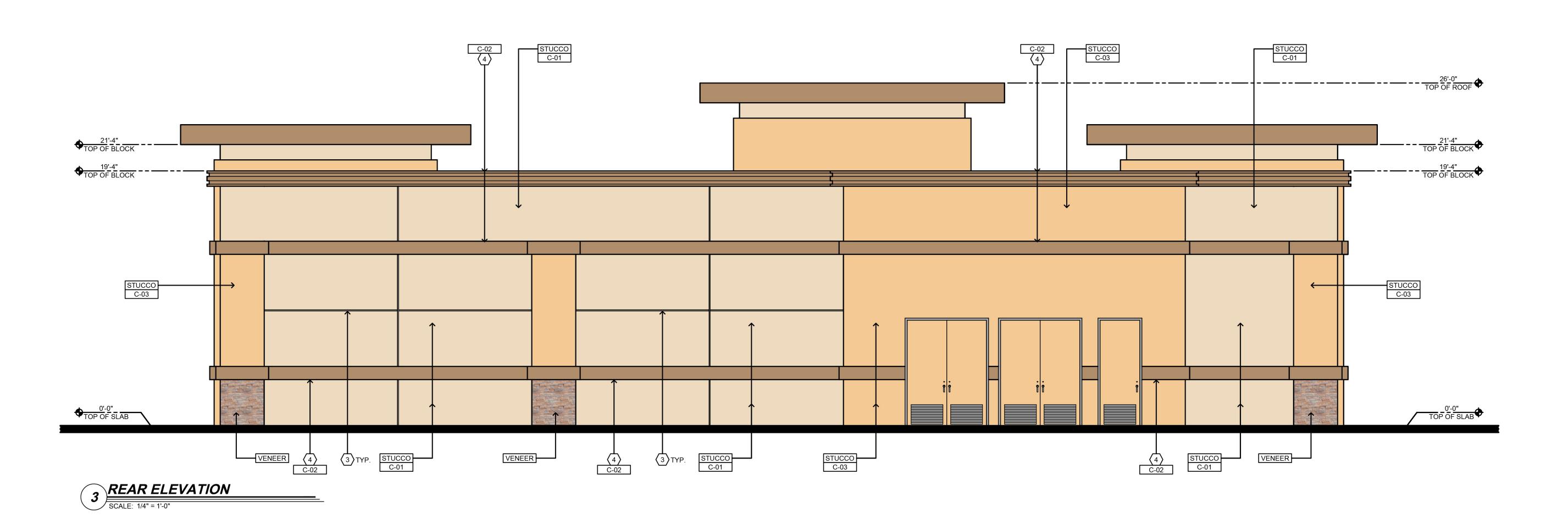
SCALE: 1/4" = 1'-0"

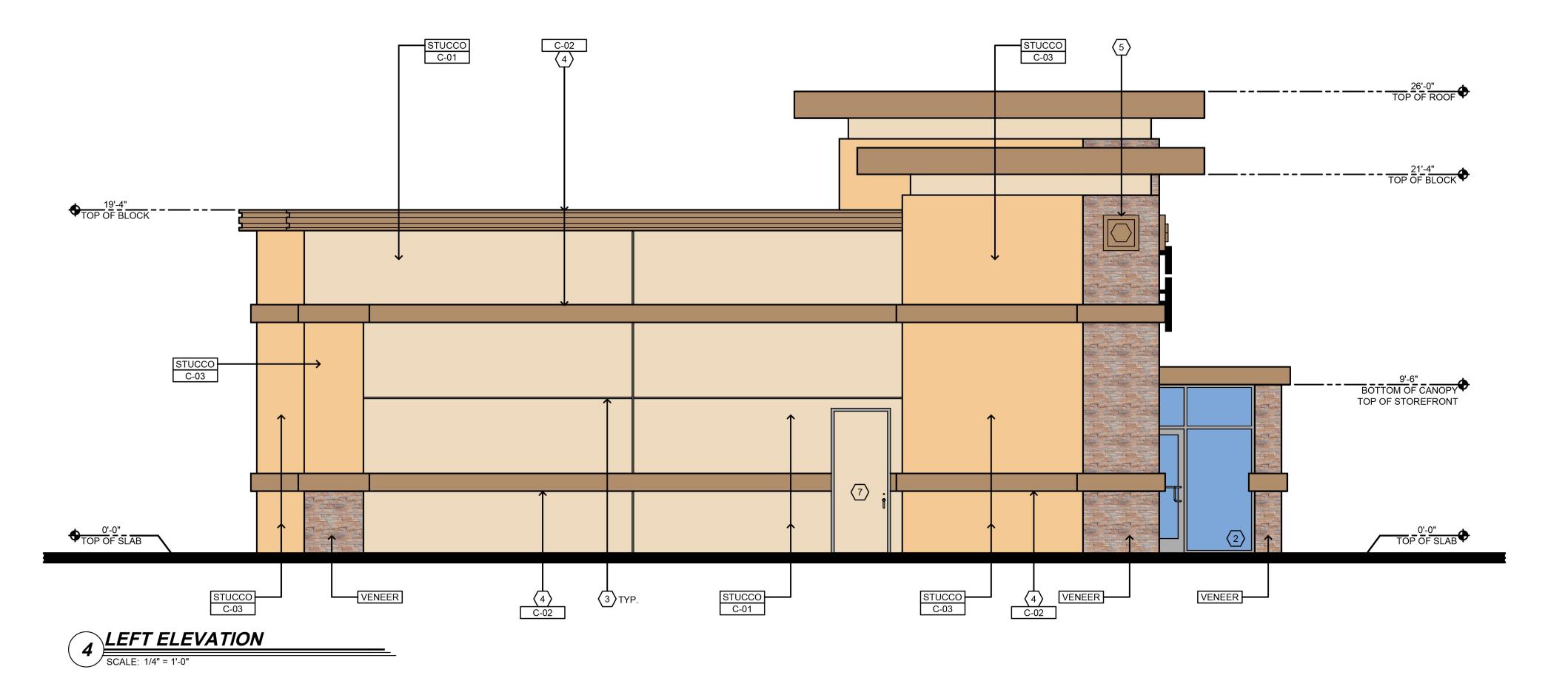






AMPM C-STORE **ELEVATIONS**





KEYED NOTES:

- (1) SIGNAGE (SEE BELOW NOTE)
 - ALUMINUM STOREFRONT SYSTEM
 - 1" EXPANSION JOINT
- CORNICE MOLDING
- DECORATIVE MEDALLION FRONT METAL CANOPY
- SERVICE DOOR AND FRAME
- ALUMINUM STOREFRONT ENTRANCE DOORS 9 8" HIGHT ILLUMINATE ADDRESS NUMBER
- NOTE:

 1. A SEPARATE PERMIT WILL BE REQUIRED FOR
- ALL EXTERIOR SIGNAGE
- 2. ALL ROOF EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW INCLUDING FREEWAY.
- 3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING

EXTERIOR FINISHES

STUCCO 7/8" SAND FINISH STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROVIDE CONTROL JOINTS AS REQUIRED

VENEER CULTURED STONE VENEER,

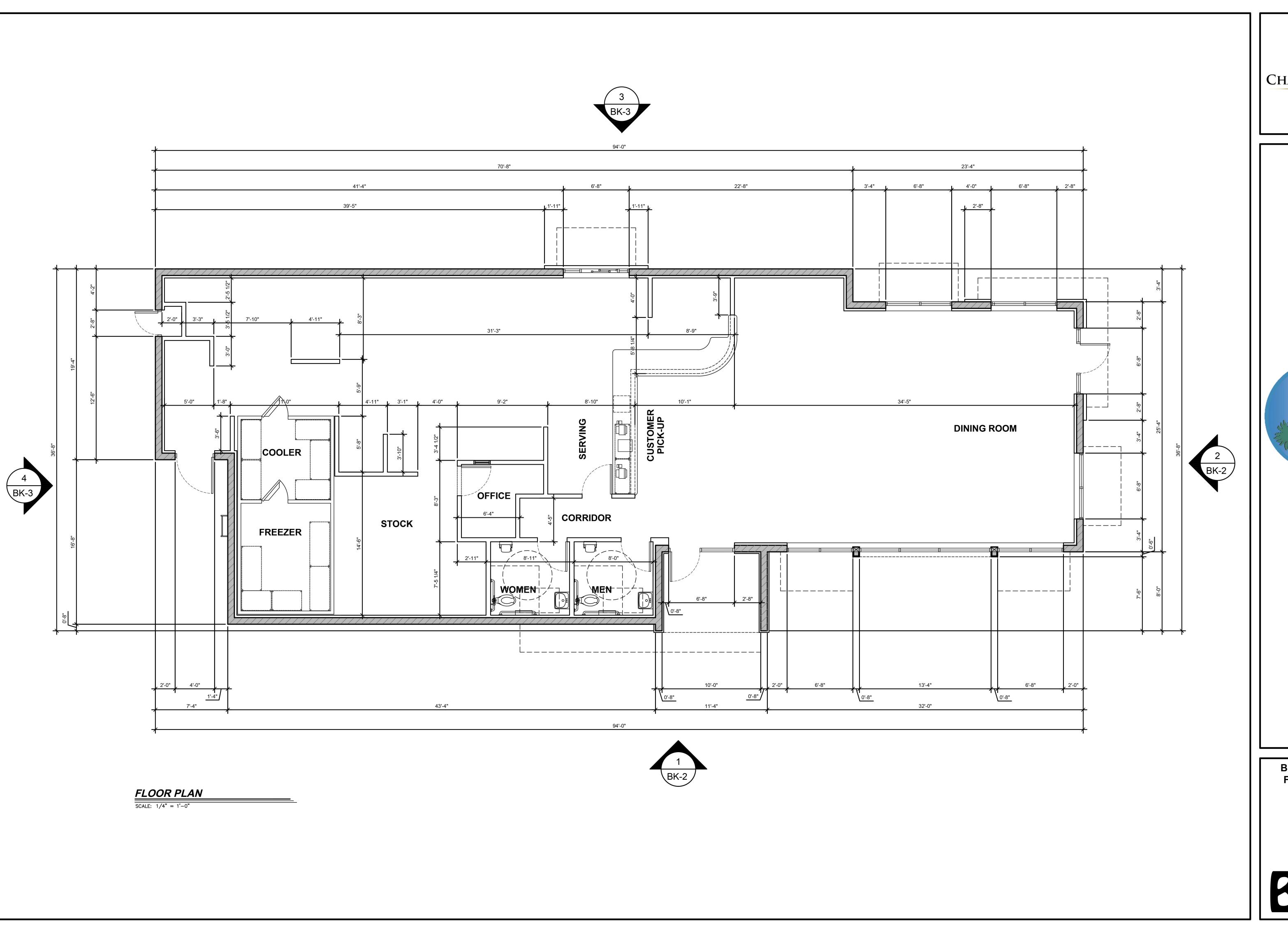
PRO-LEDGESTONE. COLOR: ALASKAN SUNSET.

EXTERIOR PAINTS

C-01 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7678 COTTAGE CREAM

C-02 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7680 LANYARD

C-03 MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 6888 PIZAZZ PEACH



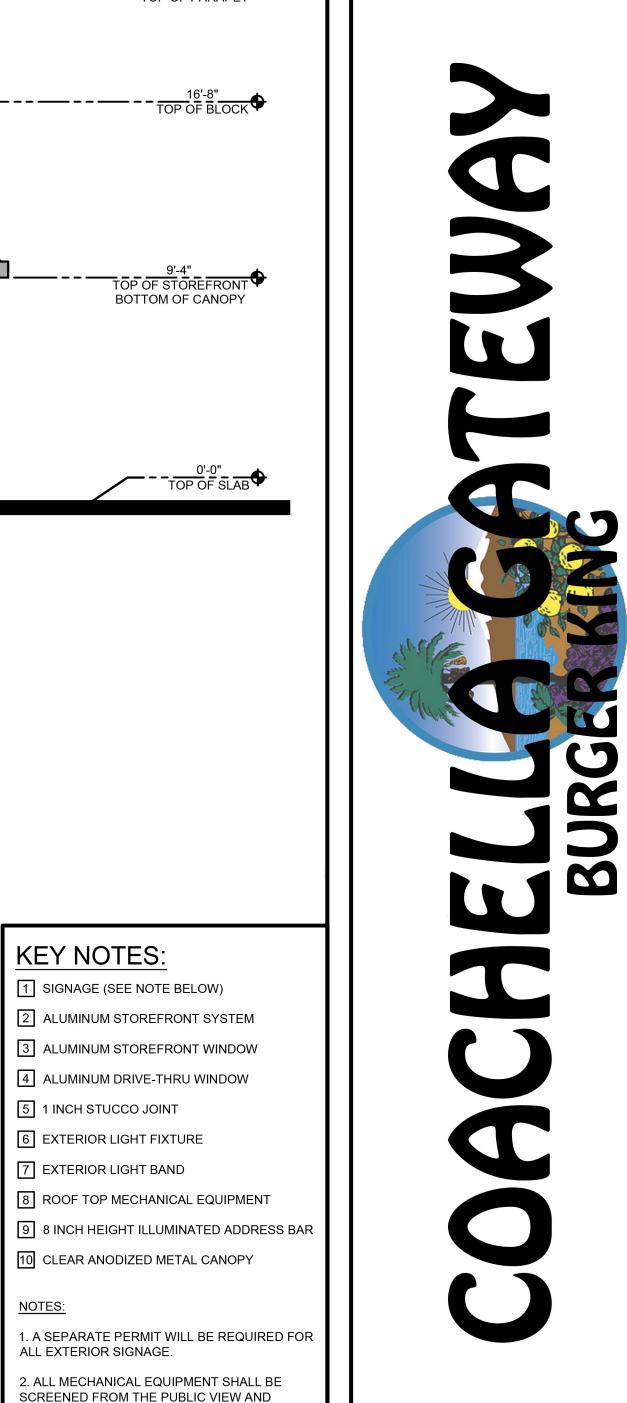




BURGER KING FLOOR PLAN

BK-1





KEY NOTES:

5 1 INCH STUCCO JOINT

7 EXTERIOR LIGHT BAND

6 EXTERIOR LIGHT FIXTURE

1 SIGNAGE (SEE NOTE BELOW)

2 ALUMINUM STOREFRONT SYSTEM

3 ALUMINUM STOREFRONT WINDOW

4 ALUMINUM DRIVE-THRU WINDOW

8 ROOF TOP MECHANICAL EQUIPMENT

10 CLEAR ANODIZED METAL CANOPY

2. ALL MECHANICAL EQUIPMENT SHALL BE SCREENED FROM THE PUBLIC VIEW AND ROADS.

EXTERIOR FINISHES:

STUCCO 7/8" FINE SAND FINISH STUCCO OVER METAL LATH AND 60 MIN. PAPER.

BRICK THIN FACE BRICK COLOR: OLD IRVINGTON O/S

PANELS CORRUGATED METAL PANELS COLOR: COAL BLACK

TILE DALTILE EXTERIOR CERAMIC TILE COLOR: SCARLET

EXTERIOR COLORS:

C-01 MANUFACTURER: PPG PAINTS
COLOR: PPG 12-13 CAMEL TAN

C-02 MANUFACTURER: PPG PAINTS COLOR: PPG 1011-7 DEEP ONYX

BURGER KING ELEVATIONS

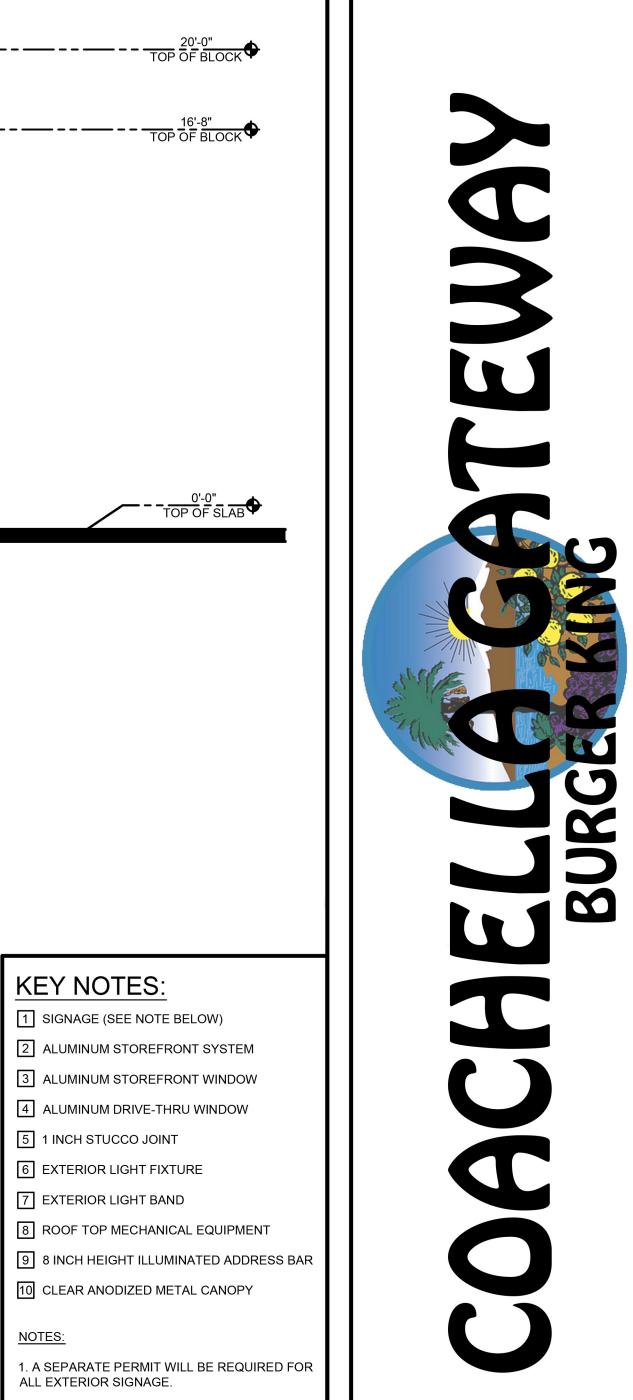


1 5TYP. 10 10 7 5TYP. 20'-0" TOP OF BLOCK BOTTOM OF CANOPY TOP OF STOREFRONT BOTTOM OF CANOPY TOP OF STOREFRONT BOTTOM OF CANOPY 3 --<u>0'-0"</u> TOP OF SLAB TOP OF SLAB PANELS BRICK PANELS TILE

2 RIGHT ELEVATION

SCALE: 1/4" = 1'-0"





KEY NOTES:

5 1 INCH STUCCO JOINT

6 EXTERIOR LIGHT FIXTURE

7 EXTERIOR LIGHT BAND

NOTES:

ROADS.

1 SIGNAGE (SEE NOTE BELOW)

2 ALUMINUM STOREFRONT SYSTEM

3 ALUMINUM STOREFRONT WINDOW

4 ALUMINUM DRIVE-THRU WINDOW

8 ROOF TOP MECHANICAL EQUIPMENT

10 CLEAR ANODIZED METAL CANOPY

2. ALL MECHANICAL EQUIPMENT SHALL BE SCREENED FROM THE PUBLIC VIEW AND

EXTERIOR FINISHES:

STUCCO

7/8" FINE SAND FINISH STUCCO

OVER METAL LATH AND 60 MIN.

PAPER.

BRICK THIN FACE BRICK COLOR: OLD IRVINGTON O/S

PANELS CORRUGATED METAL PANELS COLOR: COAL BLACK

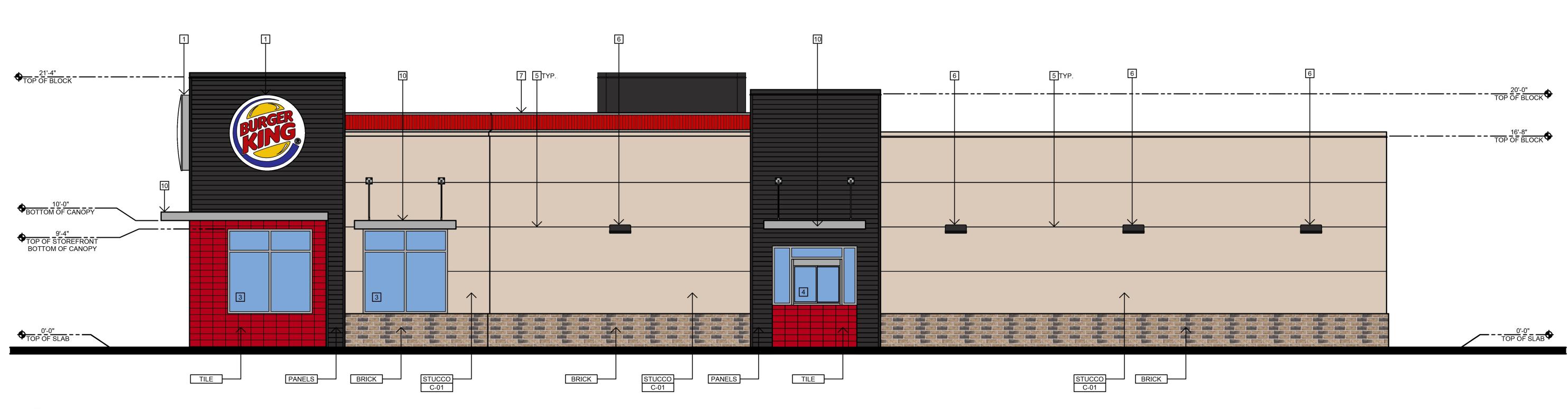
EXTERIOR COLORS:

C-01 MANUFACTURER: PPG PAINTS COLOR: PPG 12-13 CAMEL TAN

C-02 MANUFACTURER: PPG PAINTS COLOR: PPG 1011-7 DEEP ONYX

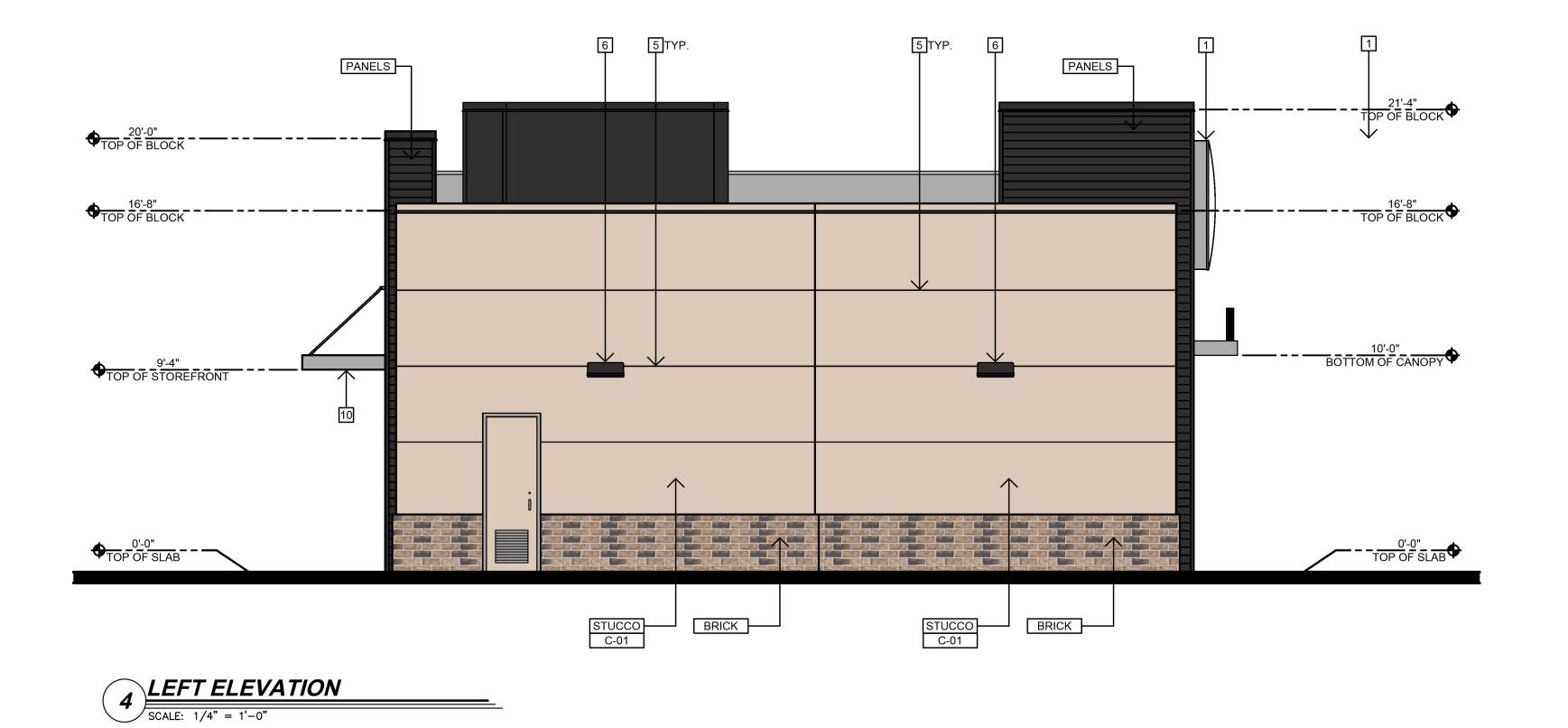
TILE DALTILE EXTERIOR CERAMIC TILE COLOR: SCARLET

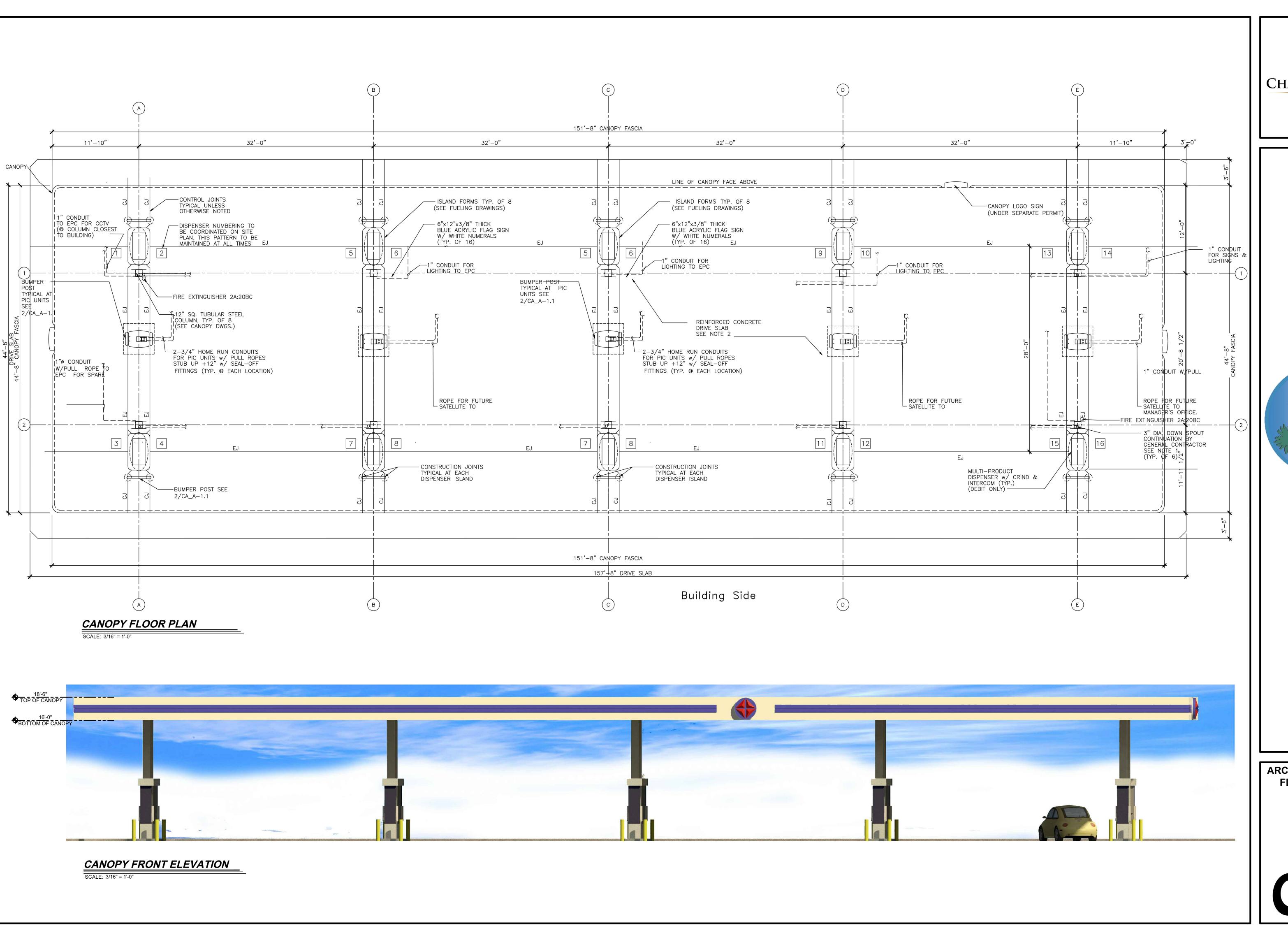
BURGER KING ELEVATIONS



3 DRIVE-THRU ELEVATION

SCALE: 1/4" = 1'-0"

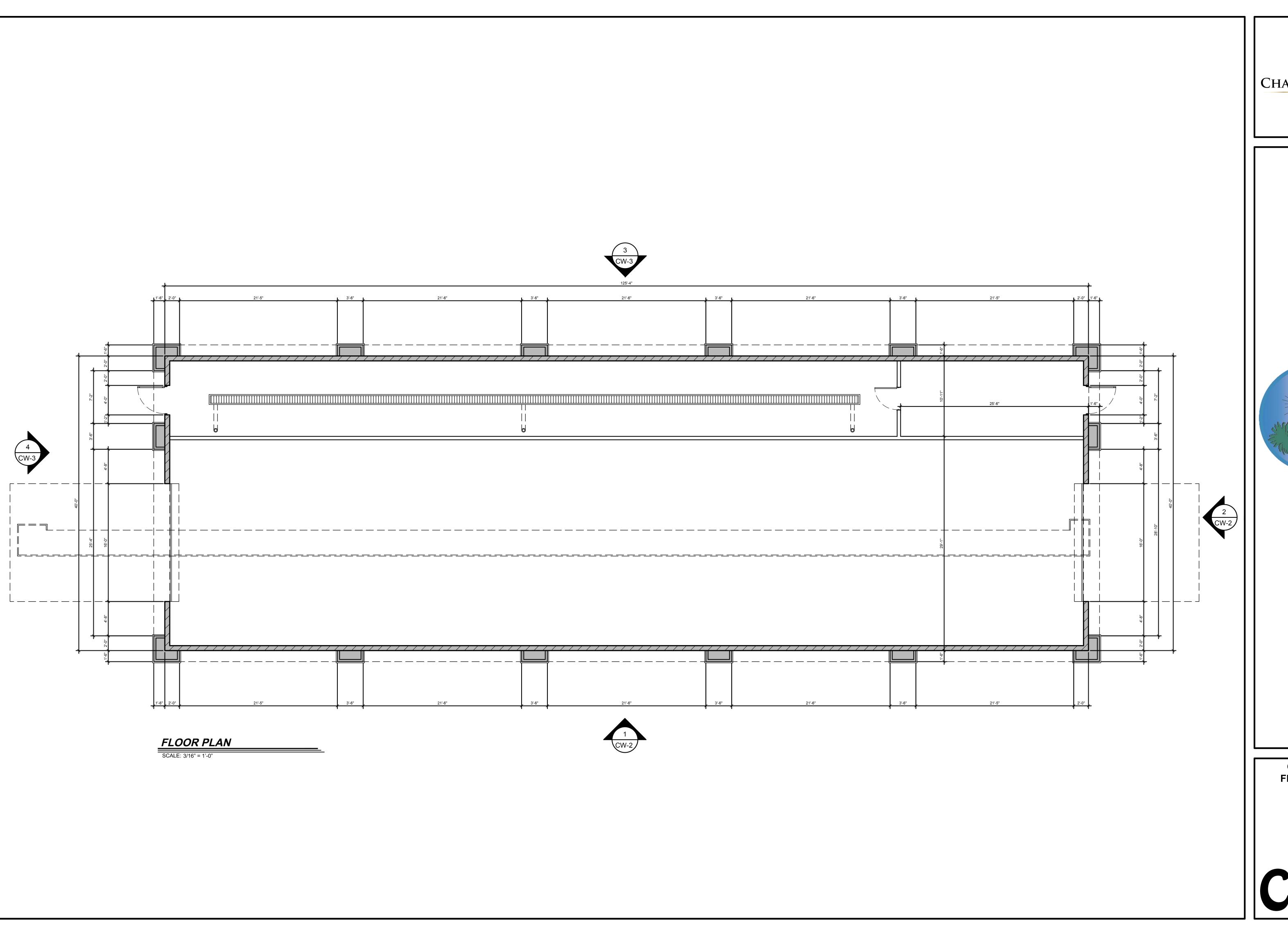






ARCO FUEL CANOPY
FLOOR PLAN &
ELEVATION

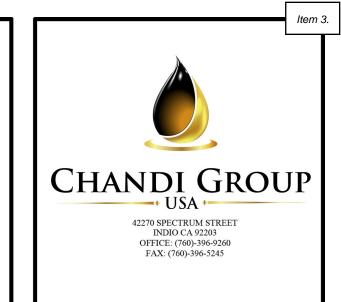
CP-1

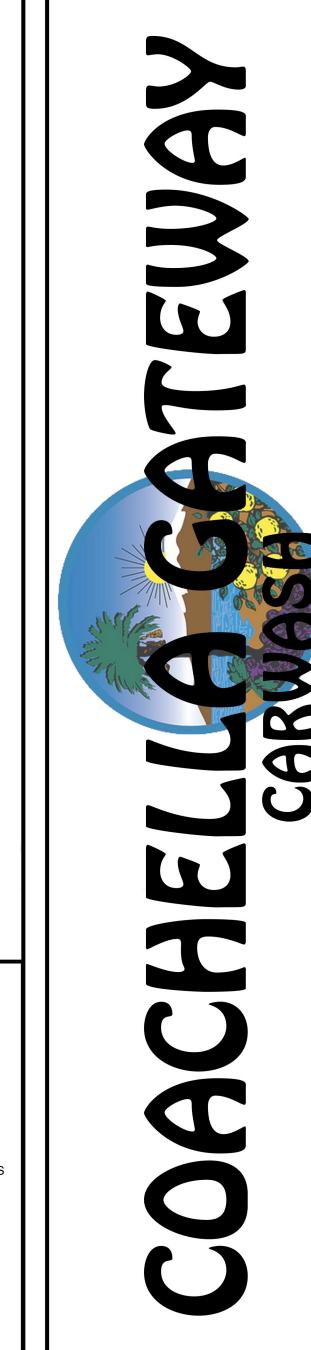


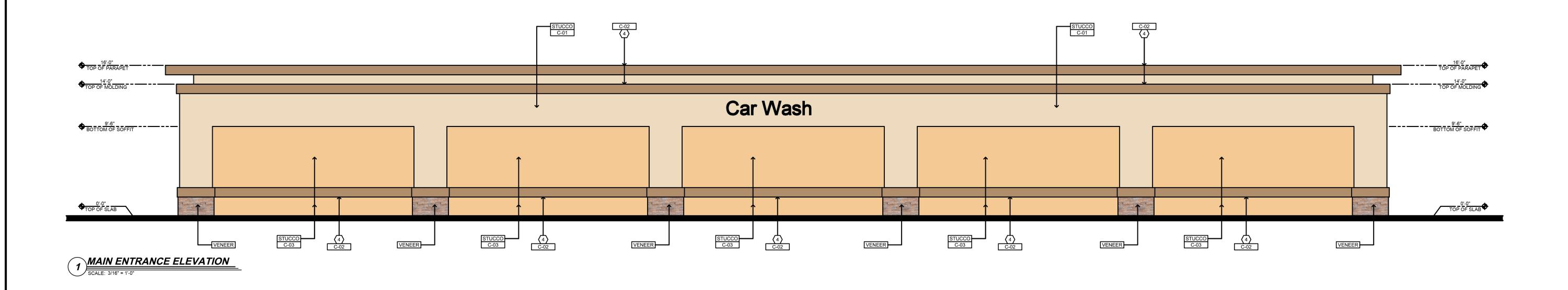


CARWASH FLOOR PLAN

CW-1







◆ 16'-0" TOP OF PARAPET ---TOP OF MOLDING 14'-0" TOP OF MOLDING TOP OF SLAB 0'-0" TOP OF SLAB RIGHT ELEVATION SCALE: 3/16" = 1'-0"

KEYED NOTES:

- (1) SIGNAGE (SEE BELOW NOTE)
- ALUMINUM STOREFRONT SYSTEM
- (3) 1" EXPANSION JOINT
- CORNICE MOLDING DECORATIVE MEDALLION
- FRONT METAL CANOPY SERVICE DOOR AND FRAME
- 8 ALUMINUM STOREFRONT ENTRANCE DOORS
- 8" HIGHT ILLUMINATE ADDRESS NUMBER
- NOTE:
 1. A SEPARATE PERMIT WILL BE REQUIRED FOR ALL EXTERIOR SIGNAGE
- 2. ALL ROOF EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW INCLUDING FREEWAY.
- 3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING

EXTERIOR FINISHES

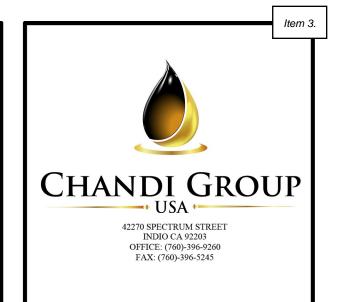
- STUCCO 7/8" SAND FINISH STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROVIDE CONTROL JOINTS AS REQUIRED
- VENEER CULTURED STONE VENEER, PRO-LEDGESTONE. COLOR: ALASKAN SUNSET.

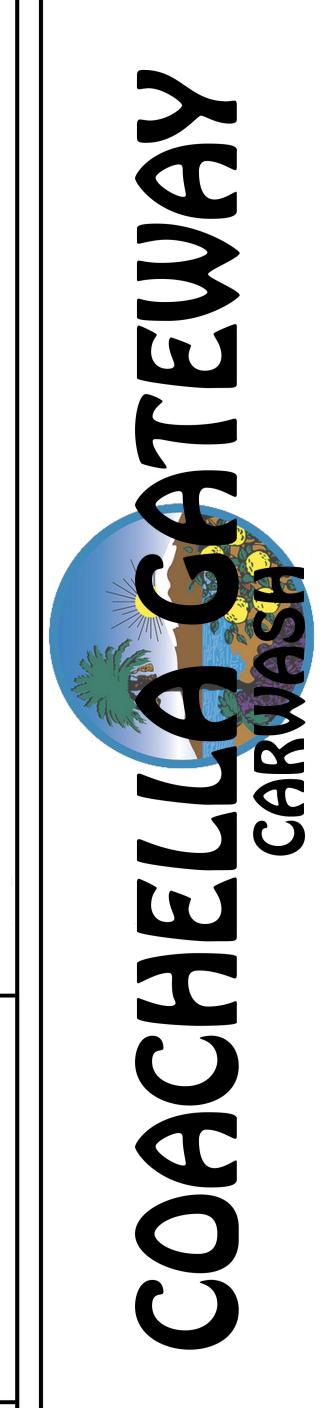
EXTERIOR PAINTS

- C-01 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7678 COTTAGE CREAM
- C-02 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7680 LANYARD
- C-03 MANUFACTURER: SHERWIN WILLIAMS
 COLOR: SW 6888 PIZAZZ PEACH

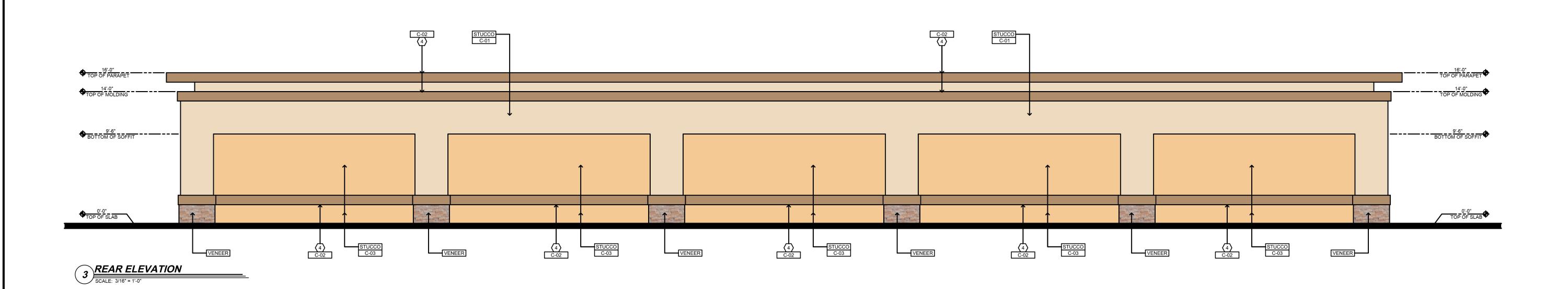
CARWASH

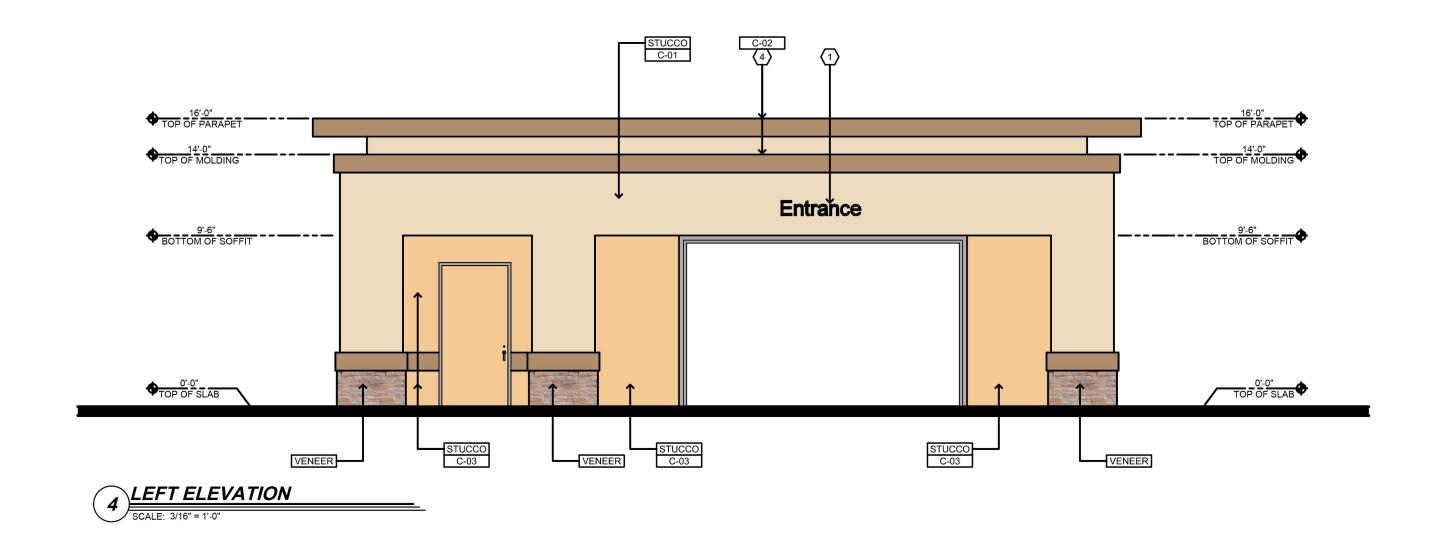
ELEVATIONS





CARWASH ELEVATIONS





KEYED NOTES:

- (1) SIGNAGE (SEE BELOW NOTE)
 - ALUMINUM STOREFRONT SYSTEM
- 1" EXPANSION JOINT
- CORNICE MOLDING DECORATIVE MEDALLION
- FRONT METAL CANOPY
- SERVICE DOOR AND FRAME ALUMINUM STOREFRONT ENTRANCE DOORS
- 9 8" HIGHT ILLUMINATE ADDRESS NUMBER
- NOTE:

 1. A SEPARATE PERMIT WILL BE REQUIRED FOR ALL EXTERIOR SIGNAGE
- 2. ALL ROOF EQUIPMENT SHALL BE SCREENED
- FROM PUBLIC VIEW INCLUDING FREEWAY.
- 3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING

EXTERIOR FINISHES

STUCCO 7/8" SAND FINISH STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROVIDE CONTROL JOINTS AS REQUIRED

VENEER CULTURED STONE VENEER,

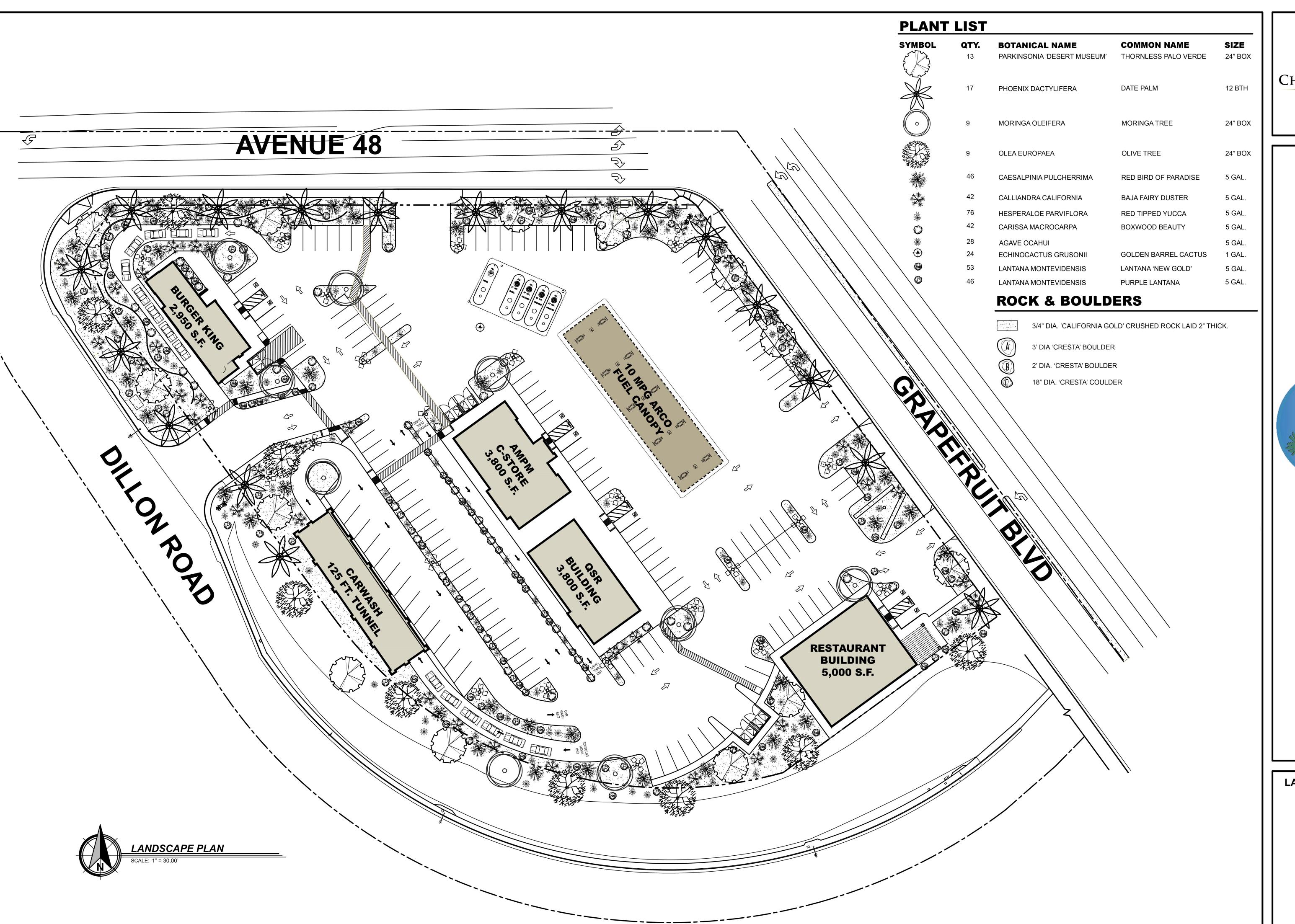
PRO-LEDGESTONE. COLOR: ALASKAN SUNSET.

EXTERIOR PAINTS

C-01 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7678 COTTAGE CREAM

C-02 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7680 LANYARD

C-03 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 6888 PIZAZZ PEACH





LANDSCAPE PLAN

L-I

MATERIAL SAMPLE BOARD

NEW BURGER KING RESTAURANT GRAPEFRUIT BLVD. COACHELLA CA





C-02 MANUFACTURER: PPG PAINTS
COLOR: PPG 1011-7 DEEP ONYX



BRICK VENEER

MANUFACTURER: PINE HALL BRICK

STYLE: OVERSIZED COLOR: OLD-IRVINGTON

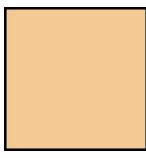
MATERIAL SAMPLE BOARD

NEW AMPM, CARWASH, QSR AND RESTAURANT GRAPEFRUIT BLVD. COACHELLA CA





MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 7680 LANYARD



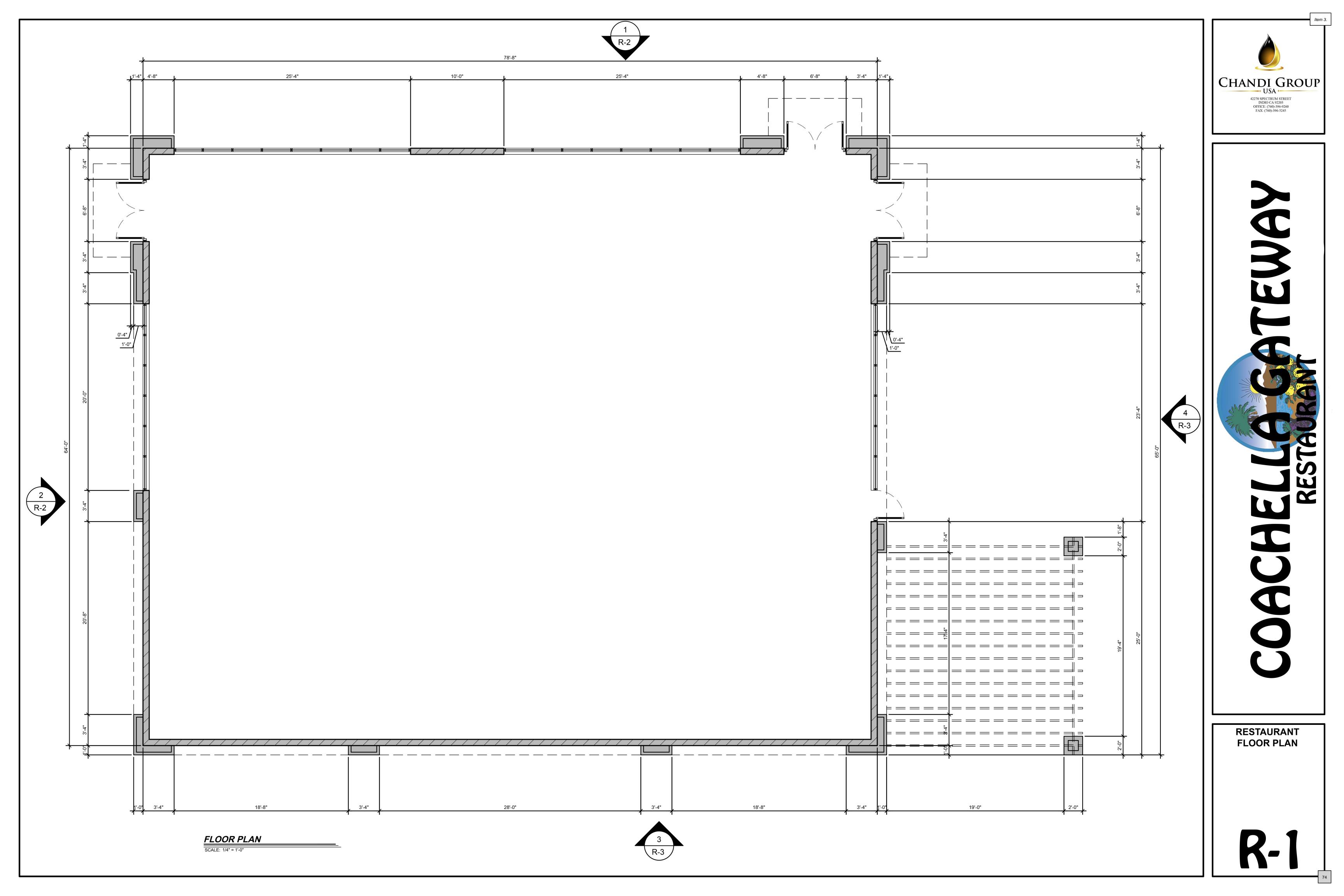
C-03 MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 6888 PIZAZZ PEACH

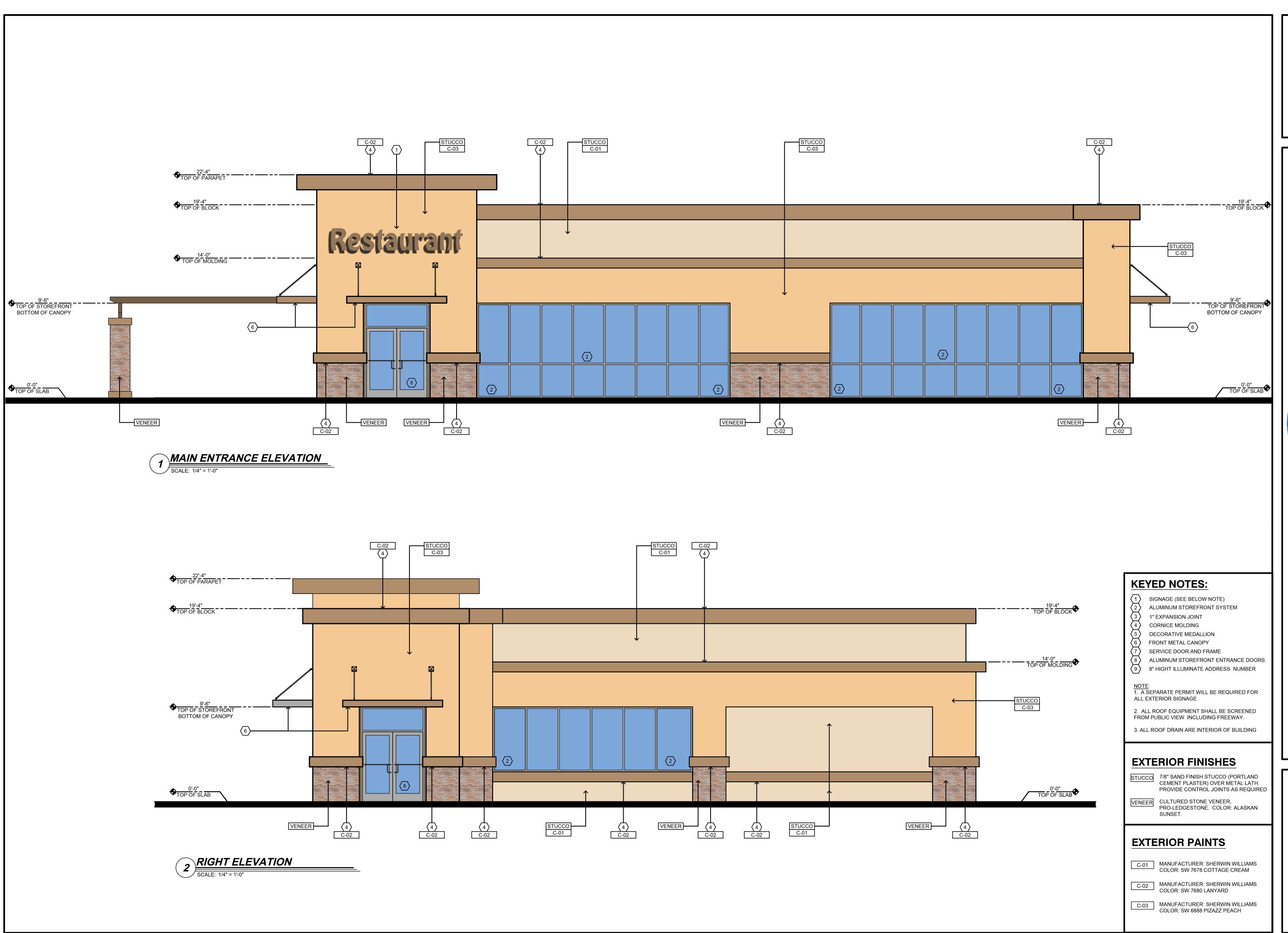


STONE VENEER

MANUFACTURER: CORONADO STONE PRODUCTS

STYLE: PRO-LEDGESTONE COLOR: ALASKAN SUNSET

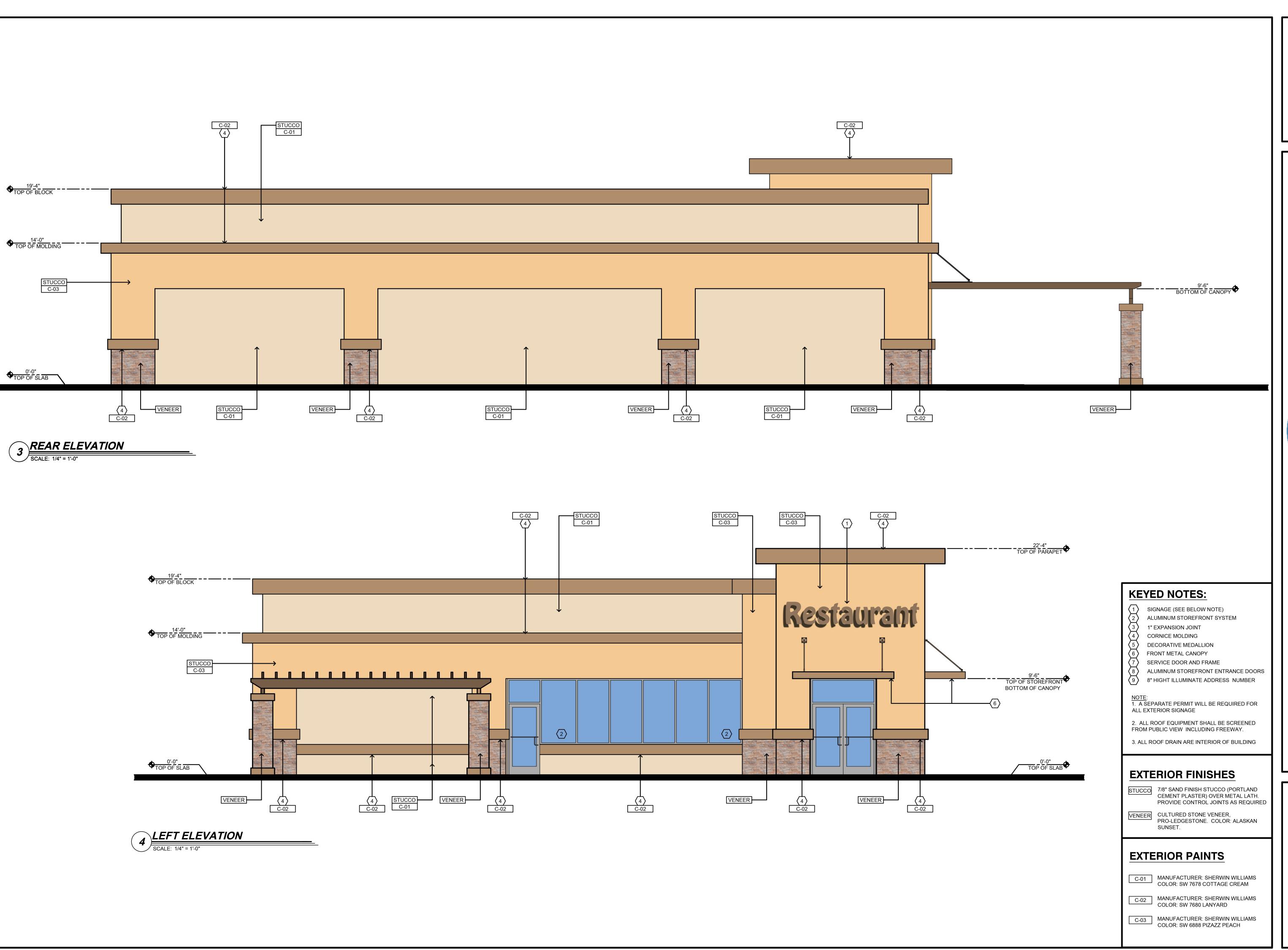






RESTAURANT ELEVATIONS

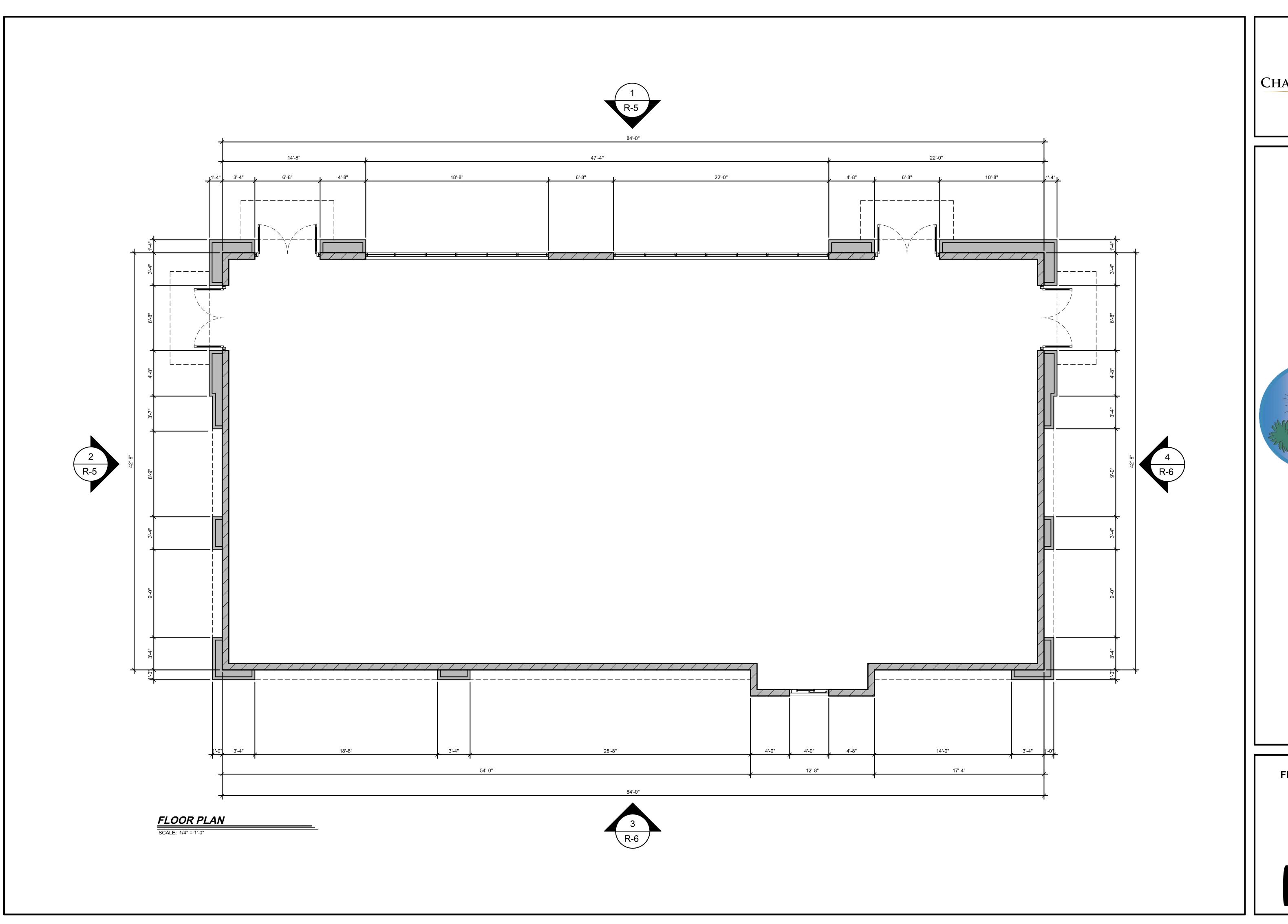
R-2





RESTAURANT ELEVATIONS

R-3





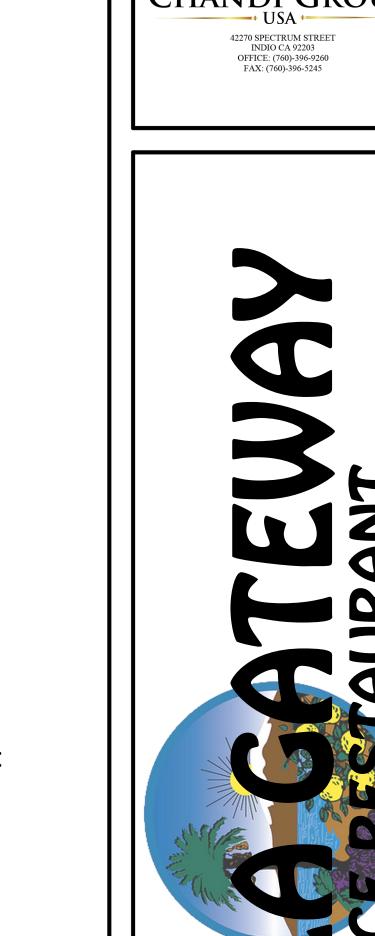


QSR FLOOR PLAN

R-4







KEYED NOTES:

(1) SIGNAGE (SEE BELOW NOTE)

1" EXPANSION JOINT CORNICE MOLDING

DECORATIVE MEDALLION
FRONT METAL CANOPY
SERVICE DOOR AND FRAME

ALUMINUM STOREFRONT SYSTEM

8" HIGHT ILLUMINATE ADDRESS NUMBER

NOTE:

1. A SEPARATE PERMIT WILL BE REQUIRED FOR ALL EXTERIOR SIGNAGE

2. ALL ROOF EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW INCLUDING FREEWAY.

3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING

STUCCO 7/8" SAND FINISH STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH. PROVIDE CONTROL JOINTS AS REQUIRED

VENEER CULTURED STONE VENEER, PRO-LEDGESTONE. COLOR: ALASKAN SUNSET.

C-01 MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 7678 COTTAGE CREAM

C-02 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7680 LANYARD

C-03 MANUFACTURER: SHERWIN WILLIAMS
COLOR: SW 6888 PIZAZZ PEACH

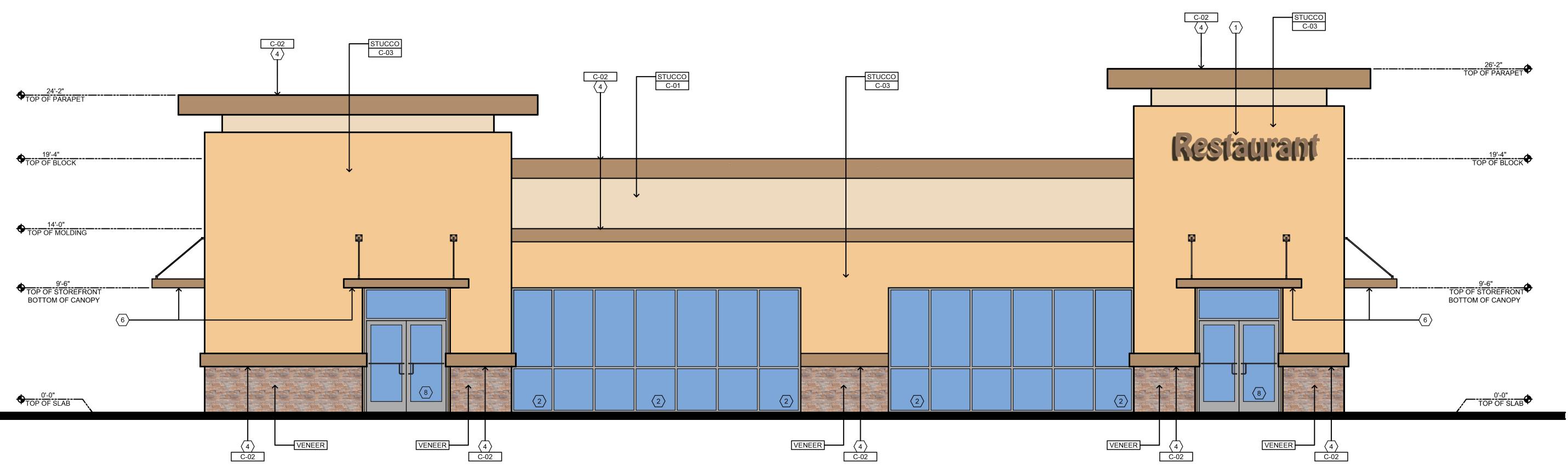
EXTERIOR FINISHES

EXTERIOR PAINTS

ALUMINUM STOREFRONT ENTRANCE DOORS

QSR ELEVATIONS

R-5

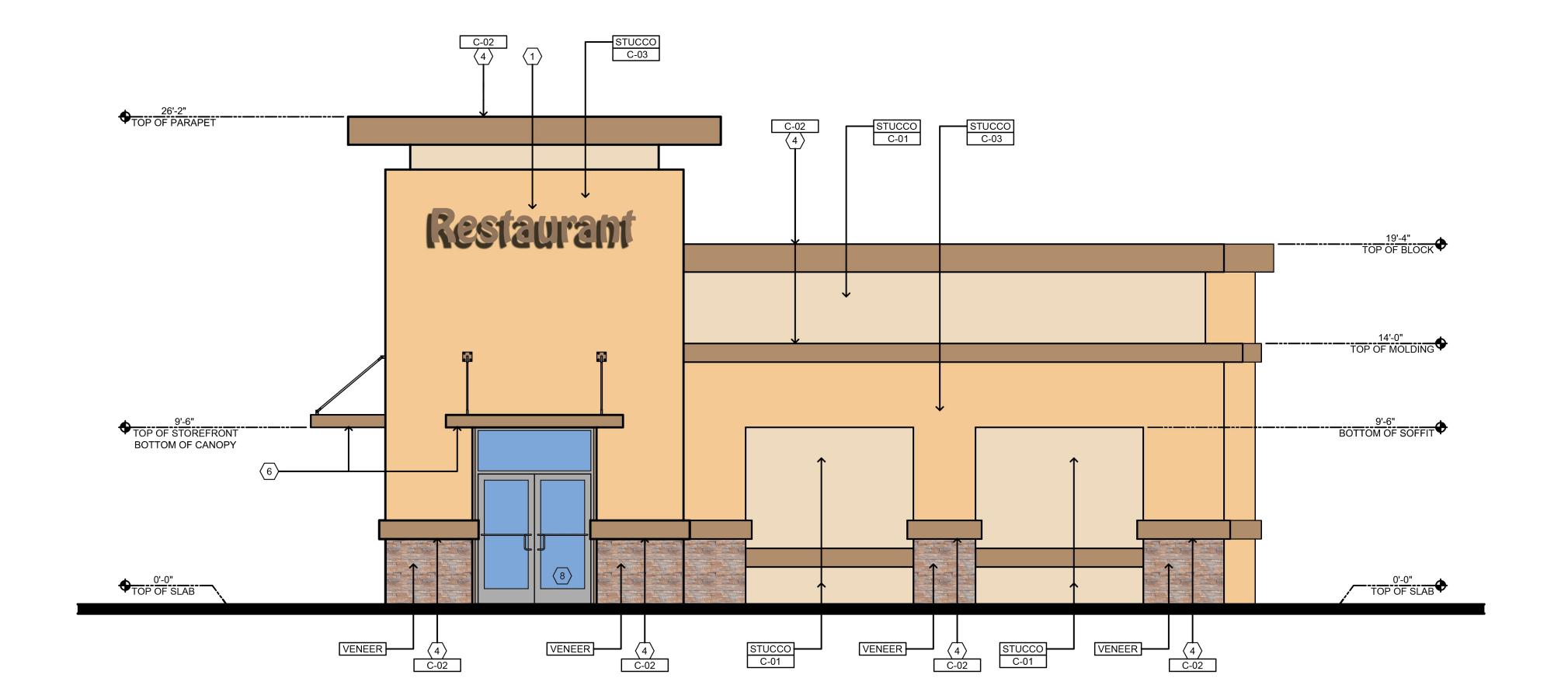


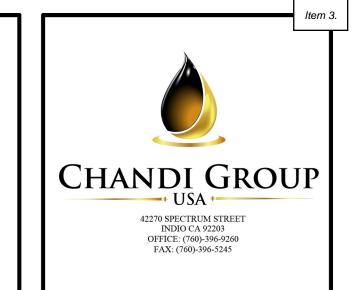
1 MAIN ENTRANCE ELEVATION

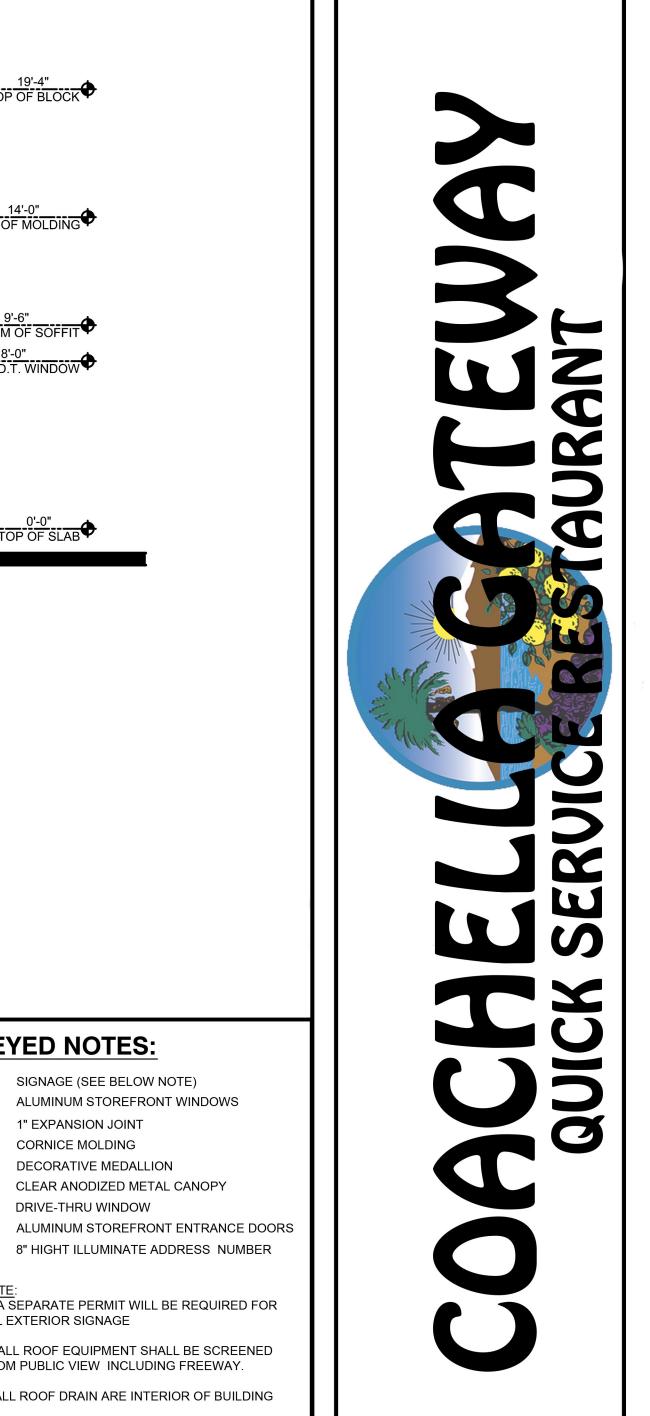
SCALE: 1/4" = 1'-0"

2 RIGHT ELEVATION

SCALE: 1/4" = 1'-0"







KEYED NOTES:

(1) SIGNAGE (SEE BELOW NOTE)

1" EXPANSION JOINT

DECORATIVE MEDALLION

DRIVE-THRU WINDOW

4 CORNICE MOLDING

ALUMINUM STOREFRONT WINDOWS

CLEAR ANODIZED METAL CANOPY

9 8" HIGHT ILLUMINATE ADDRESS NUMBER

NOTE:

1. A SEPARATE PERMIT WILL BE REQUIRED FOR ALL EXTERIOR SIGNAGE

2. ALL ROOF EQUIPMENT SHALL BE SCREENED FROM PUBLIC VIEW INCLUDING FREEWAY.

3. ALL ROOF DRAIN ARE INTERIOR OF BUILDING

T/8" SAND FINISH STUCCO (PORTLAND CEMENT PLASTER) OVER METAL LATH.
PROVIDE CONTROL JOINTS AS REQUIRED

VENEER CULTURED STONE VENEER, PRO-LEDGESTONE. COLOR: ALASKAN

C-01 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7678 COTTAGE CREAM

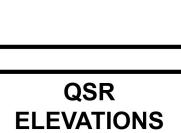
C-02 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 7680 LANYARD

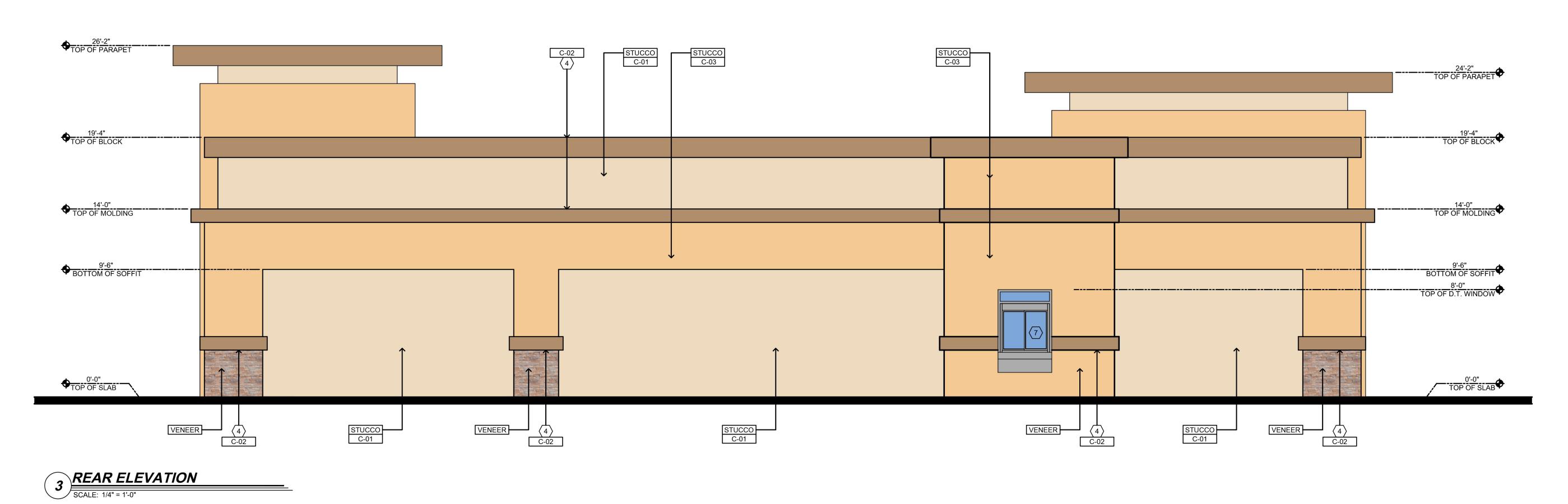
C-03 MANUFACTURER: SHERWIN WILLIAMS COLOR: SW 6888 PIZAZZ PEACH

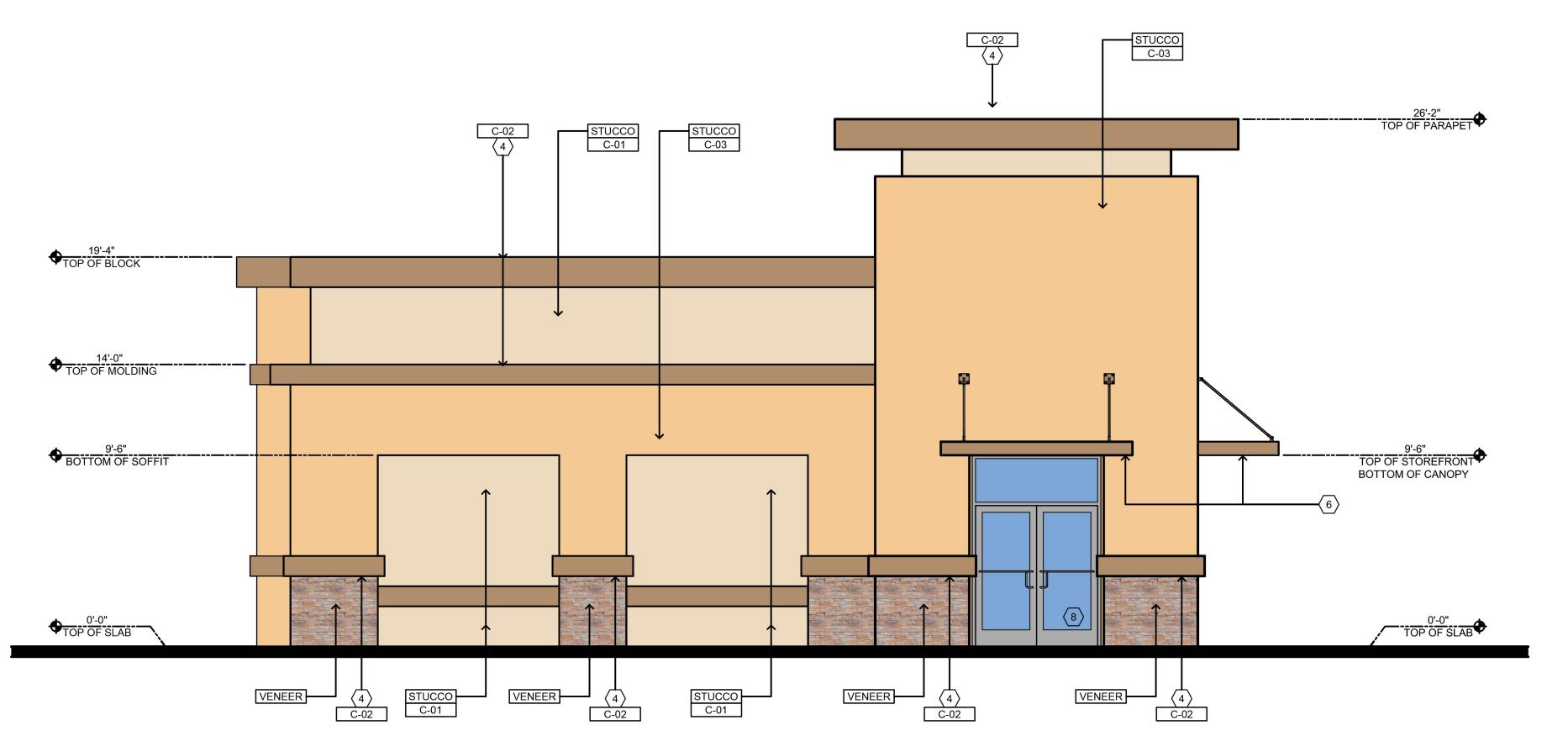
EXTERIOR FINISHES

SUNSET.

EXTERIOR PAINTS







4 LEFT ELEVATION

SCALE: 1/4" = 1'-0"



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				V	E CONTROL OF	

DEPT.

INITIALS

DATE

BUILDING

PLANNING

ENGINEERING

Coachella Gateway 48055 Grapefruit Blvd. Coachella CA.

PROJECT DIRECTORY

OWNER:

CHANDI GROUP USA INC

42270 SPECTRUM ST INDIO, CA, 92203 PH: (760) 396-9260

DESIGNER:

ALEX CUEVAS

AGC DESIGN CONCEPT INC 28524 CONSTELLATION WAY

VALENCIA, CA 91355 PH: (661) 295-1111

SIGN CONSULTANT:

SERGIO RODRIGUEZ

81211 INDIO BLVD.STE 2B

INDIO, CA 92201 PH: (760) 619-6565

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CARWASH TENANT (GO-GO EXPRESS WASH)

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14 RESTAURANT TENANT

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A. INTRODUCTION

The intent of this sign criteria is to provide the guidelines necessary to achieve a visually coordinated, balanced and appealing sign environment, harmonious with the architecture of the project, while maintaining provisions for individual graphic expression.

Performance of this sign criteria shall be rigorously enforced and any nonconforming sign shall be removed by the tenant or his sign contractor at their expense, upon demand by the owner.

Exceptions to these standards shall not be permitted without the approval from the Landlord and will require approval of a modification to the sign program application by the city.

Accordingly, the Landlord will retain full rights of approval for any sign used in the center.

No sign shall be installed without the written Landlord approval and the required City permits.

This sign program is designed in coordination with the Coachella Municipal Code Zoning Ordinance (section 070.04)

B. GENERAL LANDLORD/TENANT REQUIREMENTS:

- 1. Proir to city submittal or sign fabrication, each tenant shall submit to landlord for written approval, three (3) copies of the fully detailed shop drawings of their proposed sign, indicating conformance with the sign criteria herein outlined.
- 2. The Landlord shall determine and approve the availability and position of a tenant name on any ground sign(s).
- The tenant shall pay for all signs, related materials and installation fees (including all inspection costs).
- 4. The tenant shall obtain all necessary permits.
- 5. The tenant shall be responsible for fulfillment of all requirements of this sign criteria.
- 6. It is the responsibility of the Tenants sign company to verify all conduit and transformer locations and service access prior to fabrication.
- 7. Should a sign be removed, it is the Tenant's responsibility to patch and paint all holes to match finish and color of adjacent surface.
- 8. The Landlord may, at his sole discretion and at the Tenant's expense, correct, replace, or remove any sign that is installed without written approval and/or that is deemed unacceptable pertaining to this sign program.
- 9. If the Tenant chooses to change his exterior sign at anytime during the term of his lease, then the Tenant must comply with the requirements set forth herein and any future modifications, amendments, revisions or changes which have been made to this sign program for this center after the execution of his lease agreement.

C. GENERAL SIGN CONSTRUCTION REQUIREMENTS:

- All signs and their installation shall comply with all local building and electrical codes.
- All electrical signs will be fabricated by a U.L. approved sign company, according to U.L. specifications and bear U.L. Label.
- Sign company to be fully licensed with the City and State and shall provide proof of full Workman's Compensation and general liability insurance.
- All penetrations of building exterior surfaces are to be sealed, waterproof, and in color & finish to match existing exterior.
- Internal illumination to be 30 milliamp neon, installed and labeled in accordance with the "National Board of Fire Underwriters Specifications". Or L.E.D.'S wish ever sign company decied
- 6. Painted surfaces to have a semi gloss finish. Only paint containing acrylic polyurethane products can be used.
- 7. Logo and letter heights shall be as specified and shall be determined by measuring the normal capital letter of a type font exclusive of swashes, ascenders, and descenders.
- 8. All sign fabrication work shall be of excellent quality. All logo images and type-styles shall be accurately reproduced. Lettering that approximates type-styles shall not be acceptable. The Owner reserves the right to reject any fabrication work deemed to be below standard.

- 9. All lighting must match the exact specification of the approved working drawings. No exposed conduits or race ways will be allowed.
- 10. Signs must be made of durable rust –inhibited materials that are appropriate and complimentary to the building.
- 11. Color coatings shall exactly match the colors specified on the approved plans.
- 12. Joining of materials (e.g., seams) shall be finished in such a way as to be unnoticeable. Visible welds shall be continuous and ground smooth. Rivets, screws, and other fasteners that extend to visible surfaces shall be flush, filled, and finished so as to be unnoticeable.
- 13. Finished surfaces of metal shall be free from canning and warping. All sign finishes shall be free from dust, orange peel, drips, and runs and shall have a uniform surface conforming to the highest standards of the industry.
- 14. In no case shall any manufacturer's label be visible from the street from normal viewing angles.
- 15. Exposed raceways are not permitted unless they are incorporated into the overall sign design and acceptable to the Landlord.
- 16. Exposed junction boxes, lamps, tubing, or neon crossovers of any kind are prohibited.
- 17. All materials used in signage construction and installation must be new. No used materials will be allowed.

Coachella Gateway 48055 Grapefruit Blvd. Coachella CA.

D. SIGNAGE SPECIFICATIONS:

The intent of this criteria is to encourage creativity to ensure the individuality of each tenant sign as opposed to similar sign design, construction, and colors repeated throughout the project.

The following types of construction will be allowed:

Acrylic face channel letters
Through face and halo channel letters
Reverse pan channel letters
Open pan channel letters
Push thru letters and logos in aluminum cabinets
Logo modules with applied vinyl graphics.
Flat cut out dimensional shapes and accents
Metal screen mesh

The use of at least two types of the above to be incorporated into each sign design is encouraged unless otherwise approved by Landlord..

The idea of using dissimilar materials and creating signs with varying colors, layers and textures will create an exciting and appealing retail environment.

Stacked copy is permitted

Ascending and descending shapes shall not be included in allocated square footage except for the area they occupy.

E. PROHIBITED SIGNS:

1. Signs constituting a Traffic Hazard

No person shall install or maintain, or cause to be installed or maintained, any sign which simulates or imitates in size, color, lettering or design any traffic sign or signal, or which makes use of the words "STOP", "LOOK", "DANGER" or any words, phrases, symbols, or characters in such a manner as to interfere with, mislead or confuse traffic.

2. Signs in Proximity to Utility Lines:

Signs which have less horizontal or vertical clearance from authorized communication or energized electrical power lines that are prescribed by the laws of the State of California are prohibited.

- 3. Painted letters will not be permitted.
- 4. Wall signs may not project above the top of a parapet, the roof line at the wall, or roof line.
- 5. There shall be no signs that are flashing, moving or audible.
- 6. Signs must be architecturally compatible with the entire center.
- 7.No sign shall project above or below the sign-able area. The signable area is defined in the attached Exhibit for minor and major tenants.
- 8. Vehicle Signs:

Signs on or affixed to trucks, automobiles, trailers, or other vehicles which advertise, identify, or provide direction to a use or activity not related to its lawful activity are prohibited.

9. Light Bulb Strings:

External displays, other than temporary decorative holiday lighting installed by landlord which consists of unshielded light bulbs are prohibited. An exception hereto may be granted by the Landlord when the display is an integral part of the design character of the activity to which it relates.

- 10. Banners, Pennants & Balloons Used for Advertising Purposes: Temporary flags, banners, or pennants, or a combination of same constituting an architectural feature which is an integral part of the design character of a project may be permitted subject to Municipal Code requirements, Landlord's, and City approval.
- 11. Billboard Signs are not permitted.
- 12. The use of permanent sale signs is prohibited. The temporary use of these signs are limited to a thirty-day period and is restricted to signs affixed to the interior of windows which do not occupy more that 20% of the window area. Each business is permitted a total of not more than ninety (90) days of temporary window sale signs per calendar year.

F. ABANDONMENT OF SIGNS:

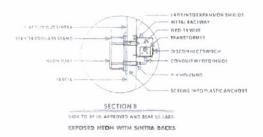
Any tenant sign left after thirty (30) days from vacating premises shall become the property of Landlord. Cost for the removal of abandoned sign shall be in accordance with lease agreement.

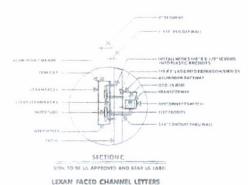
G. INSPECTION:

Landlord reserves the right to hire an independent electrical engineer at the Tenant's sole expense to inspect the installation of all Tenant's signs and to require the Tenant to have any discrepancies and/or code violations corrected at the Tenant's expense.



ACTIVIC FACED CHANNEL LETTERS





THROUGH FACE AND HALD ILLUMINATION

SECTION A

New single faced internally illuminated Lexan faced channel letter display.

Use standard aluminum construction with Matthews (or equivalent) semi gloss acrylic polyurethane finish. Faces use acrylic with 3/4" bronze trim cap, Illuminate with 30 ma nean.

Paint 5" deep returns duranadic branze.

SECTION B

New single faced illuminated wall display. Use multi layered flat cut out 1/2" Sintra graphics with applied neon overlays.

Flush mount Sinfra to wall. Paint Sinfra Matthews (or equivalent) semi gloss acrylic polyurethane. Use neon overlays with PK type housings.

SECTION C

New single faced internally illuminated Lexan faced channel letter display with through face and halo illumination.

Use standard aluminum construction with Matthews for

equivalent) semi gloss acrylic polyurethane finish, Faces use Lexan with 3/4" trim cap. Illuminate with 30 ma neon through face and halo.

Paint returns duronodic bronze

SECTION D

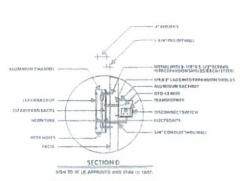
New single faced internally illuminated aluminum faced channel letter display with through face and halo illumination.

Use standard aluminum construction with Matthews for equivalenti semi gloss acrylic polyurethane finish.
Route out where graphics occur and back up with Lexan.
Illuminate with 30 ma neon through face and halo.
Paint aluminum face and returns.

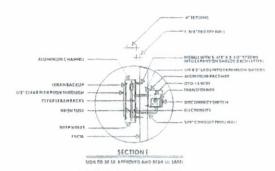
SECTION F

Same as "D" except route out where graphics occur and push through 1/2" clear plex with applied vinyl overlays.

Note: All signs may utilize digital vinyl printing on faces of illuminated or non illuminated surfaces.



REVERSE PAN CHANNELS THROUGH FACE AND HALO ILLUMINATION



REVERSE PAN CHANNELS THROUGH FACE AND HALO ILLUMINATION PUSH THROUGH GRAPHICS

Coachella Gateway 48055 Grapefruit Blvd. Coachella CA.

OF ADJACENT SURFACE

60% OF ADJACENT SURFACE OR 6'-0" MAX LETTER (WHICHEVER IS LESS)

MAJOR TENANT

MAJOR TENANTS (7,50) TO 99,999 SQUARE FEET)

MAXIMUM 70%____
OF ADJACENT SURFACE

60% OF ADJACENT SURFACE OR 4'-0" MAX LETTER (WHICHEVER IS LESS) PAD TENANT

MAXIMUM 70% ____ OF ADJACENT SURFACE

60% OF ADJACENT SURFACE OR 3'-0" MAX LETTER [WHICHEVER IS LESS] SHOP TENANT

SHOP TENANTS (UP TO 7,500 SQUARE FEET)

SIZE:TWO SQUARE FOOT OF SIGN AREA PER LINEAR
FOOT OF LEASED PREMISES, MAXIMUM OF
350 SQUARE FEET TOTAL PER ELEVATION.
MATERIALS: VARIETY OF TYPES PER SIGN CRITERIA,
SINGLE OR MULTIPLE TYPES OF CONSTRUCTION ALLOWED.
ILLUMINATION: YES
COPY: TENANT NAME AND / OR LOGO

HEIGHT: SIXTY PERCENT OF ADJACENT SURFACE LENGTH: SEVENTY FIVE PERCENT OF ADJACENT SURFACE TYPEFACE: CUSTOM LOGO AND TYPE OK

TYPEFACE: CUSTOM LOGO AND TYPE (
COLORS: CUSTOM COLORS OK

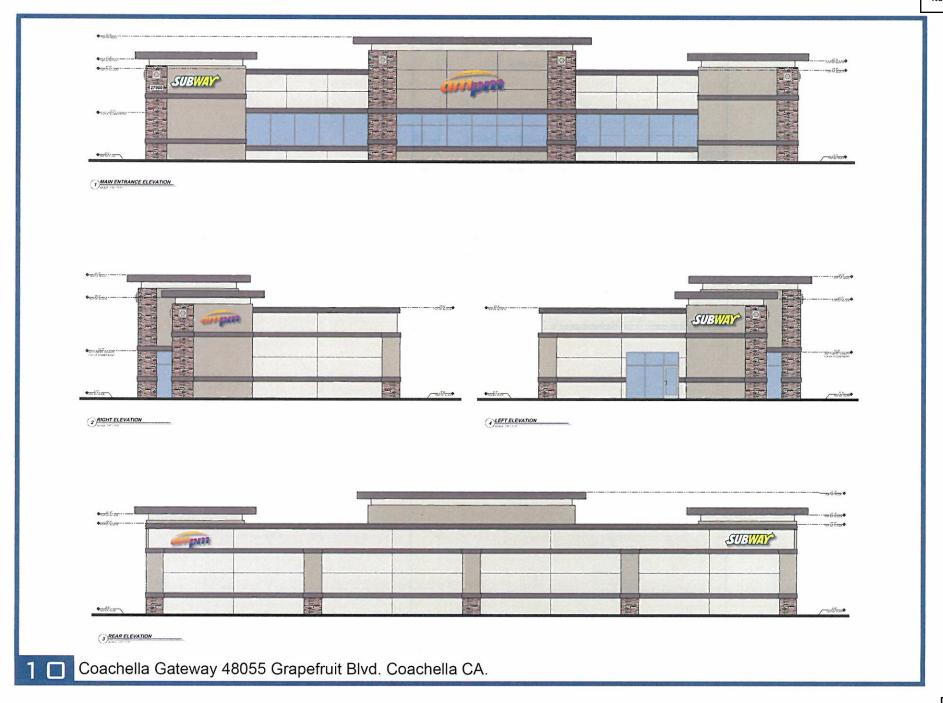
SECONDARY SIGNS: YES (NOT TO EXCEED 25% OF TOTAL ALLOWABLE SIGN AREA). TOTAL COMBINED SIGNAGE NOT TO EXCEED MAXIMUM SQUARE FOOTAGE PER FRONTAGE.

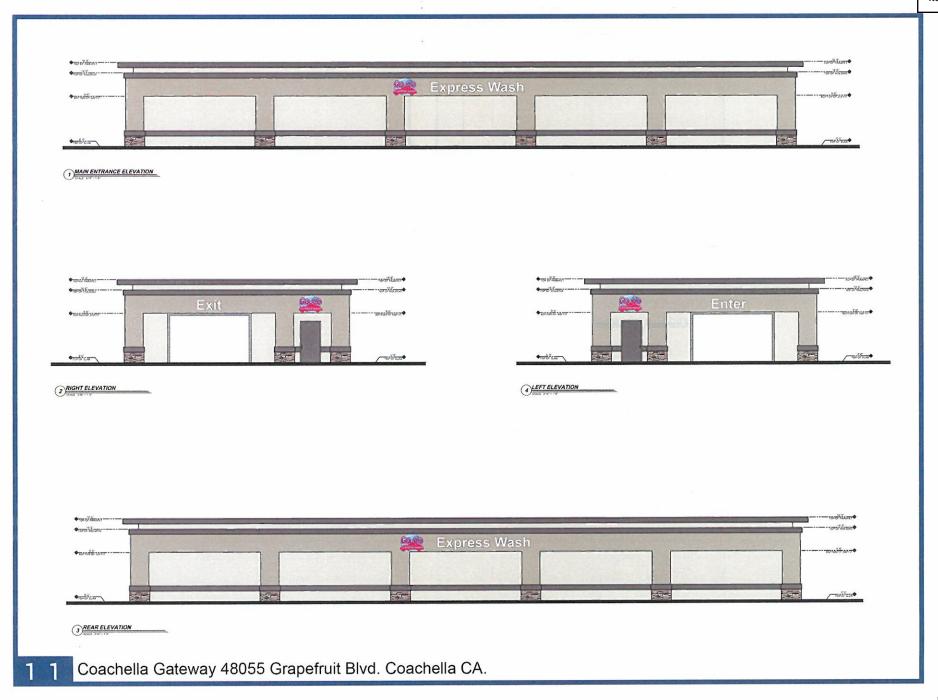
SIZE:1.5 SQUARE FOOT OF SIGN AREA PER LINEAR
FOOT OF LEASED PREMISES, MAXIMUM OF
250 SQUARE FEET TOTAL PER ELEVATION.
MATERIALS: VARIETY OF TYPES PER SIGN CRITERIA,
TWO TYPES OF CONSTRUCTION REQUIRED UNLESS
OTHERWISE APPROVED BY LANDLORD
ILLUMINATION: YES
COPY: TENANT NAME AND / OR LOGO
HEIGHT: SIXTY PERCENT OF ADJACENT SURFACE
LENGTH: SEVENTY PERCENT OF ADJACENT SURFACE
TYPEFACE: CUSTOM LOGO AND TYPE OK
COLORS: CUSTOM COLORS OK
SECONDARY SIGNS: YES INOT TO EXCEED 25% OF TOTAL
ALLOWABLE SIGN AREA]. TOTAL COMBINED SIGNAGE NOT
TO EXCEED MAXIMUM SQUARE FOOTAGE PER FRONTAGE.

SIZE:1.5 SQUARE FOOT OF SIGN AREA PER UNEAR FOOT OF LEASED PREMISES, MAXIMUM OF 150 SQUARE FEET TOTAL PER ELEVATION.
MATERIALS: VARIETY OF TYPES PER SIGN CRITERIA, TWO TYPES OF CONSTRUCTION REQUIRED UNLESS OTHERWISE APPROVED BY LANDLORD ILLUMINATION: YES COPY: TENANT NAME AND / OR LOGO HEIGHT: SIXTY PERCENT OF ADJACENT SURFACE LENGTH: SEVENTY PERCENT OF ADJACENT SURFACE TYPEFACE: CUSTOM LOGO AND TYPE OK COLORS: CUSTOM COLORS OK

Coachella Gateway 48055 Grapefruit Blvd. Coachella CA.

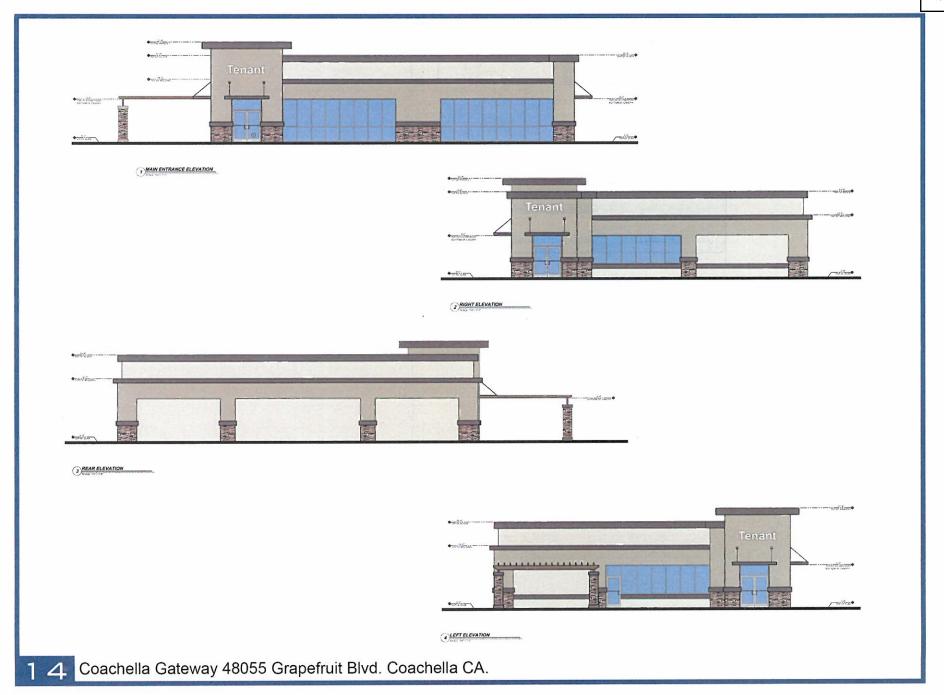


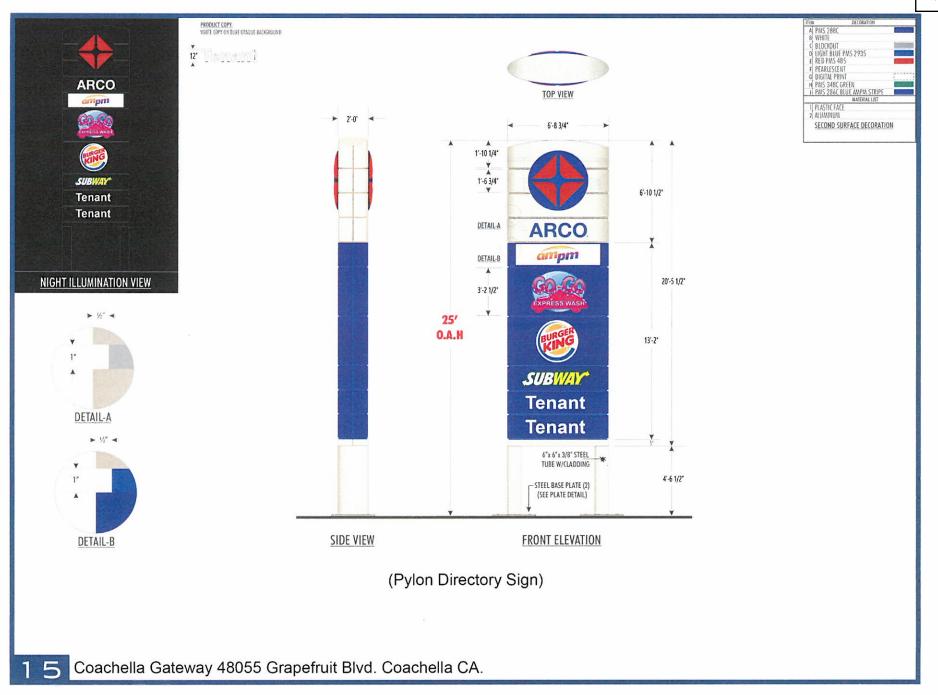




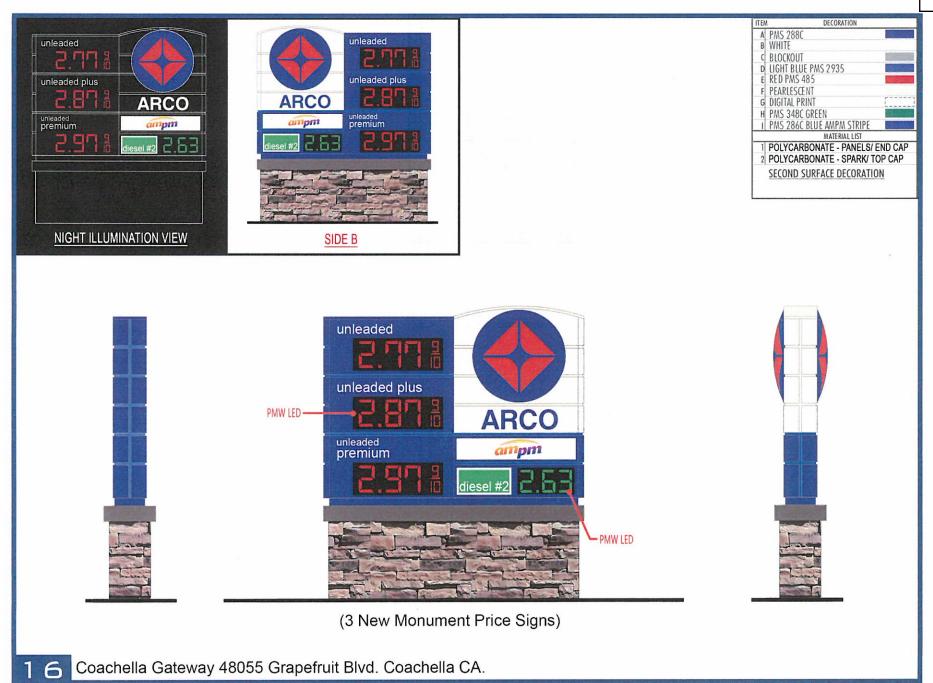








Item 3.





Vicinity Map

 From:
 Tyler Hull

 To:
 Adrian Moreno

 Cc:
 Tommy Fowlkes

Subject: FW: RAC - CUP 377 AR 24-02 Coachella Gateway

Date: February 14 24 2:22:03 PM

Attachments: <u>image002.png</u>

image003.png image005.png RAC Transmittal.docx Site Plan and Elevations.pdf

Hi Adrian-

CVWD has no comments for the proposed additional 4,627 shell building with drive-thru. Per the attached request for comments.

Thank you,

Tyler Hull Utility Coordinator



Coachella Valley Water District 760-398-2661 ext. 2571

www.cvwd.org

FFrom: Adrian Moreno <amoreno@coachella.org>

Sent: Wednesday, February 14, 2024 9:26 AM

To: Gabriel Perez <gperez@coachella.org>; Adrian Moreno <amoreno@coachella.org>; Eva Lara

<elara@coachella.org>; Maritza Martinez <mmartinez@coachella.org>; Castulo Estrada

<cestrada@coachella.org>; Abraham Vega <avega@coachella.org>; Gabriel Martin

<gmartin@coachella.org>; Lizzandro Diaz <ldiaz@coachella.org>; Andrew Simmons

<asimmons@coachella.org>; Efrain Rodriguez <erodriguez@coachella.org>; Celina Jimenez

<ciimenez@coachella.org>; ron@rgplanningconsultants.com; MAbbott@RIVCO.ORG;

DVargas@IID.com; GBarraza@IID.com; ilgerardo@iid.com; sbliss@29palmsbomi-nsn.gov;

abecerra@tmdci.org; ltorres@cvusd.us; patrick.cisneros@desertsands.us;

rvasquez@riversidesheriff.org; IC-EnvironmentalServ < IC-EnvironmentalServ@cvwd.org >; IC-Environme

Engineering <IC-Engineering@cvwd.org>; rruofmplanningeast@fire.ca.gov; kohl.hetrick@fire.ca.gov;

KTsang@rivco.org; MPablo@Rivco.org; rosa.f.clark@dot.ca.gov; cavalos@burrtecdesert.com;

iguidry@sunline.org; malcala@sunline.org

Subject: FW: RAC - CUP 377 AR 24-02 Coachella Gateway

External e-mail: Do not click on links or open attachments unless you recognize the sender and you know the content is safe. o365

Hello Everyone,

I am forwarding the development plan set at a reduced file size to ensure everyone was able to receive the email. Please see email below.

Please return comments by Wednesday, February 28, 2024.

Feel free to reach out with any questions.

Thanks,

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

53990 Enterprise Way · Coachella, CA 92236

Phone: 760-398-3502 Ext: 118 Email: amoreno@coachella.org



Office Hours: Monday - Thursday 7:00 AM to 6:00 PM

Closed Fridays

Website | Map 🚺 💟



From: Adrian Moreno

Sent: February 14 24 9:07 AM

To: Gabriel Perez <gperez@coachella.org>; Adrian Moreno <amoreno@coachella.org>; Eva Lara

<elara@coachella.org>; Maritza Martinez mmartinez@coachella.org; Castulo Estrada

<cestrada@coachella.org>; Abraham Vega <avega@coachella.org>; Gabriel Martin

<gmartin@coachella.org>; Lizzandro Diaz <ldiaz@coachella.org>; 'andrew Simmons

(asimmons@coachella.org)' <asimmons@coachella.org>; Efrain Rodriguez

<erodriguez@coachella.org>; Celina Jimenez <ciimenez@coachella.org>;

'ron@rgplanningconsultants.com' < ron@rgplanningconsultants.com; 'MAbbott@RIVCO.ORG'

<<u>MAbbott@RIVCO.ORG</u>>; 'DVargas@IID.com' <<u>DVargas@IID.com</u>>; 'GBarraza@IID.com'

<<u>GBarraza@IID.com</u>>; 'jlgerardo@iid.com' <<u>jlgerardo@iid.com</u>>; 'sbliss@29palmsbomi-nsn.gov'

<<u>sbliss@29palmsbomi-nsn.gov</u>>; 'abecerra@tmdci.org' <<u>abecerra@tmdci.org</u>>; 'ltorres@cvusd.us'

<\torres@cvusd.us>; 'patrick.cisneros@desertsands.us' patrick.cisneros@desertsands.us; 'patrick.cisneros@desertsands.us'

'rvasquez@riversidesheriff.org' <<u>rvasquez@riversidesheriff.org</u>>; 'IC-EnvironmentalServ@cvwd.org'

<<u>IC-EnvironmentalServ@cvwd.org</u>>; 'IC-Engineering@cvwd.org' <<u>IC-Engineering@cvwd.org</u>>;

'rruofmplanningeast@fire.ca.gov' < rruofmplanningeast@fire.ca.gov; 'kohl.hetrick@fire.ca.gov'

< kohl.hetrick@fire.ca.gov>; 'cavalos@burrtecdesert.com' < cavalos@burrtecdesert.com>;

'iguidry@sunline.org' <iguidry@sunline.org>; 'malcala@sunline.org' <malcala@sunline.org>;

'KTsang@rivco.org' <<u>KTsang@rivco.org</u>>; 'MPablo@Rivco.org' <<u>MPablo@Rivco.org</u>>;

'rosa.f.clark@dot.ca.gov' < rosa.f.clark@dot.ca.gov >

Subject: RAC - CUP 377 AR 24-02 Coachella Gateway

Hello Everyone,

For your review, please find the attached Request for Agency Comments for **CUP 377 AR 24-02 Coachella Gateway** project.

The submittal for a Conditional Use Permit and Architectural Review for the proposed construction of a 4,627 square foot shell building with a drive-thru at the vacant parcel on Grapefruit Boulevard, south of Avenue 48, and north of the Dillon bridge at APN 603-220-057. A total of 27 new parking spaces are proposed, 18 spaces to the north and west of the building and 9 spaces on a separate landscape island. The proposed drive-thru provides vehicle stacking for approximately 22 vehicles. The billboard on this lot will be removed as part of this project. Attached are the site plan, landscaping, and elevations for the project.

You may also access the files via Bluebeam Session ID: 640-743-919
Session URL: https://studio.bluebeam.com/hyperlink.html?link=studio.bluebeam.com/sessions/640-743-919

Please return comments by Wednesday, February 28, 2024.

Please reach out with any questions.

Thanks,

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

53990 Enterprise Way · Coachella, CA 92236

Phone: 760-398-3502 Ext: 118 Email: amoreno@coachella.org



Office Hours: Monday - Thursday 7:00 AM to 6:00 PM

Closed Fridays

Website | Map 🚹 💟







February 28, 2024

Mr. Adrian Moreno Associate Planner Development Services Department City of Coachella 1515 6th Street Coachella, CA 92236

SUBJECT: Coachella Gateway Project; CUP 377 AR 24-02

Dear Mr. Moreno:

On February 14, 2024, the Imperial Irrigation District received from the City of Coachella Development Services Department, a request for agency comments on the Coachella Gateway project; Conditional Use Permit No. 377, Architectural Review No. 24-02. The applicant, Chandi & Karan, LLC; proposes the construction of a 4,627 sq. ft. shell building with a drive-thru at the vacant parcel on 48151Grapefruit Boulevard, south of Avenue 48, and north of the Dillon bridge in Coachella, California (APN 603-220-057). A total of 27 new parking spaces are proposed, 18 spaces to the north and west of the building and 9 spaces on a separate landscape island. The proposed drive-thru will provide vehicle stacking for approximately 22 vehicles.

IID has reviewed the project information has the following comments:

- 1. Based on the preliminary information provided to the IID, the district can accommodate the power load requirements for the project by extending/upgrading distribution backbone lines (conduit and cable) from N76 circuit with a loop configuration.
- 2. It is important to note that a final study will be developed once a customer project application and approved plans and loading calculations are received. This detailed information will allow IID to perform an accurate assessment and provide a full report of any potential impacts and mitigation measures as well as costs. The conditions of service could change as a result of the additional studies.
- 3. IID will not begin any studies to provide electrical service to a project until the applicant submits a customer project application (available for download at the district website http://www.iid.com/home/showdocument?id=12923 and detailed loading information, panel sizes, project schedule and estimated in-service date. Applicant shall bear all costs associated with providing electrical service to the project, including but not limited to the construction of distribution line extensions,

Adrian Moreno February 28, 2024 Page 2

underground conduit systems and the re-configuration of distribution lines, which based on current 2024 IID rates are estimated to be \$300.000 (subject to change without notice), as well as the cost of any other related upgrades and applicable permits, zoning changes, landscaping (if required by the City) and rights-of-way and easements.

- 4. The district's ability to provide service from existing infrastructure is based on current available capacity, which may be impacted by future development in the area. IID is unable to hold system capacity to the detriment of other customers.
- 5. Underground infrastructure that includes trenching, conduits, pull boxes, switch boxes and pads should be installed following IID approved plans. Physical field installation of underground infrastructures should be verified and approved by an IID inspector prior to cable installation as per IID Developer's Guide (available at the district website https://www.iid.com/home/showdocument?id=14229).
- 6. Line and feeder extensions to serve the project will be made in accordance with IID Regulations:
 - No. 2 (http://www.iid.com/home/showdocument?id=2540),
 - No. 13 (http://www.iid.com/home/showdocument?id=2553),
 - No. 15 (http://www.iid.com/home/showdocument?id=2555)
- 7. For additional information regarding electrical service for the project, the applicant should be advised to contact the IID Energy La Quinta Division Customer Operations, 81-600 Avenue 58 La Quinta, CA 92253, at (760) 398-5841 and speak with the project development planner assigned to the area.
- 8. It is important to note that IID's policy is to extend its electrical facilities only to those project that have obtained the approval of a city or county planning commission and such other governmental authority or decision-making body having jurisdiction over said developments.
- 9. The applicant will be required to provide rights of ways and easements for any proposed power line extensions and/or any other infrastructure needed to serve the project as well as the necessary access to allow for continued operation and maintenance of any IID facilities located on adjoining properties.
- 10. Public utility easements over all private public roads and additional ten (10) feet in width on both side of the private and public roads shall be dedicated to IID for the construction, operation, and maintenance of its electrical infrastructure.
- 11. Any construction or operation on IID property or within its existing and proposed right of way or easements including but not limited to: surface improvements such

Adrian Moreno February 28, 2024 Page 3

as proposed new streets, driveways, parking lots, landscape; and all water, sewer, storm water, or any other above ground or underground utilities; will require an encroachment permit, or encroachment agreement (depending on the circumstances). A copy of the IID encroachment permit application and instructions for its completion are available at https://www.iid.com/about-iid/department-directory/real-estate. The IID Real Estate Section should be contacted at (760) 339-9239 for additional information regarding encroachment permits or agreements.

- 12. Any new, relocated, modified or reconstructed IID facilities required for and by the project (which can include but is not limited to electrical utility substations, electrical transmission and/or distribution lines, ancillary facilities associated with the conveyance of energy service; the acquisition and dedication of real property, rights of way and/or easements for the siting and construction of electrical utility substations, electrical transmission and/or distribution lines and ancillary facilities associated with the conveyance of energy service, etc.) need to be included as part of the project's California Environmental Quality Act (CEQA) and/or National Environmental Policy Act (NEPA documentation, environmental impact analysis and mitigation. Failure to do so will result in postponement of any construction and/or modification of IID facilities until such time as the environmental documentation is amended and environmental impacts are fully mitigated. Any mitigation necessary as a result of the construction, relocation and/or upgrade of IID facilities is the responsibility of the project proponent.
- Dividing a project into two or more pieces and evaluating each piece in a separate environmental document (Piecemealing or Segmenting), rather than evaluating the whole of the project in one environmental document, is explicitly forbidden by CEQA, because dividing a project into a number of pieces would allow a Lead Agency to minimize the apparent environmental impacts of a project by evaluating individual pieces separately, each of which may have a less-than-significant impact on the environment, but which together may result in a significant impact. Segmenting a project may also hinder developing comprehensive mitigation strategies. In general, if an activity or facility is necessary for the operation of a project, or necessary to achieve the project objectives, or a reasonably foreseeable consequence of approving the project, then it should be considered an integral project component that should be analyzed within the environmental analysis. The project description must include all project component, including those that will have to be approved by responsible agencies. The State CEQA Guidelines define a project under CEQA as "the whole of the action" that may result either directly or indirectly in physical changes to the environment. This broad definition is intended to provide the maximum protection of the environment. CEQA case law has established general principles on project segmentation for different project types. For a project requiring construction of offsite infrastructure, the offsite infrastructure

Adrian Moreno February 28, 2024 Page 4

must be included in the project description. San Joaquin Raptor/Wildlife Rescue Center v. County of Stanislaus (1994) 27 Cal.App. 4th 713.

14. Applicant should be advised that landscaping can be dangerous if items are planted too close to IID's electrical equipment. In the event of an outage, or equipment failure, it is vital that IID personnel have immediate and safe access to its equipment to make the needed repairs. For public safety, and that of the electrical workers, it is important to adhere to standards that limit landscaping around electrical facilities. IID landscaping guidelines are available at https://www.iid.com/energy/vegetation-management.

Should you have any questions, please do not hesitate to contact me at (760) 482-3609 or at dvargas@iid.com. Thank you for the opportunity to comment on this matter.

Respectfully,

Donald Vargas

Compliance Administrator II

Item 3.



City of Coachella Enclosure Recommendation

February 27, 2024

Project Name: Coachella Gateway

Dear Chandi & Karan, LLC.,

Thank you for contacting Burrtec to review your plans for waste and recycling enclosure specifications. Per the site plans and construction information you provided for the proposed building at 48151 Grapefruit Blvd. in Coachella, Burrtec would be able to service the waste enclosure as seen on the plans.

Please see below for additional details pertaining to our recommendation:

Requires the following services:

⊠ Sol	lid waste service: Minimum level of service 3 yard/1 time per week
⊠ Re	cycling service: Minimum level of service 3 yard/1 time per week meets AB341 Law
•	ganics recycling service: Minimum level of service (1) 64-gallon commercial service/ er week meets AB1826/SB1383 Law
□ De	Minimis Waiver for Organics: Reviewed and organic matter not referenced Business type: Explanation: Approximate organic waste produced: Below 20-gallon/1 time per
	week threshold (subject to a follow-up post project completion)

Enclosure Details: Commercial standard 3 yard bins are 81" wide (3.75') and 42" deep (3.5') with 6" curbs along the sides; rear facing commercial bins with the bins backing up to the sidewalls with 4' of clearance between them. Two 3 yard commercial bins can easily fit into an enclosure that is 12' wide and 9' deep.

That said, enclosure location appears to meet Burrtec requirements for service vehicle accessibility, safety and clearance. Burrtec is proud to service the City of Coachella. We look forward to providing services to your future project. Please contact me if you have any questions, or if I can be of further assistance at cavalos@burrtecdesert.com.

Sincerely, Cynthia Avalos District Environmental Coordinator From: <u>Cynthia Avalos</u>
To: <u>Adrian Moreno</u>

Subject: Re: RAC - CUP 377 AR 24-02 Coachella Gateway

Date: February 27 24 3:48:07 PM

Attachments: image002.png

image003.png Outlook-tijj2v5i.png

Letter of Enclosure Service - Coachella Gateway.pdf

Hello,

On behalf of Burrtec Waste & Recycling Services there are no comments regarding changes to the site plans provided.

I have attached a letter of enclosure service for your records.

Please have the builder contact me for help with the diversion plan and with help setting up services.

Thank you,

Cynthia Avalos, BA
District Environmental Coordinator
Burrtec Waste and Recycling Services
Direct Number (760) 674-1034
Cell Number (760) 851-8930



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From: Adrian Moreno <amoreno@coachella.org>

Sent: Tuesday, February 27, 2024 3:32 PM

To: Gabriel Perez <gperez@coachella.org>; Eva Lara <elara@coachella.org>; Maritza Martinez <mmartinez@coachella.org>; Castulo Estrada <cestrada@coachella.org>; Abraham Vega <avega@coachella.org>; Gabriel Martin <gmartin@coachella.org>; Lizzandro Diaz <ldiaz@coachella.org>; Andrew Simmons <asimmons@coachella.org>; Efrain Rodriguez <erodriguez@coachella.org>; Celina Jimenez <cjimenez@coachella.org>; ron@rgplanningconsultants.com <ron@rgplanningconsultants.com>; MAbbott@RIVCO.ORG <MAbbott@RIVCO.ORG>; DVargas@IID.com <DVargas@IID.com>; GBarraza@IID.com <GBarraza@IID.com>; jlgerardo@iid.com <jlgerardo@iid.com>; abecerra@tmdci.org <abecerra@tmdci.org>; ltorres@cvusd.us <ltorres@cvusd.us>; patrick.cisneros@desertsands.us

<patrick.cisneros@desertsands.us>; rvasquez@riversidesheriff.org <rvasquez@riversidesheriff.org>;
IC-EnvironmentalServ@cvwd.org <IC-EnvironmentalServ@cvwd.org>; IC-Engineering@cvwd.org <IC-Engineering@cvwd.org>; rruofmplanningeast@fire.ca.gov <rruofmplanningeast@fire.ca.gov>;
kohl.hetrick@fire.ca.gov <kohl.hetrick@fire.ca.gov>; KTsang@rivco.org <KTsang@rivco.org>;
MPablo@Rivco.org <MPablo@Rivco.org>; rosa.f.clark@dot.ca.gov <rosa.f.clark@dot.ca.gov>;
Cynthia Avalos <cavalos@burrtecdesert.com>; jguidry@sunline.org <jguidry@sunline.org>;
malcala@sunline.org <malcala@sunline.org>; Christopher Nicosia
<Christopher.Nicosia@29palmsbomi-nsn.gov>; Eric Jordan <Eric.Jordan@29palmsbomi-nsn.gov>

Subject: RE: RAC - CUP 377 AR 24-02 Coachella Gateway

You don't often get email from amoreno@coachella.org. Learn why this is important

CAUTION: This is an external email from outside the organization. Please take care when clicking links or opening attachments. When in doubt, contact the IT Department.

IMPORTANT: This email contains a **POTENTIALLY DANGEROUS ATTACHMENT.** Do not click any link and do not open attachments unless you have confirmed the sender.

Hello Everyone,

I just wanted to provide a friendly reminder that if your department has any comments for the CUP 377 AR 24-02 Coachella Gateway, **please provide by the end of the day <u>tomorrow</u> Wednesday, February 28, 2024**.

Feel free to reach out with any questions.

Thanks,

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

53990 Enterprise Way · Coachella, CA 92236

Phone: 760-398-3502 Ext: 118 Email: amoreno@coachella.org



Office Hours: Monday - Thursday 7:00 AM to 6:00 PM

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From: Adrian Moreno

Sent: February 14 24 9:26 AM

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jguidry@sunline.org; malcala@sunline.org

Subject: FW: RAC - CUP 377 AR 24-02 Coachella Gateway

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Subject: RAC - CUP 377 AR 24-02 Coachella Gateway

Hello Everyone,

For your review, please find the attached Request for Agency Comments for **CUP 377 AR 24-02 Coachella Gateway** project.

The submittal for a Conditional Use Permit and Architectural Review for the proposed construction of a 4,627 square foot shell building with a drive-thru at the vacant parcel on Grapefruit Boulevard, south of Avenue 48, and north of the Dillon bridge at APN 603-220-057. A total of 27 new parking spaces are proposed, 18 spaces to the north and west of the building and 9 spaces on a separate landscape island. The proposed drive-thru provides vehicle stacking for approximately 22 vehicles. The billboard on this lot will be removed as part of this project. Attached are the site plan, landscaping, and elevations for the project.

You may also access the files via Bluebeam Session ID: 640-743-919
Session URL: https://studio.bluebeam.com/hyperlink.html?link=studio.bluebeam.com/sessions/640-743-919

Please return comments by **Wednesday**, **February 28**, **2024**.

Please reach out with any questions.

Thanks,

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

53990 Enterprise Way · Coachella, CA 92236

Phone: 760-398-3502 Ext: 118 Email: amoreno@coachella.org



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CAL FIRE - RIVERSIDE UNIT RIVERSIDE COUNTY FIRE DEPARTMENT

Item 3.

BILL WEISER - FIRE CHIEF

Office of the Fire Marshal (East) 77-933 Las Montanas Rd., Ste 201, Palm Desert, CA 92211 Bus: (760) 863-8886 ~ Fax: ~ rivcoplus.org

PROUDLY SERVING THE UNINCORPORATED AREA OF RIVERSIDE COUNTY AND THE CITIES OF:

Chandi & Karan LLC

March 01, 2024

Grapefruit Boulevard, south of Avenue 48, and north of the Dillon bridge

BANNING

BEAUMONT

COACHELLA

DESERT HOT SPRINGS

EASTVALE

INDIAN WELLS

INDIO

JURUPA VALLEY

LAKE ELSINORE

LA QUINTA

MENIFEE

MORENO VALLEY

NORCO

PALM DESERT

PERRIS

RANCHO MIRAGE

RUBIDOUX CSD

SAN JACINTO

TEMECULA

WILDOMAR

Fire Conditional Use Permit (F

COACHELLA, CA

RAC - CUP 377 AR 24-02 Coachella Permit Number: FPCUP2400008 Project Name:

> Gateway: Bluebeam Session ID: 640-743-919: The submittal for a Conditional Use Permit and Architectural Review for the proposed construction of a 4,627 square foot shell building with a drive-thru at the vacant parcel on Grapefruit Boulevard, south of Avenue 48, and north of the Dillon bridge at APN 603-220-057. A total of 27 new parking spaces are proposed, 18 spaces to the north and west of the building and 9 spaces on a separate landscape island. The proposed drive-thru provides vehicle stacking for approximately 22 vehicles. The billboard on this lot will be removed

as part of this project.

Project Address: Grapefruit Boulevard, south of

Avenue 48, and north of the Dillon

bridge

COACHELLA, CA

APN(s): 603220057 Reviewer: Kohl Hetrick

Review Number: 2

Case Type:

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KAREN SPIEGEL **DISTRICT 2**

CHARLES WASHINGTON DISTRICT 3

V. MANUEL PEREZ **DISTRICT 4**

DR. YXSTIAN GUTIERREZ **DISTRICT 5**

Riverside County Fire Department (RVCFD) Office of the Fire Marshal (OFM) has reviewed the submitted plans for the referenced project and they are approved with the following conditions.

015 - Fire

General Fire Department Advisory Comments - Commercial

With respect to the planning conditions for the referenced project, the fire department requires the following fire protection measures be provided in accordance with Riverside County Ordinances, the current edition of California Fire Code (CFC) as adopted and amended by the County of Riverside and/or recognized fire protection standards.

These conditions are preliminary and further review will be conducted upon receipt of additional entitlement and/or construction submittals. Additional requirements may be required based upon the adopted codes at the time of submittal.

1. Fire Protection Water Supplies/Fire Flow - Minimum fire flow for the construction of all buildings is required per CFC Appendix B. Prior to building permit issuance for new construction, the applicant shall provide documentation to show a (existing/proposed) water system capable of delivering the required fire flow. Specific design features may increase

109



Item 3.



PROUDLY SERVING THE UNINCORPORATED AREA OF RIVERSIDE COUNTY AND THE CITIES OF:

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Office of the Fire Marshal (East)

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decrease the required fire flow.

- a. Will Serve Letters from the responsible water purveyor are required prior to a map recordation.
- 2. Fire Protection Water Supplies/Hydrants The minimum number of fire hydrants required, as well as the location and spacing of fire hydrants, shall comply with CFC Appendix C and NFPA 24. Fire hydrants shall be located no more than 400 feet from all portions of the exterior of the building along an approved route on a fire apparatus access road, unless otherwise approved by the Fire Department. Where new water mains are extended along streets where hydrants are not needed for protection of structures, standal fire hydrants shall be provided at spacing not to exceed 1000 feet along streets for transportation hazards. Fire hydrants shall be at least 40 feet from the building it is serving A fire hydrant shall be located within 20 to 100 feet of the fire department connection for buildings protected with a fire sprinkler system. The size and number of outlets required for the approved fire hydrants are 4" x 2 ½" x 2 ½" (super hydrant). Reference CFC as amend and NFPA 24.
- 3. Fire Department Access Fire apparatus access roads shall be provided to within 150 feet of all exterior portions of buildings, unless otherwise approved by the Fire Department Fire apparatus access roads shall have an unobstructed width of not less than 24 feet. Dead-end fire apparatus access roads in excess of 150 feet shall be provided with an approved turn around. The minimum required turning radius of a fire apparatus access roa is 38 feet outside radius and 14 feet inside radius. The construction of the fire apparatus access roads shall be all weather and capable of sustaining 75,000 lbs. Unless otherwise approved, the grade of a fire apparatus access road shall not exceed 16 percent and the cross slope shall not exceed 2.5 percent. The angles of approach and departure for fire apparatus access roads shall be a maximum of 6 percent grade change for 25 feet of approach/departure. Reference CFC as amended and Riverside County Fire Department Policies and Standards.
- a. Fire Lane marking: Identification and marking of fire lanes, including curb details and signage shall be in compliance with Riverside County Fire Department Standards.
- 4. Fire Department Building Construction Plan Review Submittal of construction plans to the Fire Department will be required. Final fire and life safety conditions will be addressed when the Fire Department reviews the plans. These conditions will be based on California Fire Code, California Building Code (CBC), and related codes/standards adopted at the tin of construction plan submittal. Reference CFC as amended.
- 5. Fire Sprinkler System All new commercial buildings and structures 3,600 square feet or larger will be required to install a fire sprinkler system. Reference CFC as amended.
- 6. Fire Alarm and Detection System A water flow monitoring system and/or fire alarm system may be required as determined at time of building construction plan review. Reference CFC as amended.
- 7. Traffic Calming Devices Requests for installation of traffic calming designs/devices or fire apparatus access roads shall be submitted for evaluation purposes, resulting in denial approval by the Fire Code Official. Reference CFC as amended.
- 8. Gate Access All electronically operated gates shall be provided with Knox key switches and automatic sensors for access. These gates shall be provided with access to gate equipment or another method to open the gate if there is a power failure. (Manual gat shall not be locked unless a Knox padlock or Knox Box containing the key to the lock is



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installed in an approved location on the approach side of the gate). A pedestrian gate, if used to provide access, shall be a minimum 3 feet wide and provided with a Knox Box/Padlock if locked. Reference CFC as amended.

9. Water Plans - If fire hydrants are required to be installed, applicant/developer shall furnish the water system fire hydrant plans to the Fire Department for review and approval prior to building permit issuance. Plans shall be signed by a registered civil engineer, and shall confirm hydrant type, location, spacing, and minimum fire flow. Once plans are signed and approved by the local water authority, the originals shall be presented to the Fire Department for review and approval. Reference CFC as amended.

We appreciate the oppprtunity to work together to ensure fire, rescue, medical and all hazard emergency services are provided to our County and all of the residents.

Should you have additional questions, please contact me via phone at or email at steven.gonzalez@fire.ca.gov.

Steven Gonzalez Fire Safety Specialist

BOARD OF SUPERVISORS

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KAREN SPIEGEL DISTRICT 2

CHARLES WASHINGTON DISTRICT 3

V. MANUEL PEREZ DISTRICT 4

DR. YXSTIAN GUTIERREZ DISTRICT 5

From: <u>Tsang, Kevin</u>
To: <u>Adrian Moreno</u>

Subject: RE: RAC - CUP 377 AR 24-02 Coachella Gateway

Date: February 27 24 4:46:39 PM

Attachments: image002.png

image003.png

Hello Adrian,

Thank you for the opportunity to review the development project. I do not have any comments to provide.

Thanks

From: Adrian Moreno <amoreno@coachella.org>

Sent: Tuesday, February 27, 2024 3:33 PM

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Subject: RE: RAC - CUP 377 AR 24-02 Coachella Gateway

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< <u>kohl.hetrick@fire.ca.gov</u>>; 'cavalos@burrtecdesert.com' < <u>cavalos@burrtecdesert.com</u>>;

'jguidry@sunline.org' <<u>jguidry@sunline.org</u>>; 'malcala@sunline.org' <<u>malcala@sunline.org</u>>;

'KTsang@rivco.org' < KTsang@rivco.org; 'MPablo@Rivco.org' < MPablo@Rivco.org; 'MPablo@Rivco.org' >;

'rosa.f.clark@dot.ca.gov' <<u>rosa.f.clark@dot.ca.gov</u>>

Subject: RAC - CUP 377 AR 24-02 Coachella Gateway

Hello Everyone,

For your review, please find the attached Request for Agency Comments for **CUP 377 AR 24-02 Coachella Gateway** project.

The submittal for a Conditional Use Permit and Architectural Review for the proposed construction of a 4,627 square foot shell building with a drive-thru at the vacant parcel on Grapefruit Boulevard, south of Avenue 48, and north of the Dillon bridge at APN 603-220-057. A total of 27 new parking spaces are proposed, 18 spaces to the north and west of the building and 9 spaces on a separate landscape island. The proposed drive-thru provides vehicle stacking for approximately 22 vehicles. The billboard on this lot will be removed as part of this project. Attached are the site plan, landscaping, and elevations for the project.

You may also access the files via Bluebeam Session ID: 640-743-919
Session URL: https://studio.bluebeam.com/hyperlink.html?link=studio.bluebeam.com/sessions/640-743-919

Please return comments by Wednesday, February 28, 2024.

Please reach out with any questions. Thanks.

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

53990 Enterprise Way · Coachella, CA 92236

Phone: 760-398-3502 Ext: 118 Email: amoreno@coachella.org



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County of Riverside California

From: Vasquez, Randy <rvasquez@riversidesheriff.org>

Sent: February 28 24 8:04 AM

To: Adrian Moreno

Subject: RE: RAC - CUP 377 AR 24-02 Coachella Gateway

No comments

Lieutenant Randy Vasquez #1541 Thermal Station 86625 Airport Boulevard Thermal, CA 92274

E-mail: rvasquez@riversidesheriff.org

Office: 760-863-8990 Desk: 760-863-7962



From: Adrian Moreno amoreno@coachella.org

Sent: Tuesday, February 27, 2024 3:33 PM

To: Gabriel Perez <<u>gperez@coachella.org</u>>; Eva Lara <<u>elara@coachella.org</u>>; Maritza Martinez <<u>mmartinez@coachella.org</u>>; Castulo Estrada <<u>cestrada@coachella.org</u>>; Abraham Vega

<a href="mailto:<a hr

< <u>Idiaz@coachella.org</u>>; Andrew Simmons < <u>asimmons@coachella.org</u>>; Efrain Rodriguez

<erodriguez@coachella.org>; Celina Jimenez <cjimenez@coachella.org>;
ron@rgplanningconsultants.com; MAbbott@RIVCO.ORG; DVargas@IID.com; GBarraza@IID.com;
jlgerardo@iid.com; abecerra@tmdci.org; ltorres@cvusd.us; patrick.cisneros@desertsands.us; Vasquez,
Randy <rvasquez@riversidesheriff.org>; IC-EnvironmentalServ@cvwd.org; IC-Engineering@cvwd.org;
rruofmplanningeast@fire.ca.gov; kohl.hetrick@fire.ca.gov; Tsang, Kevin <KTSANG@RIVCO.ORG>; Pablo,
Marisela <MPABLO@RIVCO.ORG>; rosa.f.clark@dot.ca.gov; cavalos@burrtecdesert.com;
jguidry@sunline.org; malcala@sunline.org; Christopher Nicosia <Christopher.Nicosia@29palmsbomi-</p>

nsn.gov>; Eric Jordan < Eric.Jordan@29palmsbomi-nsn.gov>
Subject: RE: RAC - CUP 377 AR 24-02 Coachella Gateway

CAUTION: This email originated from outside the **Riverside Sheriff** email system.

DO NOT click links or open attachments unless you recognize the sender and know the content is safe.

Hello Everyone,

I just wanted to provide a friendly reminder that if your department has any comments for the CUP 377 AR 24-02 Coachella Gateway, please provide by the end of the day <u>tomorrow</u> Wednesday, February 28, 2024.

Feel free to reach out with any questions.

Thanks,

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

53990 Enterprise Way · Coachella, CA 92236

Phone: 760-398-3502 Ext: 118 Email: amoreno@coachella.org



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From: Adrian Moreno

Sent: February 14 24 9:26 AM

To: Gabriel Perez <gperez@coachella.org>; Adrian Moreno <amoreno@coachella.org>; Eva Lara

<<u>elara@coachella.org</u>>; Maritza Martinez <<u>mmartinez@coachella.org</u>>; Castulo Estrada <<u>cestrada@coachella.org</u>>; Abraham Vega <<u>avega@coachella.org</u>>; Gabriel Martin

<gmartin@coachella.org>; Lizzandro Diaz <<u>Idiaz@coachella.org</u>>; andrew Simmons

(<u>asimmons@coachella.org</u>) < <u>asimmons@coachella.org</u>>; Efrain Rodriguez < <u>erodriguez@coachella.org</u>>;

Celina Jimenez < cjimenez@coachella.org >; ron@rgplanningconsultants.com; MAbbott@RIVCO.ORG;

 $\underline{DVargas@IID.com}; \underline{GBarraza@IID.com}; \underline{ilgerardo@iid.com}; \underline{sbliss@29palmsbomi-nsn.gov};$

abecerra@tmdci.org; ltorres@cvusd.us; patrick.cisneros@desertsands.us;

rvasquez@riversidesheriff.org; IC-EnvironmentalServ@cvwd.org; IC-Engineering@cvwd.org; rruofmplanningeast@fire.ca.gov; kohl.hetrick@fire.ca.gov; KTsang@rivco.org; MPablo@Rivco.org;

rosa.f.clark@dot.ca.gov; cavalos@burrtecdesert.com; jguidry@sunline.org; malcala@sunline.org

Subject: FW: RAC - CUP 377 AR 24-02 Coachella Gateway

Hello Everyone,

I am forwarding the development plan set at a reduced file size to ensure everyone was able to receive the email. Please see email below.

Please return comments by Wednesday, February 28, 2024.

Feel free to reach out with any questions.

Thanks.

Adrian Moreno | Associate Planner

City of Coachella · Development Services Department

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From: Adrian Moreno

Sent: February 14 24 9:07 AM

To: Gabriel Perez <gperez@coachella.org>; Adrian Moreno amoreno@coachella.org; Eva Lara

<<u>elara@coachella.org</u>>; Maritza Martinez <<u>mmartinez@coachella.org</u>>; Castulo Estrada <<u>cestrada@coachella.org</u>>; Abraham Vega <<u>avega@coachella.org</u>>; Gabriel Martin

<gmartin@coachella.org>; Lizzandro Diaz <\ldiaz@coachella.org>; 'andrew Simmons

(<u>asimmons@coachella.org</u>)' <<u>asimmons@coachella.org</u>>; Efrain Rodriguez <<u>erodriguez@coachella.org</u>>;

Celina Jimenez <<u>cjimenez@coachella.org</u>>; 'ron@rgplanningconsultants.com'

<ron@rgplanningconsultants.com>; 'MAbbott@RIVCO.ORG' <MAbbott@RIVCO.ORG>;

'DVargas@IID.com' <<u>DVargas@IID.com</u>>; 'GBarraza@IID.com' <<u>GBarraza@IID.com</u>>; 'jlgerardo@iid.com'

 $<\!\!\underline{\mathsf{ilgerardo@iid.com}}\!\!>; \mathsf{'sbliss@29palmsbomi-nsn.gov'} <\!\!\underline{\mathsf{sbliss@29palmsbomi-nsn.gov}}\!\!>;$

'abecerra@tmdci.org' <abecerra@tmdci.org>; 'ltorres@cvusd.us' <ltorres@cvusd.us'>;

'patrick.cisneros@desertsands.us' < patrick.cisneros@desertsands.us; 'rvasquez@riversidesheriff.org'

<rvasquez@riversidesheriff.org>; 'IC-EnvironmentalServ@cvwd.org' <IC-EnvironmentalServ@cvwd.org>;

'IC-Engineering@cvwd.org' < IC-Engineering@cvwd.org; 'rruofmplanningeast@fire.ca.gov'

<<u>rruofmplanningeast@fire.ca.gov</u>>; 'kohl.hetrick@fire.ca.gov' <<u>kohl.hetrick@fire.ca.gov</u>>;

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<jguidry@sunline.org>; 'malcala@sunline.org' <malcala@sunline.org>; 'KTsang@rivco.org'

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STAFF REPORT 5/1/2024

To: Planning Commission Chair and Commissioners

FROM: Adrian Moreno, Associate Planner

SUBJECT: <u>CUP 375 AR No. 23-14 – Coachella Islamic Center Parking Lot</u> for the

construction of a parking lot to serve the existing building at 84650 Ave 49.

Applicant: Islamic Society of Palm Springs.

STAFF RECOMMENDATION:

Staff recommends that the Planning Commission continue consideration of Resolution No. PC2024-07 approving Conditional Use Permit 375 and Architectural Review No. 23-14 for the construction of a parking lot to serve the existing building at 84650 Ave 49, (APN: 603-250-015) to the Planning Commission meeting of May 15, 2024 as the project will require site plan modifications subject to Fire Department safety concerns.